

NORTH PEAK

PLANNED COMMUNITY



SPECIFIC PLAN
TMC COMMUNITIES

**FINAL
NORTH PEAK SPECIFIC PLAN
LAKE ELSINORE, CALIFORNIA**

**ADOPTED FEBRUARY 12, 1991
BY
THE CITY OF LAKE ELSINORE**

ORDINANCE NO. 908

**Prepared for:
TMC Communities**

**Prepared by:
The SWA Group
Laguna Beach, California**

APPROVED
2-26-91

ORDINANCE OF THE CITY COUNCIL OF THE CITY OF LAKE ELSINORE, CALIFORNIA, APPROVING THE NORTH PEAK SPECIFIC PLAN AND MAKING CERTAIN FINDINGS WITH RESPECT THERETO.

WHEREAS, TMC Developments has prepared the North Peak Specific Plan for an area of approximately 2,250 acres situated in the foothills of Steele Peak and Wasson Canyon; bounded by Steele Valley to the north, Wasson Canyon to the east, Highway 74 and the Lake Elsinore city limits to the south, and El Toro Road to the west for a residential and commercial development; and

WHEREAS, the Planning Commission has reviewed the North Peak Specific Plan and on December 5, 1990, adopted Resolution 90-18 recommending that the City Council adopt the North Peak Specific Plan; and

WHEREAS, an Environmental Impact Report was prepared for the North Peak Specific Plan (SCH 90020487) in accordance with the California Environmental Quality Act and the City's CEQA Guidelines, and the City Council has considered the information contained therein and in the other documents referred to therein; and

WHEREAS, the City Council has certified the EIR as complete and adequate.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKE ELSINORE DOES ORDAIN THAT:

Section I. Upon recommendation of the Planning Commission and based upon the findings adopted hereby with regard to the approval of the Project, the City Council hereby:

1. finds that the North Peak Specific Plan for the project area is consistent with the General Plan of the City of Lake Elsinore,
2. finds that the adoption of the North Peak Specific Plan is in the public interest, and
3. approves and adopts the North Peak Specific Plan.
4. adopts the environmental findings, facts in support of findings, and Statement of Overriding Considerations, attached hereto as Exhibit 1 and incorporated herein by reference.
5. adopts the mitigation measures and monitoring requirements set forth in the Mitigation Monitoring Program, attached hereto as Exhibit 2 and incorporated herein by reference.

Section 2. This Ordinance shall take effect thirty (30) days after the date of its passage. The City Clerk shall certify as to adoption of this Ordinance and cause this Ordinance to be published and posted in the manner required by law.

PASSED UPON FIRST READING this 12th day of February, 1991, by the following roll call vote:

AYES:	COUNCILMEMBERS:	BUCK, DOMINGUEZ, STARKEY, WASHBURN
NOES:	COUNCILMEMBERS:	NONE
ABSENT:	COUNCILMEMBERS:	NONE
ABSTENTIONS:	COUNCILMEMBERS:	WINKLER

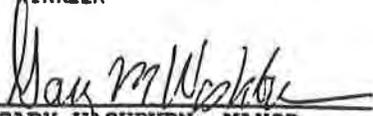
I declare under penalty of perjury that the foregoing instrument is a full, true and correct copy of the original on file in this office.

ATTEST: Adria L. Bryning
ADRIA L. BRYNING, Deputy City Clerk

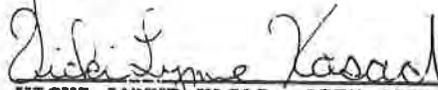
DATED: 3-1-91

PASSED, APPROVED AND ADOPTED UPON SECOND READING this 26th
day of February, 1991, by the following roll call vote:

AYES: COUNCILMEMBERS: DOMINGUEZ, STARKEY, WASHBURN
NOES: COUNCILMEMBERS: NONE
ABSENT: COUNCILMEMBERS: BUCK
ABSTENTIONS: COUNCILMEMBERS: WINKLER

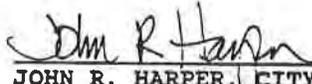

GARY WASHBURN, MAYOR
CITY OF LAKE ELSINORE

ATTEST:


VICKI LYNNE KASAD, CITY CLERK
CITY OF LAKE ELSINORE

(SEAL)

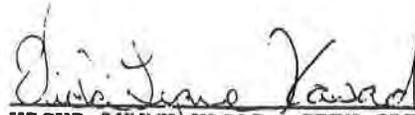
APPROVED AS TO FORM AND LEGALITY:


JOHN R. HARPER, CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) SS:
CITY OF LAKE ELSINORE)

I, Vicki Lynne Kasad, City Clerk of the City of Lake
Elsinore, DO HEREBY CERTIFY that the foregoing Ordinance was duly
adopted by the City Council of the City of Lake Elsinore at a
regular meeting of said Council on the 26th day of February 1991,
and that is was so adopted by the following vote:

AYES: COUNCILMEMBERS: DOMINGUEZ, STARKEY, WASHBURN
NOES: COUNCILMEMBERS: NONE
ABSENT: COUNCILMEMBERS: BUCK
ABSTENTIONS: COUNCILMEMBERS: WINKLER


VICKI LYNNE KASAD, CITY CLERK
CITY OF LAKE ELSINORE

(SEAL)

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) SS:
CITY OF LAKE ELSINORE)

I, Vicki Lynne Kasad, City Clerk of the City of Lake Elsinore, DO HEREBY CERTIFY that the above and foregoing is a full, true and correct copy of Ordinance No. 908 of said Council, and that the same has not been amended or repealed.

DATED: February 27, 1991

Vicki Lynne Kasad
VICKI LYNNE KASAD, CITY CLERK
CITY OF LAKE ELSINORE

(SEAL)

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SECTION I: EXECUTIVE SUMMARY

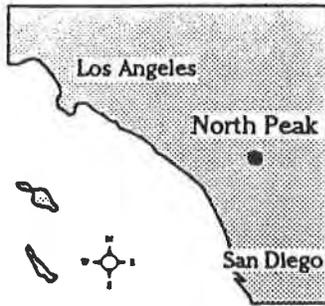


EXHIBIT 1.1A
REGIONAL LOCATION

1.1

INTRODUCTION

This document, together with its accompanying maps and diagrams, is the North Peak Specific Plan. It has been prepared by TMC Communities for the City of Lake Elsinore, California, and describes the proposed development of approximately 1964 acres at the northeastern edge of the City. Exhibit 1.1a shows the regional location of the site.

North Peak will be a planned community—a community that contains a mixture of open space, residential, commercial and public uses, as well as roads and trails that have been carefully planned to create an integrated, dynamic environment in which residents can live, work, shop and play. The North Peak Specific Plan describes all aspects of the planned community in detail and will guide its development.

1.2

PURPOSE

There are compelling reasons why the North Peak Specific Plan has been prepared at this time. Perhaps the most important is that the regional demand for a high quality, planned community living environment has increased significantly in the Lake Elsinore community in the last few years.

TMC Communities, North Peak's landowner, has a strong interest in supporting the City's desire to foster orderly growth. The North Peak Specific Plan addresses the City's needs by providing the tool to guide development of the property. North Peak will be a place in which the various elements of the community have been integrated to create a coherent, dynamic whole.

In addition to carefully locating each land use, open space area and circulation component in North Peak, the Plan proposes a Community Design Element that will unify the community by creating an overall identity that is immediately recognizable and fosters a sense of continuity with Southern California's rich historical landscape tradition.

1.3**DESCRIPTION OF THE SPECIFIC PLAN**

Physically, the North Peak site is divided into three general planning areas. The southern end of the site, adjacent to highway 74, consists of gently rolling hills from which Lake Elsinore and the mountains beyond are visible (Exhibit 1.3a). The middle of the site is a broad, enclosed valley that forms a transition between the hills to the south and the rugged uplands to the north (Exhibit 1.3b). In the shadow of several rocky peaks that form the northern third of the site is a highland plateau which is the last of the three planning areas (Exhibit 1.3c).

In response to the geography of the site, the Plan proposes three villages for North Peak, each occupying a distinct physiographic unit. The villages will be called North Peak Hills, North Peak Valley, and North Peak Highlands in recognition of their origin. Each Village will consist of a mix of residential living environments, and will focus on a Village Center that contains a mix of private and public uses. Each Village will also contain a neighborhood school/park intended primarily for residents of that Village.



EXHIBIT 1.3A
NORTH PEAK HILLS SITE



EXHIBIT 1.3B
NORTH PEAK VALLEY SITE



EXHIBIT 1.3C
NORTH PEAK HIGHLANDS SITE

1.0 EXECUTIVE SUMMARY

To unify and provide structure for the community, the Plan sets aside over half of the total area of the site as permanent open space. This open space will create a dramatic setting for North Peak's three Villages, and will also provide both active and passive recreation experiences for North Peak residents.

Also, to further create unity, the Plan proposes extensive design guidelines that are intended to evoke the traditional Southern California landscape as it existed in the early years of the twentieth century. Responding to landscapes traditionally found in the Lake Elsinore area (Exhibit 1.3d), the Plan proposes eucalyptus woodlands, orchards, processional rows of palms, as well as retention of a significant area of the native landscape in furtherance of this goal.

The development program for the site calls for the construction of 4621 residential dwellings, 32 acres of neighborhood and highway commercial uses, three elementary schools, three active use parks and other amenities designed to serve North Peak's anticipated population of nearly 15,000 persons.



EXHIBIT 1.3d
HISTORIC LAKE VIEW INN, LAKE ELSINORE

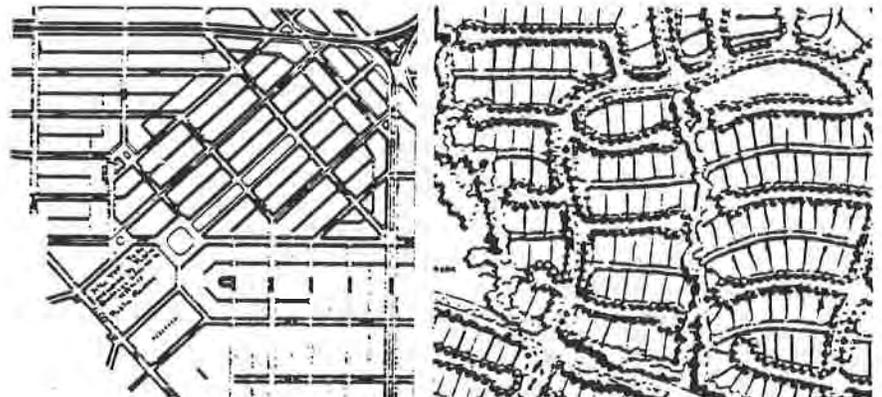
1.4

UNIQUE FEATURES OF THE PLAN

Several aspects of the North Peak Specific Plan are noteworthy in that they are innovative responses to the changing attitudes and life-styles of Southern Californians about planned communities.

First, the Plan accommodates development of traditional, as well as contemporary, neighborhoods. Traditional neighborhoods are characterized by geometrical street layouts, alleys, a fine-grained mix of uses and emphasis on accommodating pedestrians, especially on streets. Contemporary neighborhoods generally have curvilinear streets, many of which are cul-de-sacs, garages that are oriented to streets, and land uses separated according to function. The North Peak Plan recognizes that both approaches are valid and provides standards and guidelines that make both types of development possible within the overall context of the Plan (Exhibit 1.4a).

Second, the Plan proposes three Village Centers that will serve as the social, commercial, recreational, and spiritual center of each Village. Envisioned as an evocative likeness of a traditional town center, they are intended to enhance the overall identity of the entire North Peak community as well as the Village they serve. Although each Center varies somewhat in makeup, they are intended to contain a complementary mix of residential, commercial, and public uses.



A TRADITIONAL CALIFORNIA NEIGHBORHOOD

A CONTEMPORARY CALIFORNIA NEIGHBORHOOD

EXHIBIT 1.4A

NORTH PEAK MAY HAVE BOTH TRADITIONAL AND CONTEMPORARY NEIGHBORHOODS

1.5**THE NORTH PEAK ILLUSTRATIVE PLAN**

The Illustrative Plan on the facing page (Exhibit 1.5a) is intended to convey a general idea of how the planning concepts described in the Specific Plan may be realized as North Peak is developed. Special emphasis has been given in this drawing to the Village Centers and overall landscape framework which will play important roles in defining the character and quality of the community. It is important to realize, however, that this drawing is only intended to convey the overall concept; it is not a commitment to a specific site plan or design for those areas delineated with buildings and lots.

1.6**GUIDE TO THE SPECIFIC PLAN**

The Specific Plan is divided into six parts. Part I, which concludes here, contains a brief summary of the Specific Plan, with special attention to the Plan's principal concepts and special features. Part II is an introduction to the Specific Plan that describes the location and environmental setting for the Plan. Part III is a complete description of the Plan and contains detailed descriptions of the proposed land use, circulation, infrastructure and public facility development concepts. The Village Centers are also described, as well as the open space and recreation concepts. Part IV contains the Community Design Element of the Specific Plan. It describes the elements of the Plan that will create the overall identity of North Peak. Part V contains information about the implementation of the North Peak Specific Plan, such as phasing, infrastructure financing and the development approval process. Finally, Part VI contains the Specific Plan Regulations that supplement or replace the traditional zoning regulations that apply in other areas of the City of Lake Elsinore.

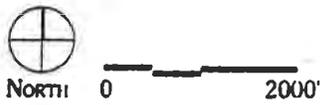
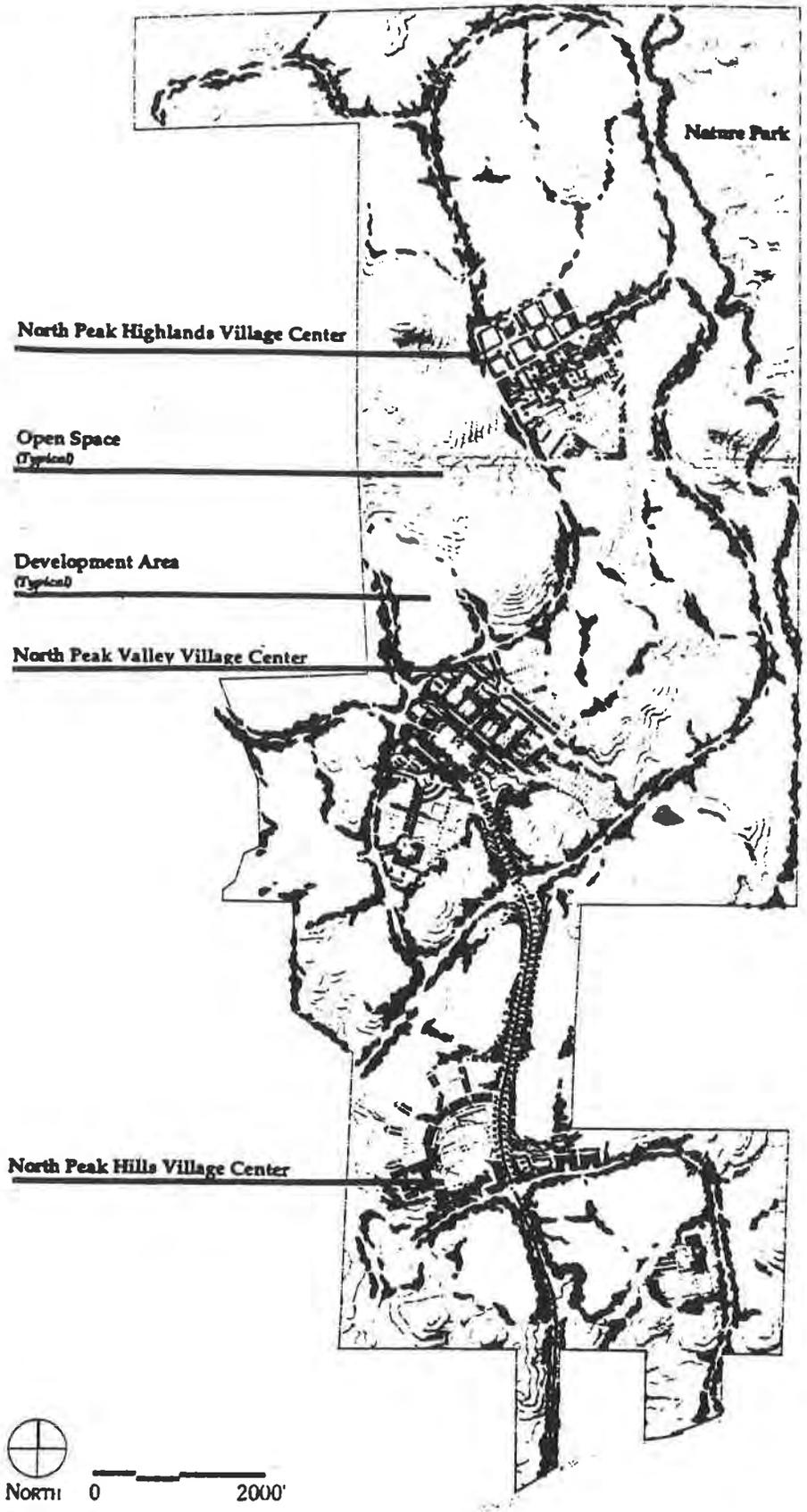


EXHIBIT 1.5A
NORTH PEAK ILLUSTRATIVE PLAN

SECTION II: INTRODUCTION

2.1

PURPOSE OF THE SPECIFIC PLAN

The City of Lake Elsinore is undergoing a period of growth, a trend that is expected to continue for the next several years. In response to this, the City is currently updating its General Plan and other planning documents, including its Zoning Ordinance. The policies, plans and programs of the City's General Plan will establish the basic framework for development of the North Peak planning area. The primary purpose of this Specific Plan, then, will be to implement the policies of the City. The North Peak Specific Plan addresses interrelated environmental considerations, balancing competing needs. When adopted, the Specific Plan will serve as the primary planning document to guide and regulate development within the North Peak planning area, consistent with the policies of the General Plan.

2.2

LEGAL BASIS FOR THE SPECIFIC PLAN

The North Peak Specific Plan has been created through the authority granted to the City of Lake Elsinore by the California Government Code. The adoption of the North Peak Specific Plan by the City of Lake Elsinore is authorized by the California Government Code, Sections 65450 through 65457. Pursuant to these provisions, the City's Planning Commission must hold a public hearing before the Planning Agency can recommend the adoption of the Specific Plan. Following a public hearing, the City Council may then adopt the Specific Plan by ordinance or resolution.

2.3

CONTENT OF THE SPECIFIC PLAN

The California Government Code stipulates that a specific plan contain text and diagrams that specify the following in detail:

LAND USE

The Specific Plan must specify the distribution, location and extent of the uses of land, including open space, within the area covered by the Plan.

PUBLIC FACILITIES

The Specific Plan must show the proposed distribution, location and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste

disposal, energy and other essential facilities located within the area covered by the Plan and needed to support the land uses described in the Plan.

DEVELOPMENT STANDARDS

The Specific Plan also must include standards and criteria by which development will proceed, and standards for the conservation, development and utilization of natural resources, where applicable.

IMPLEMENTATION MEASURES

The Specific Plan must include a program of implementation measures, including regulations, programs, public works projects and financing measures.

CONSISTENCY WITH THE GENERAL PLAN

The Specific Plan must include a statement of the relationship of the Specific Plan to the General Plan.

OPTIONAL CONTENTS

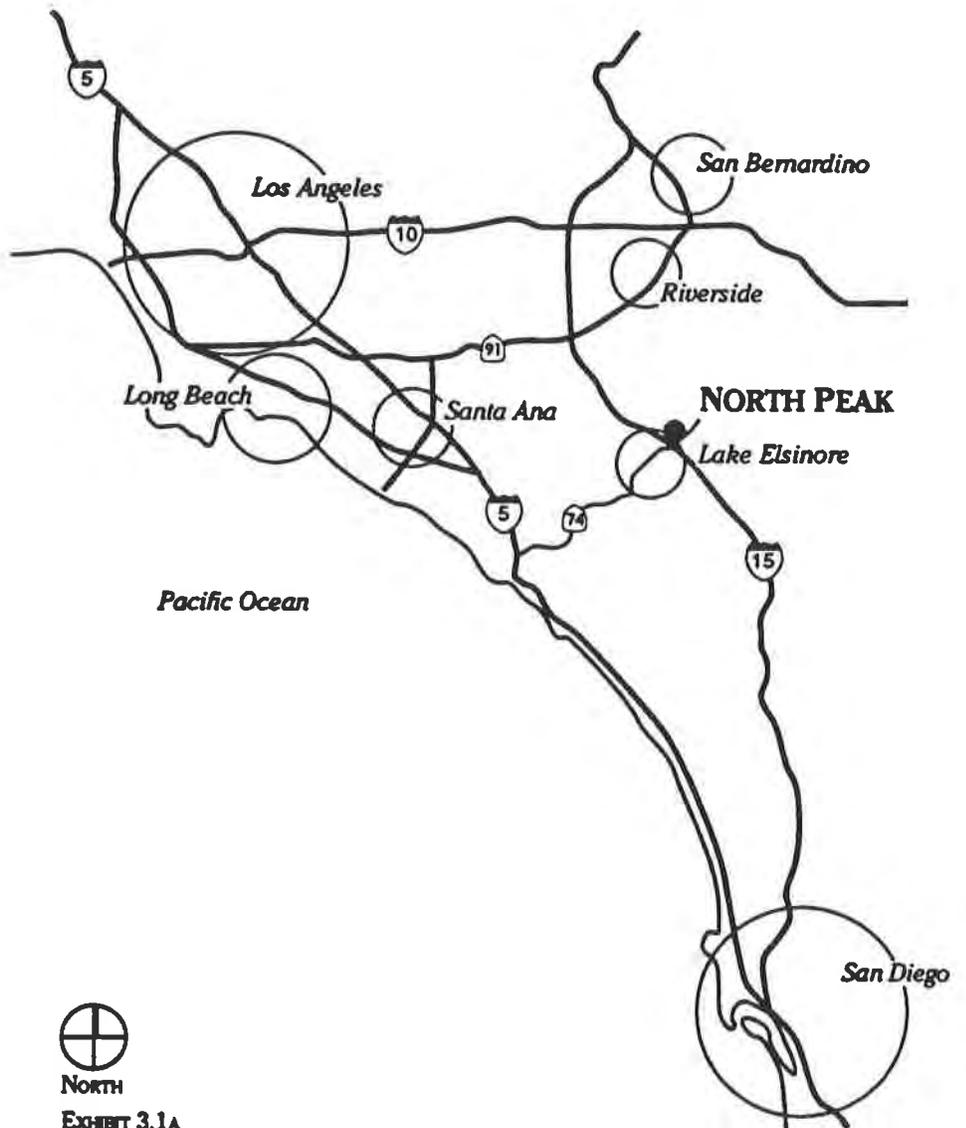
The Specific Plan may address any other subjects which, in the judgment of the planning agency, are necessary or desirable for implementation of the General Plan.

In order to obtain approval, local public works projects, tentative or parcel maps, and zoning ordinances within the North Peak planning area must be consistent with the North Peak Specific Plan.

3.1

LOCATION AND EXTENT

North Peak is located approximately 50 miles southeast of Los Angeles near Lake Elsinore. Exhibit 3.1a is a regional location map that shows the relationship of the site to the Southern California environment.



NORTH
EXHIBIT 3.1A
REGIONAL LOCATION
Not to Scale

3.0 ENVIRONMENTAL SETTING

The North Peak Specific Plan planning area contains about 1,964 acres. However, as part of the consideration of the Specific Plan, the City will be annexing three additional parcels for a total of 2,250 acres to be annexed with North Peak. Exhibit 3.1b shows both the planning area and the three areas proposed for annexation. It also shows the city limits of Lake Elsinore and other jurisdictional boundaries and principal circulation routes in the Lake Elsinore area.

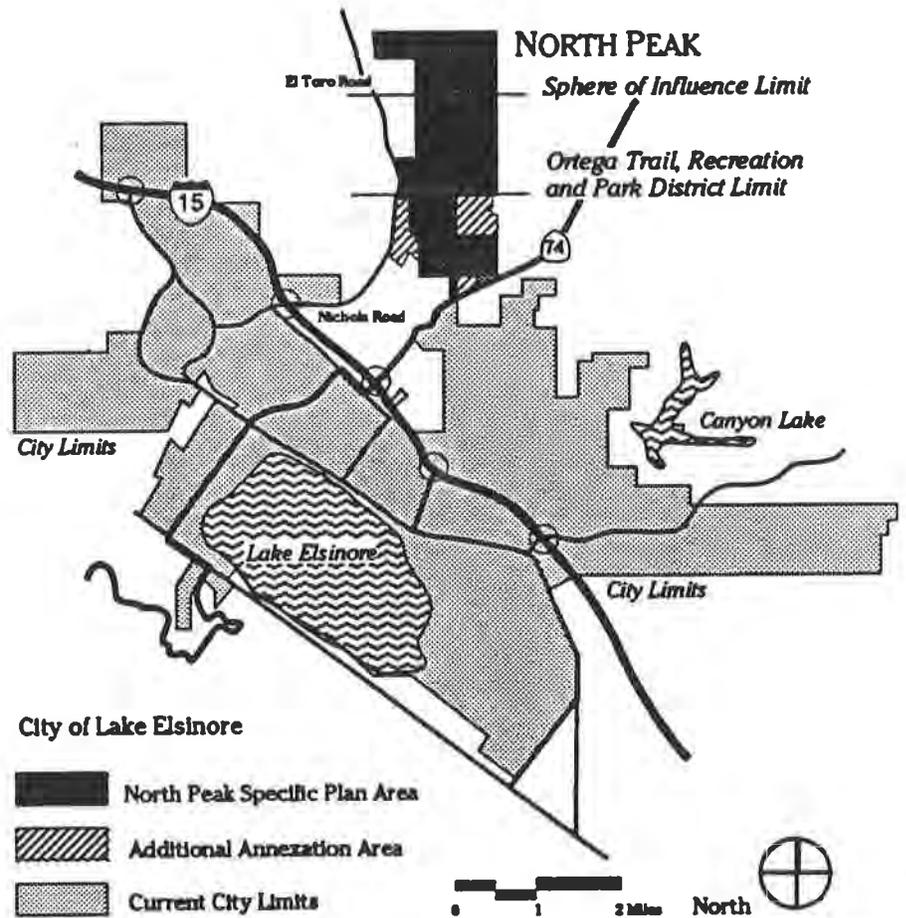


EXHIBIT 3.1b
LAKE ELSINORE REGION

3.2

EXISTING CONDITIONS

The following is a brief description of the existing conditions at North Peak. A detailed description can be found in the EIR which accompanies this document. In the following description, all references to the site include only those parcels that will be covered by this Specific Plan.

GEOLOGY AND LANDFORM

Geologically, the site is divided into two parts. The southern half of the site consists of rolling hills underlain by metasediments, that is, sedimentary rocks altered by heat and pressure. The central portion of the site is a geologic transition between the metasediments of the southern half and granitic material of the northern half of the site. The northern part of the site contains many granite boulders on the surface and is the location of the Peak from which North Peak gets its name. No active faults are located within the site. Nine different soil types occur within the site; most are sandy to rocky or gravelly loams and are of low suitability for intense agricultural use.

HYDROLOGY AND DRAINAGE

The site is located in the Santa Ana River basin, which covers a large area of Southern California. Two intermittent streams drain the site: Arroyo del Toro, which is located to the west of the site and forms a short segment of its western boundary, and Wasson Canyon Wash, which flows across the northeastern corner of the site. Each drains approximately one-half of the site: Arroyo del Toro the southern half, and Wasson Canyon Wash the northern half. Both streams flow in natural channels and neither presents flood hazard to the site, according to the Flood Insurance Rate Map published by the Federal Emergency Management Agency. According to U.S. Soil Conservation Service soil classifications, 85% of the soils on site have moderate to high runoff potential because of the steep slopes and sparse vegetation. Due to the hilly terrain, the site does not contribute to an identified groundwater basin, and no seeps or springs have been identified. Based on the depth of wells on adjacent property and the presence of green vegetation in canyon bottoms, it is likely that there is ground water close to the surface of low-lying areas.

TOPOGRAPHY AND SLOPE

The North Peak site ranges in elevation from 1,430 feet to 2,420 feet. Generally, the site is divided into three topographic areas. The southern third consists of hilly terrain dissected by stream-cut arroyos. A valley-like landform with more gentle slopes is located near the geographic center of the site, and the northern third consists of both a rocky plain and several tall granite peaks. Exhibit 3.2a is a slope map that shows the distribution of three slope categories within the project site.

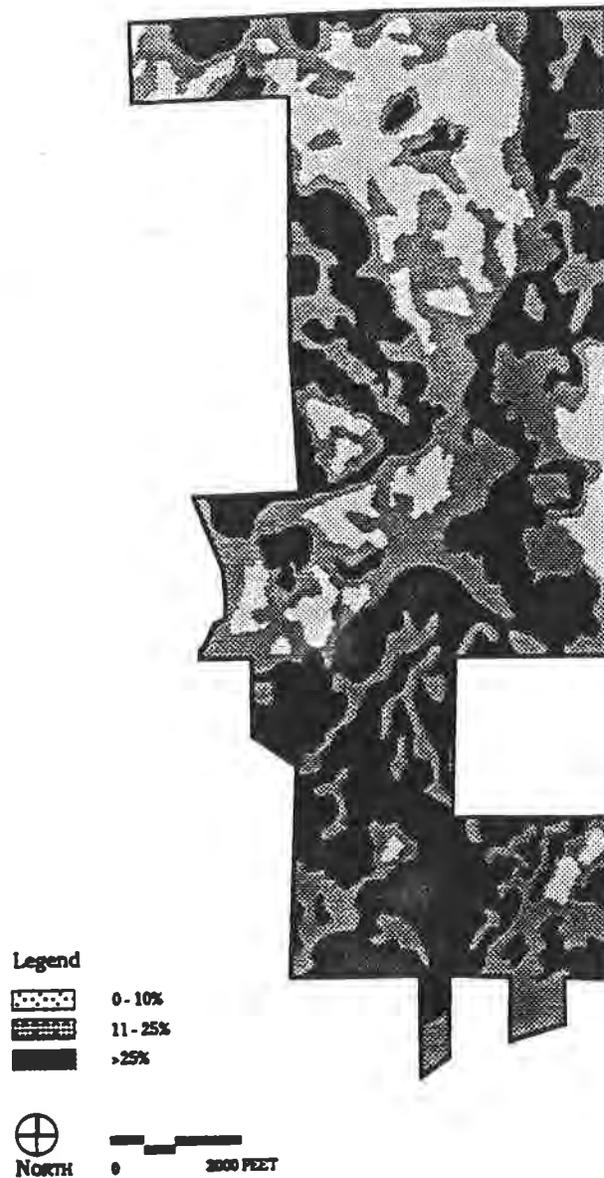


EXHIBIT 3.2A
SLOPE MAP

VEGETATION AND WILDLIFE

Although there is a diversity of plant communities located on the project site, nearly all have been significantly disturbed by grazing. The most common plant community is Coastal Sage Scrub, but there are also areas of Chaparral, Annual Grassland and Riparian vegetation. Animals associated with these plant communities include mammals such as rodents, coyotes and deer, as well as reptiles, birds, and amphibians. The greatest diversity and concentration of animals is found near the riparian areas. Several species of both plants and animals that are considered to be sensitive because of their rarity can be found within the project site. The best known of these is the Steven's kangaroo rat.

EXISTING LAND USE

Currently, the site is vacant and unused. An old, abandoned farmhouse and several outbuildings remain in a canyon in the southern third of the property. They have been empty for several years and are in a dilapidated condition.

JURISDICTION

The site is within the jurisdiction of Riverside County, California. However, the southern two-thirds of the site is located within the Sphere of Influence of the City of Lake Elsinore.

OWNERSHIP

Except for six small parcels of about 40-acres in the northern third of the site, and a parcel of approximately 160-acres in the southern third belonging to the Bureau of Land Management (BLM), ownership of the property is controlled by TMC Communities. Currently, TMC Communities is negotiating an exchange in which they will trade land off-site for the land owned by the BLM.

SURROUNDING LAND USE

The dominant land use surrounding the North Peak site is vacant, unimproved land. Much of the terrain is quite rugged and there is little access other than El Toro Road, a narrow, unpaved County road that eventually connects to Perris 10 miles to the

north. The exceptions to the unimproved nature of the surrounding area are an orange grove and microwave relay station located to the north of the site, a rural residential area near the southeast corner of the site, and another rural residential area to the southwest.

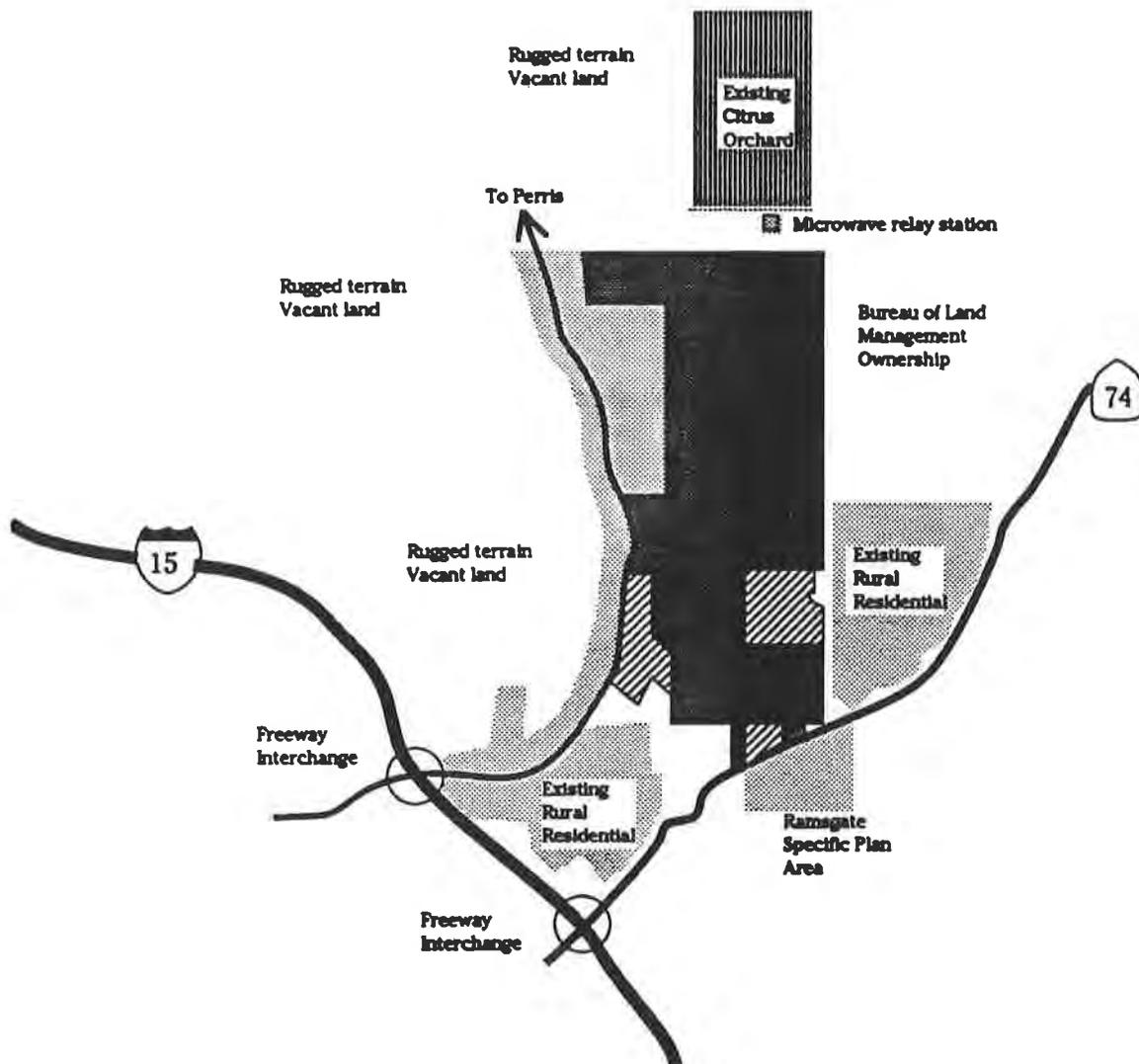


EXHIBIT 3.1c
PLANNING, ANNEXATION, SPECIFIC PLAN AREA

CURRENT PLANNING

Since the site is currently within the jurisdiction of the County, the Riverside General Plan establishes the existing planning and zoning designations. However, because of the approaching urbanization of the area around North Peak, the City of Lake Elsinore is seeking to annex this area to the City (see Exhibit 3.1c). Once annexed, development of these areas will be controlled by the City's General Plan and supporting ordinances.

The City of Lake Elsinore adopted its current General Plan in December, 1982 (Resolution No. 82-83). The plan projects the development of the community to the year 1992. At this time the City is updating its General Plan to better respond to the future, including the imminent development of large areas of land such as North Peak. The Draft General Plan (dated July, 1990) designates the North Peak Specific Plan Area 2.5 dwelling units to the acre.

3.3

SUMMARY OF EXISTING CONDITIONS

Exhibit 3.3a on the following page is a map of the site that shows the existing environmental conditions that are relevant to the development of the site.

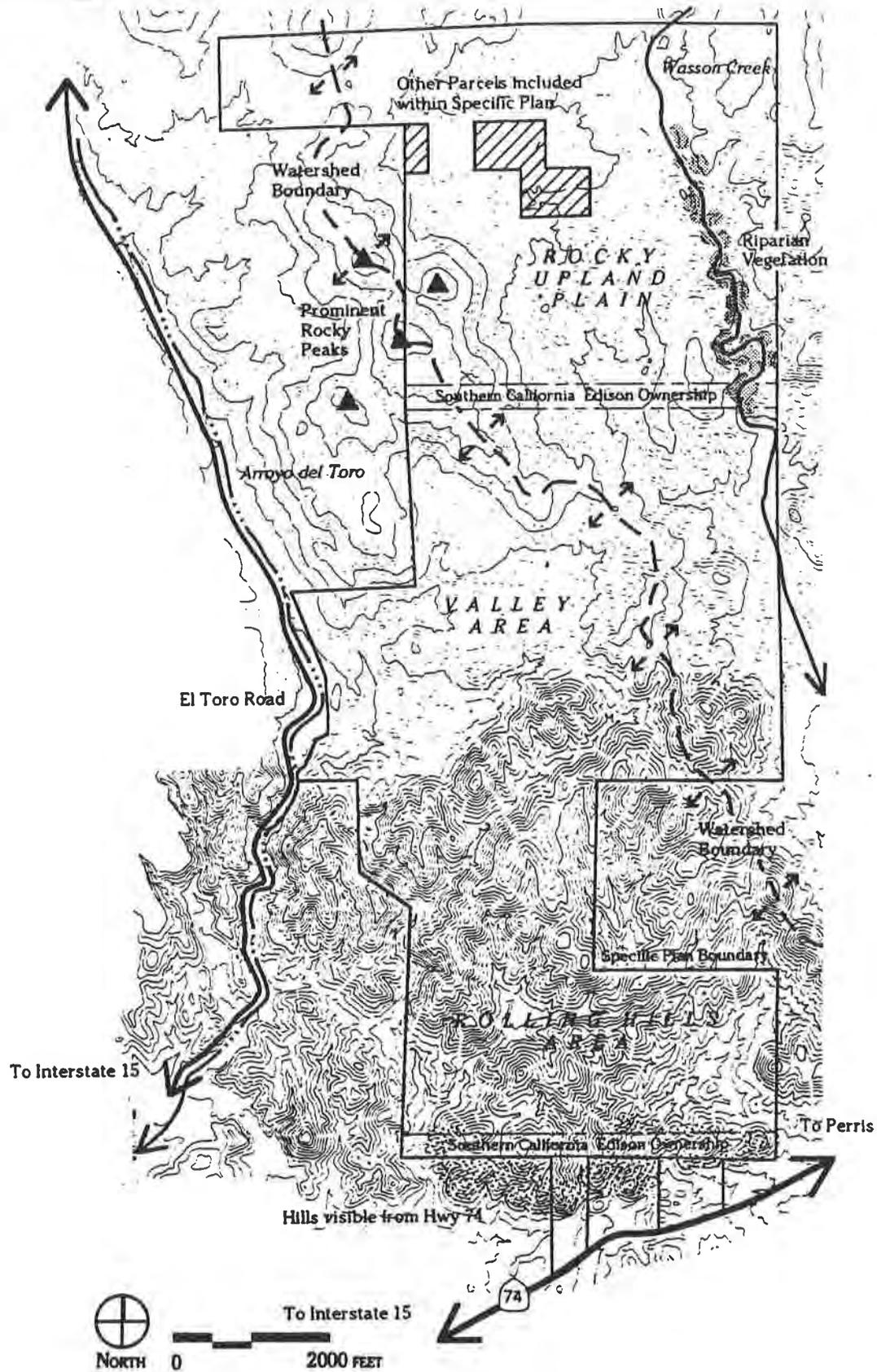


EXHIBIT 3.3A
SUMMARY OF EXISTING CONDITIONS

SECTION III: DEVELOPMENT PLAN

4.1

OBJECTIVES OF THE SPECIFIC PLAN

Several fundamental objectives have guided the preparation of the North Peak Specific Plan. They are:

- Provide attractive neighborhoods which offer a wide range of housing opportunities.
- Create distinct neighborhood identities through the establishment of "villages" with internal commercial services and recreation facilities.
- Provide neighborhood commercial uses within the Specific Plan which serve the immediate needs of its residents, while not conflicting or competing with the City's downtown development potential.
- Reduce the number and length of vehicle trips by including internal commercial services, recreation and multi-modal transportation links within residential villages.
- Provide public facilities and improvements necessary to accommodate the build out of the Specific Plan and to maintain a high quality level of service for these residents.
- Preserve substantial areas for open space, including significant natural resources.
- Provide sufficient area for both active and passive recreation in conjunction with open space and trail systems throughout the Specific Plan.

4.2

OVERALL PLAN CONCEPTS

To achieve the fundamental planning goals, the Plan defines five planning concepts that form the basis of the physical plan for North Peak. They include:

- Provision within the Plan for a well-integrated mix of land uses;
- Division of the planning area into three villages based on the physical geography of the site;
- Accommodation of both traditional and contemporary neighborhoods within the community;
- Village centers that serve as a focus for each village; and
- A strong community identity that evokes the Southern California landscape tradition.

The following paragraphs describe each of these fundamental concepts in detail.

4.3

A WELL-INTEGRATED MIX OF LAND USES

A fundamental planning objective for North Peak is to integrate a mix of diverse land uses within the community. Among the uses that are planned for North Peak are residential uses at a variety of densities, commercial uses such as retail establishments, public facilities such as schools and churches, and open space, including parks and recreational facilities. Special consideration has been given to the location and arrangement of these uses to promote the integrity and vitality of the overall plan. For example, the open space system will provide a structural backbone to the overall community design.

The organization of the uses within the community has been considered, too. For example, higher density residential areas are generally located close to the Village centers and the major roads, while lower density uses are located nearer the village perimeter. This approach will help reduce congestion. Also, residential areas have been varied along major roads to create variety in the visual experience of travelers on North Peak's roads.

4.4

THREE VILLAGES

As described in Section 1.3, the physical geography within the planning area divides the site into three areas: the southern portion of the site, which consists of hills and stream-cut arroyos; the central portion, which is a valley-like transition area; and the rocky peaks and highland plateau of the northern third of the site.

Each physiographic area will become a separate village within the North Peak planning area. The southern village will be called North Peak Hills and will be located next to State Highway 74 on the southern third of the site. The transition area between the southern hills and the rocky peaks will be called North Peak Valley. The northern rocky plateau and peaks will be called North Peak Highlands.

Each village will contain of a mixture of residential uses intended to appeal to people with a range of incomes and variety of life-styles. There will also be a village center which will contain commercial, civic and recreational uses intended primarily for North Peak residents.

The three villages are described in more detail in Sections 5.2, 5.3, and 5.4.

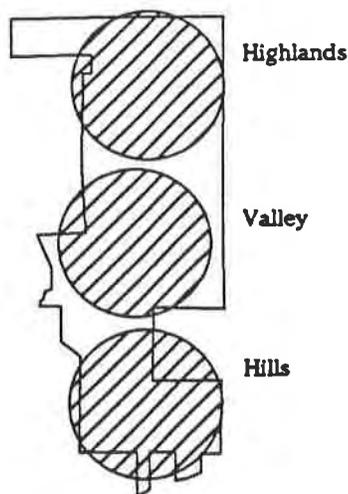


EXHIBIT 4.4A
NORTH PEAK'S THREE PHYSIOGRAPHIC
AREAS FORM THE BASIS FOR THE
THREE VILLAGES

4.5

TRADITIONAL AND CONTEMPORARY PLANNING TECHNIQUES

North Peak will be unique among Southern California planned communities because of the incorporation of standards which will permit the development of traditional, as well as contemporary, neighborhoods.

Traditional neighborhoods exist throughout Southern California. Generally, they are located in areas developed prior to 1940, and have a character that is different than the suburbs that were built after 1950. Among the factors that distinguish these older, traditional neighborhoods are their street widths, which are often narrow, the subdivision design, which often incorporates a grid street layout, and the heterogeneity of neighborhood land uses. (Exhibit 4.5a)

Contemporary neighborhoods have been developed since the 1950's and display many differences from the tradition established in the early twentieth century. Curvilinear road layouts and cul-de-sacs are common. Residential neighborhoods are often the result of the work of a single builder, rather than many unrelated builders. (Exhibit 4.5b)



EXHIBIT 4.5A
TRADITIONAL NEIGHBORHOOD



EXHIBIT 4.5B
CONTEMPORARY NEIGHBORHOOD

The following table is a summary of some of the differences that distinguish traditional from contemporary neighborhoods:

	<i>Traditional Neighborhood</i>	<i>Contemporary Neighborhood</i>
<i>Land Uses</i>	Heterogeneous; often mixed	Homogeneous; rarely mixed
<i>Neighborhood Design</i>	Geometric; Grid common	Curvilinear; cul-de-sacs common
<i>Street Design</i>		
<i>Pavement Width</i>	Narrow (often less than 30')	Wide (usually more than 35')
<i>Design Speeds</i>	Low; usually less than 25 mph	Medium; often more than 30 mph
<i>Intersections</i>	Cross intersections common on local streets; T intersections rare	T intersections common on local streets; cross intersections rare
<i>Public Alleys</i>	Common; often required	Rare; often prohibited
<i>Sidewalk design</i>	4' to 8' parkways with street trees common	No parkway (curb adjacent to street); street trees rare
<i>Buildings</i>	Variety from construction of neighborhood on lot-by-lot basis	Consistency from development of neighborhoods as large tracts
<i>Residential Garages</i>	Often in rear yard; inconspicuous from street	Often conspicuous from street
<i>Commercial District Parking</i>	On-street parking common; Off-street lots often small	Large, off-street lots; On-street parking often prohibited

TABLE 4.5A

TRADITIONAL VS. CONTEMPORARY NEIGHBORHOOD CHARACTERISTICS

In North Peak, provision for traditional neighborhood design has been made within the village centers to increase the appeal of the community to people with a desire to live in a more traditional neighborhood environment.

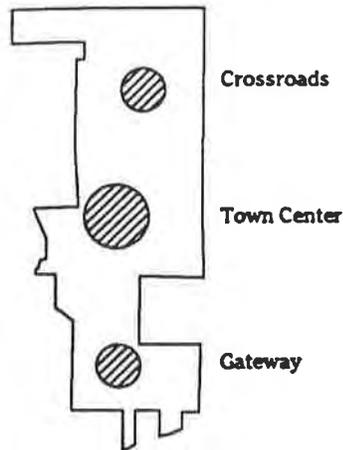


EXHIBIT 4.6A
NORTH PEAK VILLAGE CENTERS

4.6

VILLAGE CENTERS

North Peak's village centers are another fundamental element in the overall concept for the Community. Basically, they are intended to serve as a focus for village social, commercial, recreational, civic and spiritual activity. As such, they will be located near the geographic center of each village and will contain a diverse mix of residential, commercial, and public land uses in close proximity. To accomplish the unique purpose of the village centers, the Plan proposes a Village Center Overlay District. Within this district, special standards will apply that permit more traditional types of development.

Each of the three centers will serve somewhat different purposes within the overall community and so will vary from one another in size, mix of uses, and function within the community. The intent is to establish a hierarchy in which the center in North Peak Valley is the largest center and serves as a focal point for the entire planned community, as well as the village itself. The centers in North Peak Hills and North Peak Highlands will be smaller, and their functions more limited and specialized.

The Village Center Overlay District and its role in establishing the character of the community is described in greater detail in Section 9.0. Also, Section 9.0 contains design criteria intended to guide development of the village centers in the traditional neighborhood fashion, and Section 26.0 contains the Village Center Overlay District Regulations.

4.7**THE SOUTHERN CALIFORNIA LANDSCAPE TRADITION**

Because of its Mediterranean climate and remarkable history, the Southern California landscape has an unforgettable character. An important contributor to this character is the tradition of well tended orchards, palm lined streets, and eucalyptus groves. The image of orange groves under snow-capped peaks still draws visitors and new residents to California, much as it did fifty or sixty years ago. Southern California roads and highways also create romantic visions with their rows of stately palms or lacy eucalyptus. The landscape character of North Peak will be based on this tradition and will help establish North Peak's reputation as a community of California tradition.

Four landscape elements are essential to this objective. First, extensive areas of native landscape will be retained in large areas of open space within the community. Second, eucalyptus trees will be widely planted along roads in a manner that recalls the eucalyptus plantations of the past. Third, in areas of more intense human use, such as the village centers, orchard style planting will help reinforce the community character. Finally, palms and other trees with a majestic quality will be planted in formal rows in areas that are especially important. These four elements will weave together throughout the community to evoke the Southern California landscape tradition.

The Landscape Master Plan in Section 14.0 describes the landscape plan in more detail and provides specific criteria for implementation of the overall landscape concept.



EXHIBIT 4.7A SOUTHERN CALIFORNIA'S UNFORGETTABLE LANDSCAPE CHARACTER DEPICTED ON SUNNY COVE ORANGE CRATE LABEL.

5.1

DESCRIPTION OF THE LAND USE PLAN

Exhibit 5.1a is the land use plan for the North Peak community. This plan shows the location of all major land use areas, major roads, and other important elements of the community. When approved, it will guide land use decisions within the planning area. Accompanying this plan is Table 5.1a which summarizes the amount and distribution of land use within North Peak.

Community Totals

<i>Land Use</i>	<i>Map Designation</i>	<i>Density Range</i>	<i>Average Density</i>	<i>Area</i>	<i>Dwellings/Square Feet</i>	<i>% of Total Dwellings</i>
Residential						
Rural Residential	RR	0-2 du/a	1 du/a	50.0 acres	50	1%
Estate Residential	ER	2-4 du/a	3.5 du/a	310.0 acres	1,085	23%
Single Family	SF	4-7 du/a	6 du/a	333.0 acres	1,998	43%
Garden Residential	GR	7-12 du/a	9 du/a	77.0 acres	693	15%
Village Residential	VR	12-16 du/a	15 du/a	25.0 acres	375	8%
Multifamily	MF	16-24 du/a	20 du/a	21.0 acres	420	9%
Commercial						
Village Commercial	VC	.25-.50 FAR	0.25 FAR	18.0 acres	196,020	
Highway Commercial	HC	.25-.50 FAR	0.30 FAR	14.0 acres	182,952	
Public Use						
School/Park	SCH/P			30.0 acres		
Public Park	P			49.0 acres		
Nature Park	NP			230.0 acres		
Open Space						
Natural Open Space	OS			648.8 acres		
Utility Corridors	UC			63.1 acres		
Circulation						
Major Roads				95.0 acres		
			Totals	2.4 du/a	1963.9 acres	4,621 Dwellings 378,972 SF

TABLE 5.1A
NORTH PEAK LAND USE TABULATIONS

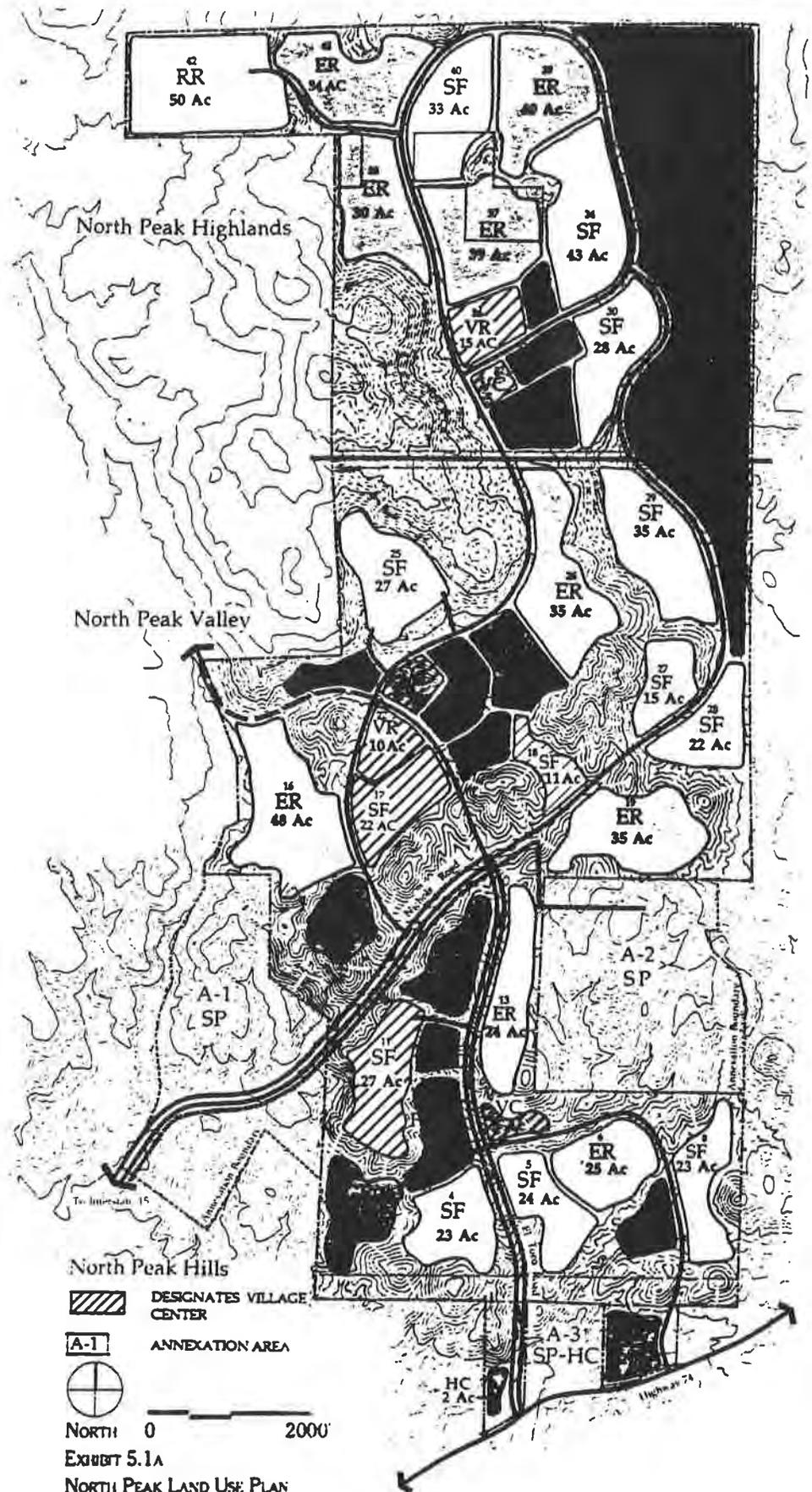


EXHIBIT 5.1A
NORTH PEAK LAND USE PLAN

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Three parcels shown on the land use plan will not be part of the Specific Plan. They are labeled Area A-1, A-2, and A-3, and are designated SP, SP and HC, respectively. These parcels will be annexed to the City of Lake Elsinore at the same time as North Peak. The designations refer to designations described in the Lake Elsinore Zoning Ordinance. Table 5.1b contains information these areas.

Annexation Areas

<i>Annexation Area</i>	<i>Map Designation*</i>	<i>Average Density</i>	<i>Area</i>	<i>Dwellings/Square Feet</i>
Area A-1	SP	2.5 du/a	147 acres	368
Area A-2	SP	2.5 du/a	152 acres	380
Area A-3	HC	0.3 FAR	12 acres	156,816

*Map Designations refer to the Lake Elsinore Zoning Ordinance

TABLE 5.1B
ANNEXATION PARCELS LAND USE

In order to better understand the organization of the plan and its various elements, the following sections describe each of the three villages in North Peak.

5.2

NORTH PEAK HILLS

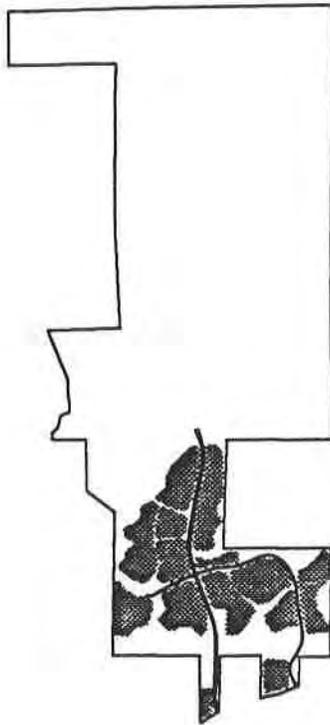


EXHIBIT 5.2A
LOCATION OF NORTH PEAK HILLS

North Peak Hills is the southernmost Village in North Peak. Located on the gently rolling hills immediately north of Highway 74, North Peak Hills will be the first of the three Villages to be developed. Automobile access to the Village will be from the realigned El Toro Road, which will bisect the Village perpendicular to Highway 74. Additional access will be from a collector road that intersects Highway 74 approximately one quarter mile east of El Toro.

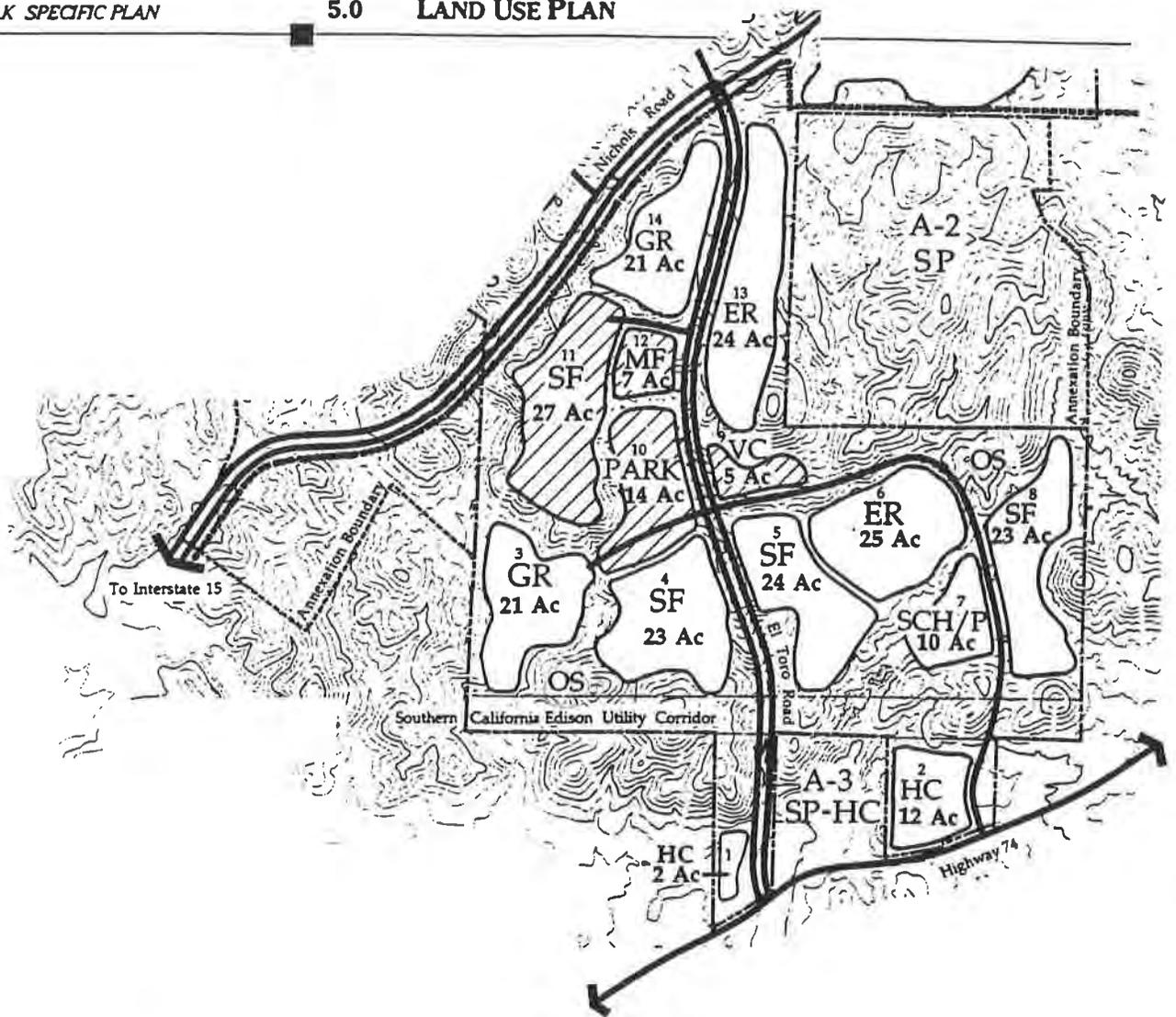
North Peak Hills will consist of residential dwellings that include single family houses, town houses and apartments. An elementary school in the southeast corner of the Village will serve local school children. On Highway 74, two parcels will be reserved for commercial uses oriented to city-wide needs. Potential uses include retail stores, offices or business park uses. The Village Center, described in more detail in Section 9.8, will serve as a gateway to the entire North Peak community, as well as meet local Village needs.

The proposed land use plan for the Village is shown on Exhibit 5.2b. The following tabulation summarizes the distribution of land uses within the Village.

North Peak Hills

Land Use	Map Designation	Density Range	Average Density	Area	Dwellings/Square Feet	% of Total Dwellings
Residential						
Rural Residential	RR	0-2 du/a	1 du/a	0.0 acres	0	0%
Estate Residential	ER	2-4 du/a	3.5 du/a	49.0 acres	171	13%
Single Family	SF	4-7 du/a	6 du/a	97.0 acres	582	46%
Garden Residential	GR	7-12 du/a	9 du/a	42.0 acres	378	30%
Village Residential	VR	12-16 du/a	15 du/a	0.0 acres	0	0%
Multifamily	MF	16-24 du/a	20 du/a	7.0 acres	140	11%
Commercial						
Village Commercial	VC	25-50 FAR	0.25 FAR	5.0 acres	54,450	
Highway Commercial	HC	25-50 FAR	0.30 FAR	14.0 acres	182,952	
Public Use						
School/Park	SCH/P			10.0 acres		
Public Park	P			14.0 acres		
Nature Park	NP			0.0 acres		
Open Space						
Natural Open Space	OS			151.3 acres		
Utility Corridors	UC			39.2 acres		
Circulation						
Major Roads				43.0 acres		
			Totals	2.7 du/a	471.5 acres	1,271 Dwellings 237,402 SF

TABLE 5.2A
NORTH PEAK HILLS - LAND USE TABULATION



- Residential**
- Rural Residential RR
- Estate Residential ER
- Single Family SF
- Garden Residential GR
- Village Residential VR
- Multifamily MF
- Commercial**
- Village Commercial VC
- Highway Commercial HC
- Public Use**
- School/Park SCH/P
- Public Park P
- Nature Park NP
- Open Space**
- Natural Open Space OS
- Utility Corridors UC
- Annexation area A-__

Circulation



Village Center



NORTH 0 1000 FEET

EXHIBIT 5.2B
NORTH PEAK HILLS - LAND USE PLAN

5.3

NORTH PEAK VALLEY

Located in the center of the community, North Peak Valley will also contain a mix of residential uses similar in range to North Peak Hills to the south. Due to the types of uses, the overall size, and character, the North Peak Village Center (described in more detail in Section 9.0) will serve as a central focus for the entire North Peak community. The land use plan shown in Exhibit 5.3a and the following summary tabulation describe the proposed land uses for this Village.

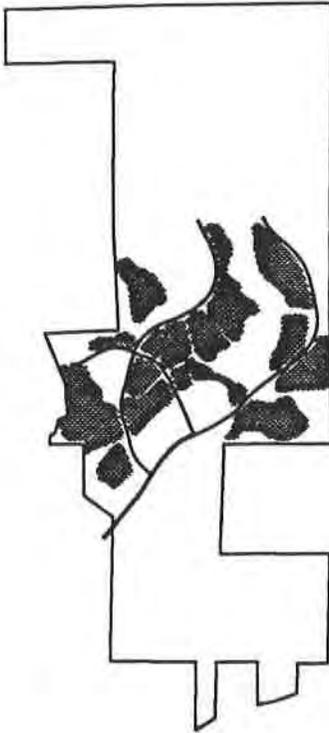
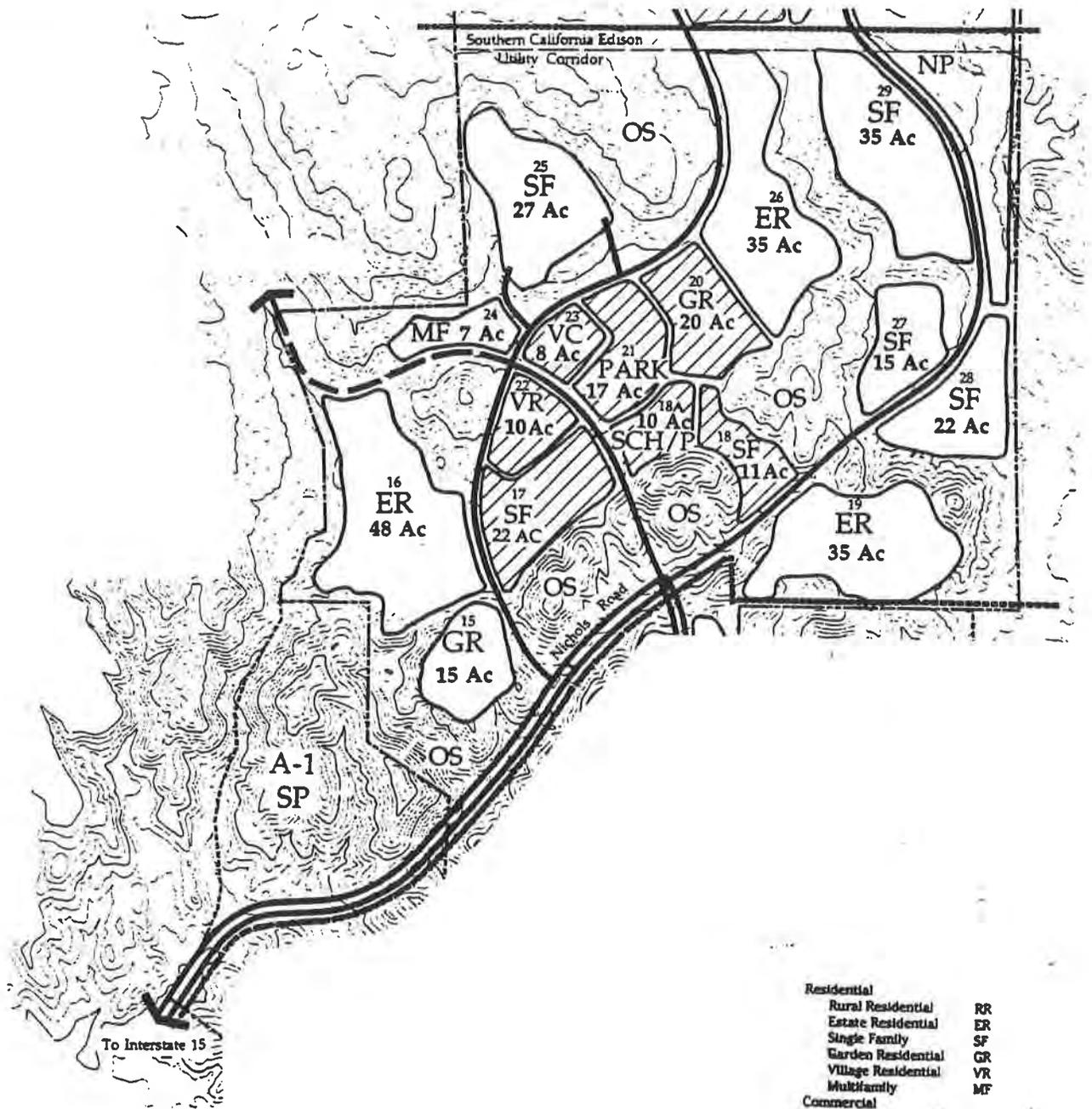


EXHIBIT 5.3A
LOCATION OF NORTH PEAK VALLEY

North Peak Valley

Land Use	Map Designation	Density Range	Average Density	Area	Dwellings/Square Feet	% of Total Dwellings
Residential						
Rural Residential	RR	0-2 du/a	1 du/a	0.0 acres	0	0%
Estate Residential	ER	2-4 du/a	3.5 du/a	118.0 acres	413	23%
Single Family	SF	4-7 du/a	6 du/a	132.0 acres	792	44%
Garden Residential	GR	7-12 du/a	9 du/a	35.0 acres	315	17%
Village Residential	VR	12-16 du/a	15 du/a	10.0 acres	150	8%
Multifamily	MF	16-24 du/a	20 du/a	7.0 acres	140	8%
Commercial						
Village Commercial	VC	.25-.50 FAR	0.25 FAR	8.0 acres	87,120	
Highway Commercial	HC	.25-.50 FAR	0.30 FAR	0.0 acres	0	
Public Use						
School/Park	SCH/P			10.0 acres		
Public Park	P			17.0 acres		
Nature Park	NP			34.0 acres		
Open Space						
Natural Open Space	OS			338.0 acres		
Utility Corridors	UC			23.9 acres		
Circulation						
Major Roads				29.0 acres		
			Totals	761.9 acres	1,810 Dwellings	87,120 SF

TABLE 5.3A
NORTH PEAK VALLEY - LAND USE TABULATION



<i>Residential</i>	
Rural Residential	RR
Estate Residential	ER
Single Family	SF
Garden Residential	GR
Village Residential	VR
Multifamily	MF
<i>Commercial</i>	
Village Commercial	VC
Highway Commercial	HC
<i>Public Use</i>	
School/Park	SCH/P
Public Park	P
Nature Park	NP
<i>Open Space</i>	
Natural Open Space	OS
Utility Corridors	UC
Annexation area	A-
<i>Circulation</i>	
	
Village Center	



EXHIBIT 5.3a
NORTH PEAK VALLEY - LAND USE PLAN

TABLE 5.3A
NORTH PEAK VALLEY - LAND USE TABULATION
5.4

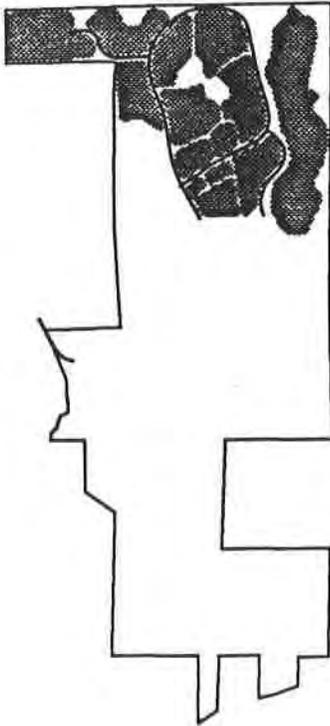


EXHIBIT 5.4A
LOCATION OF NORTH PEAK
HIGHLANDS

NORTH PEAK HIGHLANDS

The northern third of the planning area is located on a rocky plateau in the shadow of three large peaks. North Peak Highlands is the most secluded of the three Villages of North Peak and will contain the largest area devoted to single family residences of the three Villages, including 50 acres intended for residential lots with an average size of 1 acre. The Village Center, described in Section 9.9, will resemble a crossroads retail center and will serve local needs.

Exhibit 5.4b shows the proposed land use plan for North Peak Highlands. The following table (Table 5.4a) is a summary of land use distri-

North Peak Highlands

<i>Land Use</i>	<i>Map Designation</i>	<i>Density Range</i>	<i>Average Density</i>	<i>Area</i>	<i>Dwellings/Square Feet</i>	<i>% of Total Dwellings</i>
Residential						
Rural Residential	RR	0-2 du/a	1 du/a	50.0 acres	50	3%
Estate Residential	ER	2-4 du/a	3.5 du/a	143.0 acres	501	33%
Single Family	SF	4-7 du/a	6 du/a	104.0 acres	624	41%
Garden Residential	GR	7-12 du/a	9 du/a	0.0 acres	0	0%
Village Residential	VR	12-16 du/a	15 du/a	15.0 acres	225	15%
Multifamily	MF	16-24 du/a	20 du/a	7.0 acres	140	9%
Commercial						
Village Commercial	VC	.25-.50 FAR	0.25 FAR	5.0 acres	54,450	
Highway Commercial	HC	.25-.50 FAR	0.30 FAR	0.0 acres	0	
Public Use						
School/Park	SCH/P			10.0 acres		
Public Park	P			18.0 acres		
Nature Park	NP			196.0 acres		
Open Space						
Natural Open Space	OS			159.5 acres		
Utility Corridors	UC			0.0 acres		
Circulation						
Major Roads				23.0 acres		
	Totals		2.1 du/a	730.5 acres	1,540 Dwellings	54,450 SF

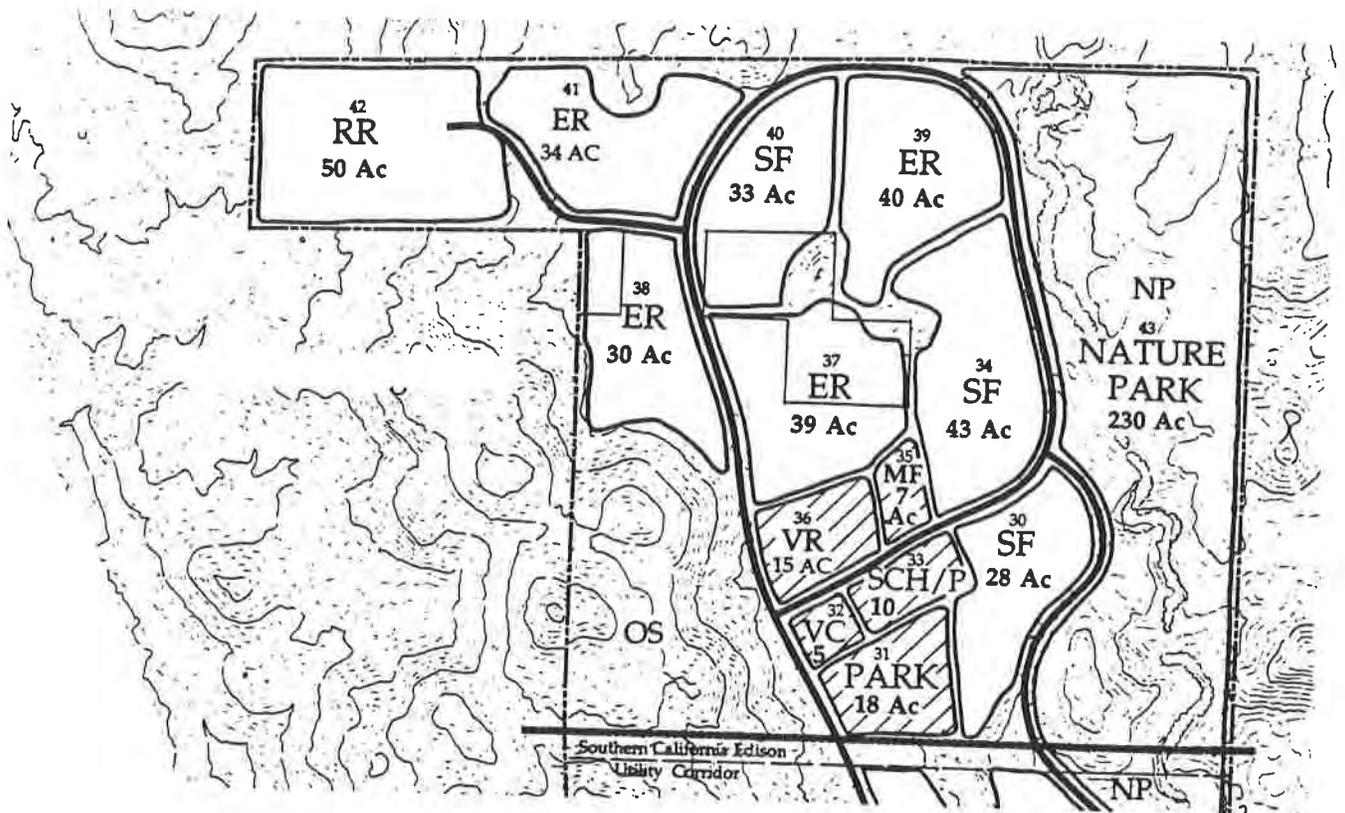
TABLE 5.4A
NORTH PEAK HIGHLANDS - LAND USE TABULATION

bution within the Village.

5.5

COMPONENTS OF THE NORTH PEAK SPECIFIC PLAN

The following sections describe in greater detail the various components of the North Peak Specific Plan. Included in these descriptions are the open space, residential, commercial, and village center components of the land use plan; the circulation plan; the public facilities plan; the grading and drainage plan; and the utility plan.



- Residential
 - Rural Residential RR
 - Estate Residential ER
 - Single Family SF
 - Garden Residential GR
 - Village Residential VR
 - Multifamily MF
- Commercial
 - Village Commercial VC
 - Highway Commercial HC
- Public Use
 - School/Park SCH/P
 - Public Park P
 - Nature Park NP
- Open Space
 - Natural Open Space OS
 - Utility Corridors UC
- Annexation area
 - A...



EXHIBIT 5.4B
NORTH PEAK HIGHLANDS - LAND USE PLAN

6.1**OPEN SPACE: UNIFYING THE COMMUNITY**

Encompassing more than half of the land within the planning area, open space is the dominant land use within the North Peak community. Over 1,000 acres of the 1,964-acre site will be devoted to open space uses. The fundamental purposes of this open space are: 1) to retain significant natural areas; and 2), to provide a strong integrating element for the overall community, tying various parts of the plan into a coherent, recognizable whole.

To achieve this goal, the open space plan strives to achieve three primary objectives. First, open space will provide visual unity and continuity throughout the community. For example, many of the important roads in North Peak have been aligned to maximize their exposure to areas that will remain permanent open space. Second, open space will provide connections between various elements of the plan: from village-to-village, neighborhood-to-neighborhood and onsite natural open space to existing natural open space in public ownership offsite. Trails within the open space will assure that the connections are physical as well as visual. Third, open space will serve as a key element of the overall identity for the community. In particular, the landscape treatment of the open space will furnish a strong backbone identity apparent to all in the community.

6.2**HIERARCHY OF OPEN SPACE**

In recognition of the important role of open space in the design of North Peak, and to make better use of the space available, the Plan establishes an open space hierarchy that serves both the functional needs of the residents and provides a strong community open space identity. Four types of open space treatment are the basis for this hierarchy: 1) natural open space, 2) public rights-of-way and manufactured slopes, 3) public parks, and 4) private recreation facilities as shown on the open space plan. They are described in the following paragraphs.

6.3**NATURAL OPEN SPACE**

The largest area of open space in the community will remain much as it is now. Nearly 650 acres—approximately one-third of the community—will be natural open space. Not only will this zone serve as a visual amenity for the community, but it will also serve as a border between adjacent land uses. Consisting primarily of rugged terrain, conservation and preservation of native plants and animals will also be a primary benefit of this open space zone. Human use of the natural open space will be accommodated with nature trails that follow the existing jeep and animal paths that criss-cross the site.

6.4**PUBLIC RIGHTS-OF-WAY AND MANUFACTURED SLOPES**

This zone includes most of the area visible from the major public roads, and is the second largest open space use in the community. Approximately 95 acres is contained within the rights-of-way of major roads. Due to varying topographic conditions along these roads, it is not possible to precisely the extent of open space contained within areas of manufactured slopes. Current grading plans show approximately 100 acres.

To help create a stronger identity for the community, this open space will be designed to accomplish three purposes. In some areas, such as along El Toro Road from Highway 74 to the center of North Peak Hills, it will serve as a large-scale gateway and will emphasize the arrival experience. In other areas, such as from the center of North Peak Hills to the center of North Peak Valley along El Toro Road, it will serve as a connector between important elements of the plan. Finally, it will provide an edge and enclosure to the major travel ways within North Peak, such as along the loop road that serves North Peak Highlands.

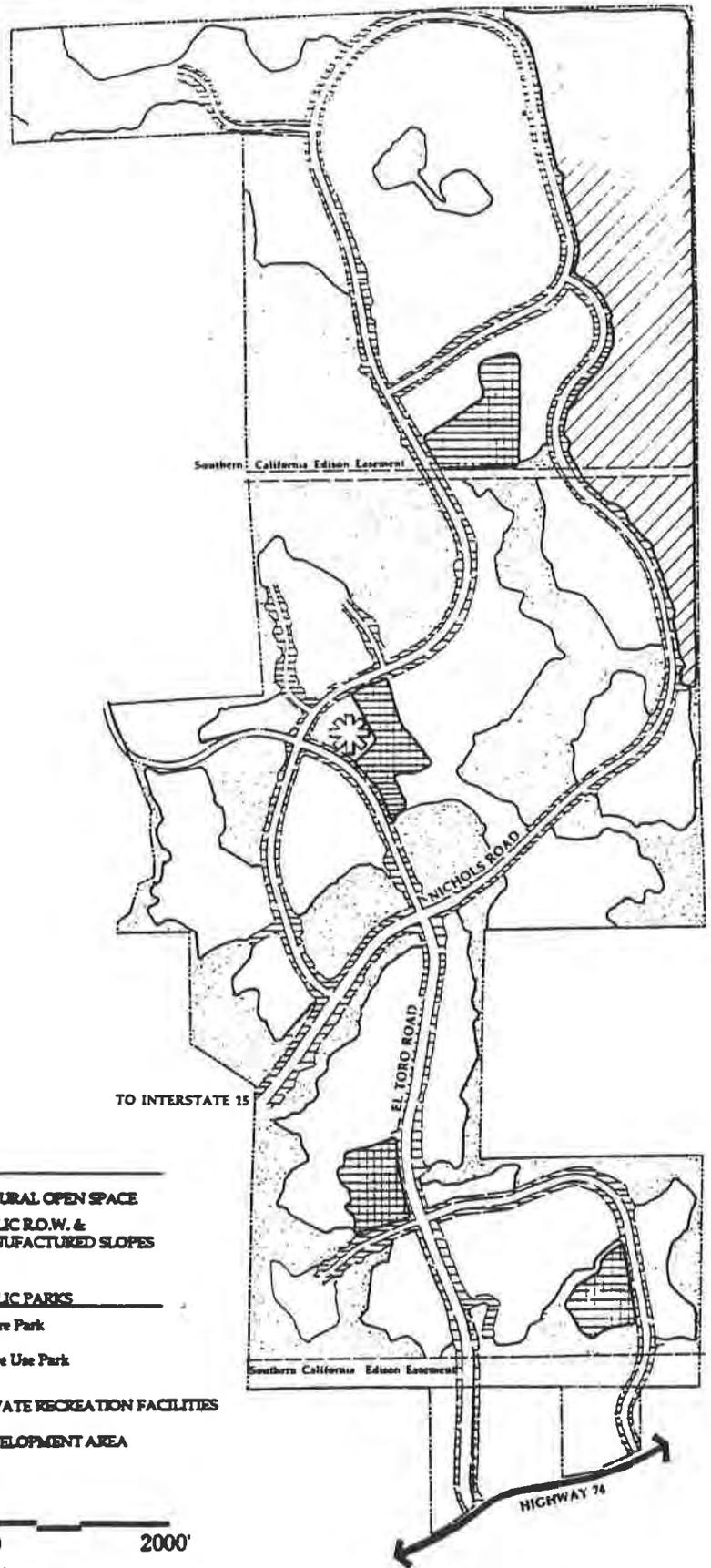
6.5**PUBLIC PARKS**

There will be two types of public parks within North Peak, nature parks and active use parks. The total acreage of these parks will be approximately 324 acres.

In North Peak Valley and North Peak Highland there is a 230-acre nature park located next to the upper loop road which helps integrate the natural landscape of the riparian area along the eastern property line with the road right-of-way open space. This nature park will be dedicated to a public entity and will contain trails, picnic facilities and an exercise course.

Active use parks will be located in each village and will contain facilities for daylight use by baseball, soccer and other field sports. Each will be located near the village center and will provide a visual, as well as recreational amenity to the village. Tot lots, picnic facilities, and rest rooms will also be provided. Parking will be provided by a combination of on-street and off-street facilities. Where possible, parking may be shared with adjacent uses. The intent is to make as much of the parks usable for active uses as possible.

Shared school/park facilities are another type of active use park within the community. Approximately 5 acres in size, they will be a part of the community's three 10 acre public school sites. Serving as the school's outdoor play area, they will provide another place for active recreation uses. Parking will be provided on-street where possible, or in off-street lots shared with the school.



LEGEND

-  NATURAL OPEN SPACE
-  PUBLIC R.O.W. & MANUFACTURED SLOPES
- PUBLIC PARKS**
-  Nature Park
-  Active Use Park
-  PRIVATE RECREATION FACILITIES
-  DEVELOPMENT AREA


NORTH 0 2000'

EXHIBIT 6.7A
NORTH PEAK OPEN SPACE MASTER PLAN

6.6

PRIVATE RECREATION FACILITIES

In addition to parks for general public use, a recreation facility intended for North Peak residents will be located at the Village Center in North Peak Valley. The facilities will provide space for specialized recreation activities such as swimming and tennis. Residents will pay a monthly fee which will maintain the facilities. Within some residential areas, there may be other private facilities intended only for the residents within that district.

6.7

OPEN SPACE PLAN

Exhibit 6.7a shows the distribution of the four components of the Open Space Plan for North Peak. The following table summarizes the distribution of the various types of open space found in North Peak.

Open Space Areas

<i>Designation</i>	<i>Area</i>
Natural Open Space	648.8 acres
Utility Corridors	63.1 acres
Nature Park	230.0 acres
Active Use Park	49.0 acres
Public ROW/Manufactured Slopes	95.0 acres*
Total	1,085.9 acres

*estimated minimum; actual acreage likely to be higher

TABLE 6.7

OPEN SPACE ACREAGE DISTRIBUTION

7.1

RESIDENTIAL LAND USES

Residential land uses are the second largest use area within North Peak and comprise approximately 40% of the total planning area, or about 821 acres. Residential land will be developed in a variety of ways, the intent being to appeal to many different kinds of people with varying needs and incomes. For example, many residential areas will contain homes intended for young families with elementary school aged children. Other areas will be intended for more mature families, or for single persons or elderly people. The mix of different types of residential dwellings will assure a heterogeneous, diverse community.

In the Village Center Overlay District, where traditional neighborhood development may occur, the mix of residential types may be quite diverse (as discussed in section 9.0), and different residential uses may exist together on the same street. The location of the Village Center Overlay District is shown on the Land Use Plan, Exhibit 5.1a. In the contemporary neighborhoods, larger areas of a single use will be common.

To accommodate the varying types of dwellings envisioned by the Plan, seven residential districts have been established. They are described in the paragraphs following the summary table below.

Residential Land Use

<i>Land Use</i>	<i>Map Designation</i>	<i>Density Range</i>	<i>Average Density</i>	<i>Area</i>	<i>Dwellings</i>	<i>% of Total Dwellings</i>
Rural Residential	RR	0-2 du/a	1 du/a	50.0 acres	50	1%
Estate Residential	ER	2-4 du/a	3.5 du/a	310.0 acres	1065	23%
Single Family	SF	4-7 du/a	6 du/a	338.0 acres	1996	43%
Garden Residential	GR	7-12 du/a	9 du/a	77.0 acres	693	15%
Village Residential	VR	12-16 du/a	15 du/a	25.0 acres	375	8%
Multifamily	MF	16-24 du/a	20 du/a	21.0 acres	420	9%
Totals		Totals		821.0 acres	4621 Dwellings	

TABLE 7.1A
NORTH PEAK RESIDENTIAL LAND USE TABULATION

7.2**RURAL RESIDENTIAL (RR)**

Rural residential is the lowest density residential type in North Peak and is intended to accommodate development of less than 2 dwelling units per acre. Occurring on a 50-acre parcel in North Peak Highlands, 50 custom lots are anticipated within this category, for an average density of 1 dwelling unit per acre. Because of the rugged terrain on this parcel, it is likely that this area will be developed in a way that retains a significant amount of the natural grade.

7.3**ESTATE RESIDENTIAL (ER)**

In this category, land may be developed at a density of 2 to 4 dwelling units per acre and the typical residential lot size will range from 8,000 to 10,000 square feet. Each of the three villages will contain some dwellings in this category. Currently, the Plan accommodates 1,085 dwellings on 310 acres, for an average density of 3.5 dwelling units per acre.

7.4**SINGLE FAMILY RESIDENTIAL (SF)**

Single family residential will be the largest category of residential use within North Peak covering approximately 338 acres and containing 1,998 dwellings. Intended primarily for detached dwellings, the minimum residential lot will be 5,000 square feet, and the average density will be 6 dwelling units per acre. The net residential density will not exceed 7 dwelling units per acre. Enclosed garages will be provided with each dwelling.

7.5**GARDEN RESIDENTIAL (GR)**

This category is intended to accommodate zero-lot-line homes, townhouses and other residential uses that range from 7 to 12 dwelling units per acre. Detached dwellings in this category may be located on lots that range in size from 3,000 to 5,000 square feet. A total of 693 dwellings on 77 acres is anticipated by the Plan for an average density of 9 dwelling units per acre. Development in this category may require the common ownership of land and recreation facilities. In that case, home owners associations will be established to maintain common facilities. Resident parking will be provided in carports or enclosed garages. Guest parking may be in surface lots or, within the Village

Center, on-street parking may be used.

7.6

VILLAGE RESIDENTIAL (VR)

Residential development in this category will primarily be attached townhouse dwellings. Parking may be provided in garages, carports or on surface lots; within the Village Center, on-street parking may be used. Development will range from 12 to 16 dwellings per acre. Currently, the Plan contains 25 acres of this type of development with a total of 375 dwellings for an average density of 15 dwellings per acre.

7.7

MULTI FAMILY RESIDENTIAL (MF)

This category is intended for residential development ranging from 16 to 24 dwellings per acre. Typically, this type of development includes stacked-flats in two and three story buildings. Both rental and individual ownership units may be included in this district, and it is possible that some units may be developed exclusively for seniors. Parking will usually be provided in surface lots located on the same parcel, though within the Village Center, on-street parking may be used. Senior residential areas will require fewer parking spaces per unit. Within North Peak, three parcels of 7.0-acres each are currently planned, one in each village, for a total of 420 dwellings on 21 acres. The average density will be 20.0 dwellings per acre.

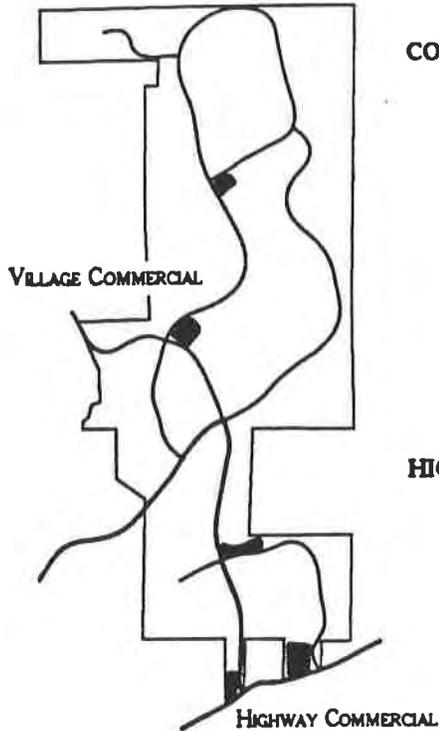


EXHIBIT 8.1A
FIVE COMMERCIAL PARCELS ARE PROPOSED IN NORTH PEAK. TWO HIGHWAY-ORIENTED PARCELS ALONG HIGHWAY 74 AND THREE VILLAGE CENTER RETAIL PARCELS.

8.1 COMMERCIAL LAND USES

Two broad categories of commercial land use are anticipated within North Peak. Adjacent to State Highway 74, commercial will be oriented to the Lake Elsinore community and others passing through the City. Within the heart of the North Peak community, however, commercial uses will be intended primarily for North Peak residents. Two land use designations are proposed by the Plan to accommodate these needs: highway commercial and village commercial.

8.2 HIGHWAY COMMERCIAL

This land use category includes two parcels in North Peak Hills that contain a total of 14 acres. Located adjacent to Highway 74, permitted uses will include highway oriented retail development, business park, or other uses that would benefit from exposure to this major thoroughfare. These two sites will be developed with a floor area ratio (FAR) of 0.30, and the total square footage permitted will be approximately 183,000 square feet.

8.3 VILLAGE COMMERCIAL

The village commercial district will include retail and office uses consistent with the needs of local village residents. Markets, convenience stores, restaurants, service establishments, and small professional offices are the most likely users of this district. Private recreation facilities may also be included in this district. Three parcels, one in each village, for a total of 18 acres are included within this category. The floor area ratio (FAR) permitted in this district will be 0.25, which will yield a total of approximately 196,000 square feet.

The following table summarizes the commercial land use program for North Peak.

Commercial Land Use Summary

<i>Land Use</i>	<i>Map Designation</i>	<i>Average Density</i>	<i>Area</i>	<i>Square Feet</i>
Village Commercial	VC	0.25 FAR	18.0 acres	196,020
Highway Commercial	HC	0.30 FAR	14.0 acres	182,952
Total			32.0 acres	378,972

TABLE 8.3A
COMMERCIAL LAND USE SUMMARY

9.1

THE VILLAGE CENTER OVERLAY DISTRICT

North Peak's three village centers will be important elements in establishing the overall character and quality of the North Peak community. The purpose of this section of the specific plan is to describe the basic goals that should guide development of the centers.

The sketches and photographs of historic Southern California village centers in this section and elsewhere in the text are intended to serve as examples of the kind of character and quality intended for the village centers of North Peak.

9.2

CENTRAL FOCUS OF THE VILLAGES AND THE COMMUNITY

Each of the three proposed village centers will be a focus of social activity within the village. Each will be centrally located and easily accessible to everyone in the village. They will be a focus for local commerce and will become a village landmark. Uses located in the Centers will attract people for a variety of purposes. They will be places where one can shop, meet friends, work, relax at a restaurant, or visit a park. The visual appeal of the center will also be important and attention will be given to making the village centers attractive, pedestrian-friendly environments.



EXHIBIT 9.2A
PEDESTRIAN-ORIENTED VILLAGE CENTER

9.3

TRADITIONAL SOUTHERN CALIFORNIA CHARACTER

Because of their central location and important role in establishing the identity of the community, the village centers will strongly represent North Peak's traditional southern California character. The intent is to convey a feeling the centers might have been created between 1900 and 1930, and resemble places associated with Southern California's past. Places that have a character similar to what is proposed for the North Peak village centers include downtown Ojai, the Malaga Cove district at Palos Verdes Estates, Claremont and the town center at Rancho Santa Fe. The Table in Section 4.5 contains a comparison of traditional and contemporary neighborhoods.

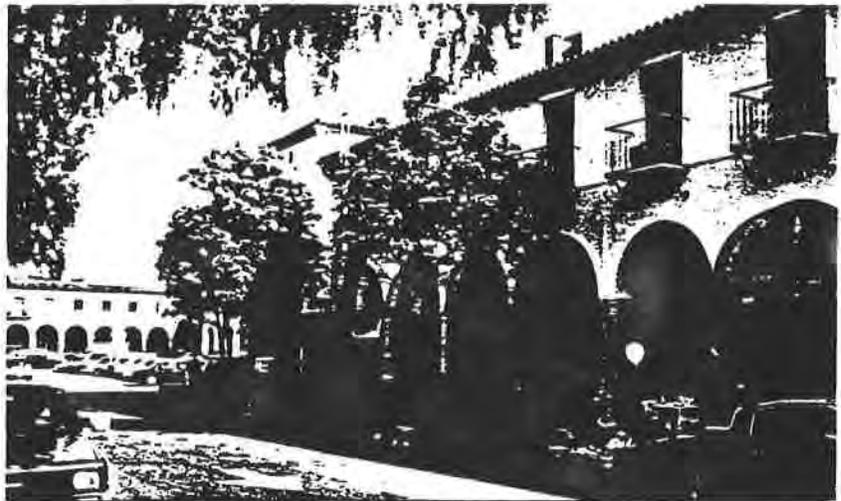


EXHIBIT 9.3A
MALAGA COVE SHOPPING CENTER, PALOS VERDES, CONSTRUCTED IN 1925, HAS A
TRADITIONAL SOUTHERN CALIFORNIA CHARACTER.



EXHIBIT 9.3B
EARLY CALIFORNIA TOWN, CIRCA 1900

9.4

STRONG CONNECTIONS TO THE REST OF THE COMMUNITY

In order to assure their importance to the community, the village centers will be well-connected to the rest of the community. For example, automobile connections should not only be made from major roads, but, whenever possible, should also be made from local streets in order to make the centers more accessible and make them function as part of the community. Direct pedestrian connections are also important, especially those that go into the heart of the centers from the surrounding neighborhoods. Walking to the center will be a pleasant and safe experience.



EXHIBIT 9.4A
EARLY CALIFORNIA TOWN CHARACTER

9.5

A COMPLETE NEIGHBORHOOD

The village center is intended to be a complete neighborhood and will include a variety of inter-related uses. Traditional retail development will serve as the basis for the village center development, but they may also include residential uses, civic uses, open space, and recreational areas. The exact mix of uses will vary from one center to another, and not all centers will contain all possible uses. Rather, each center will be tailored to the needs of the individual village. Sections 9.8 to 9.10 describe the intent for each center in more detail.

9.6

FLEXIBILITY IN THE VILLAGE CENTER DISTRICT

In order to achieve the character envisioned for the centers, it is important that there be considerable flexibility in how each of the centers is

planned and designed. The reason for this is that the village centers should be designed as a unit and the precise physical relationship between the various elements of the center can't be designed until a more precise program for each center is established. To accommodate this need for flexibility, a detailed site plan of the village centers will be prepared when the actual program of each center is defined, when tentative tract maps are prepared. The role of the specific plan, therefore, is to identify the amount and general location of various uses so that major roads and other infrastructure may be properly sized. Part of the intent of maintaining flexibility in the design of the village centers is to take advantage of opportunities which will arise as development of the planned community proceeds, and which cannot be predicted now. The village center overlay district, then, is one in which the precise uses and their locations may be adjusted in order to improve the quality and character of the center, but in which the overall intensity of development will be controlled by the specific plan.

9.7

SPECIAL DEVELOPMENT STANDARDS

Special design guidelines for the village centers will help assure that the centers are developed in the way envisioned in the Plan. Of particular importance are guidelines that describe how an appropriate scale can be achieved, how important connections can be made, the architectural character of the development, and the design and treatment of streets and parking lots. These standards are described in section 16.0 and 26.0.



EXHIBIT 9.7A

EUCLID AVENUE, ONTARIO, CALIFORNIA, CIRCA 1887.

BUILDINGS FORM GATEWAY TO TREE-LINED BOULEVARD

9.8

NORTH PEAK HILLS - A GATEWAY CENTER

Located in the middle of the village on El Toro Road, the North Peak Hills Village center will serve as an entry gateway into the entire North Peak community. Through a unified use of architecture and landscape, this village center will become identified as the formal entry and arrival place for North Peak. As the center of the village, it will contain a 14-acre public park and high density residential development, in addition to approximately 5 acres of retail uses. Exhibit 9.8b is a preliminary, diagrammatic plan for this center that shows how the various elements that make up this center might be arranged to achieve the purposes envisioned for this center.

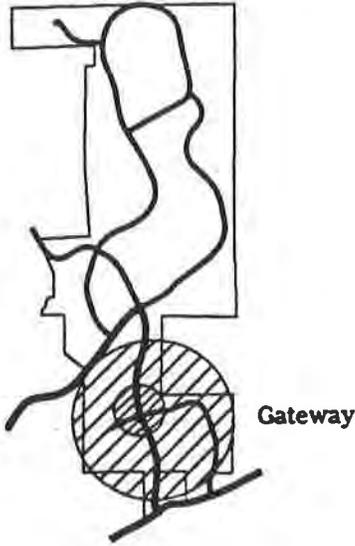


EXHIBIT 9.8A
NORTH PEAK HILLS VILLAGE
CENTER

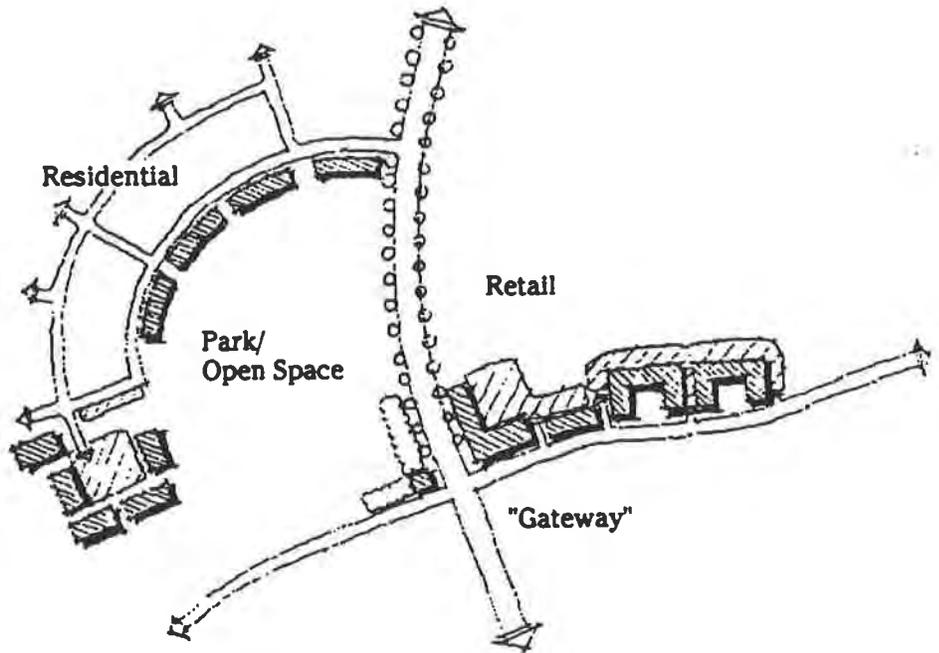


EXHIBIT 9.8b
NORTH PEAK HILLS VILLAGE CENTER CONCEPT

9.9

NORTH PEAK HIGHLANDS - THE CROSSROADS CENTER

The village center in North Peak Highlands will be similar in size to the center in North Peak Hills, but because of its location deep within the community, is intended to be more like a traditional small-town crossroads. In addition to retail development, there will be an 18-acre public park and medium density residential development within the center. As with the other village centers, provision can be made for a church site, day-care center, or other special use within the center, if such an opportunity arises. Exhibit 9.9b shows a preliminary conceptual plan for this center.

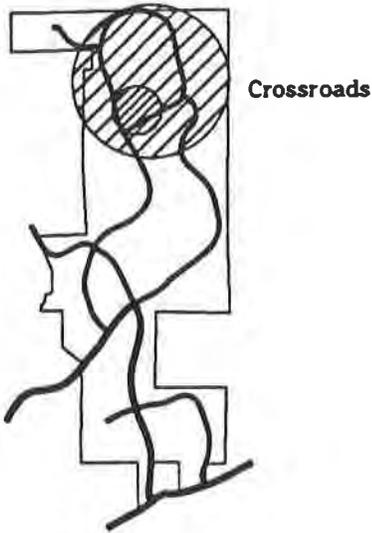


EXHIBIT 9.9A
NORTH PEAK HIGHLANDS VILLAGE
CENTER

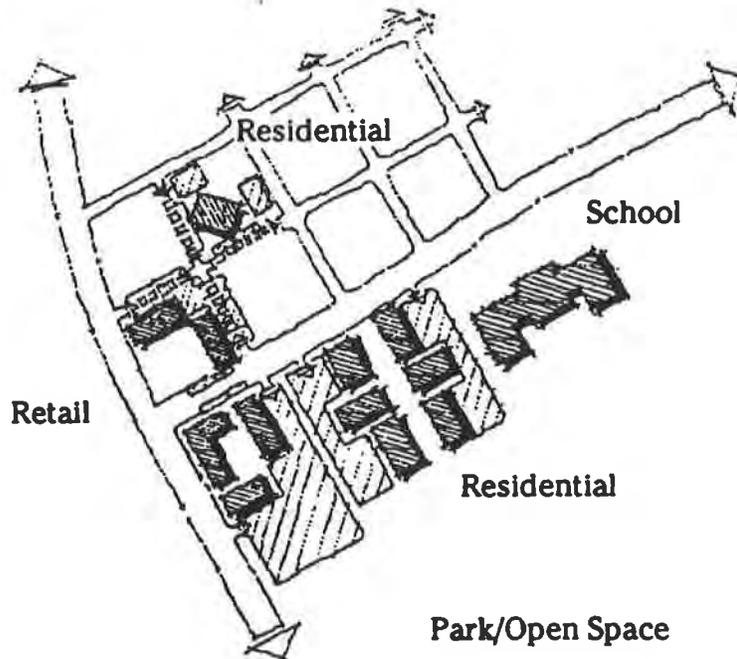


EXHIBIT 9.9B
NORTH PEAK HIGHLANDS VILLAGE CENTER CONCEPT

9.10

NORTH PEAK VALLEY - THE TOWN CENTER

The village center in North Peak Valley will be the largest of the three village centers and will serve as a town center for the entire North Peak community. In addition to having the greatest amount of retail development of all the centers, it will contain a private recreation center of approximately 5 acres intended for all North Peak residents, a 17-acre public park, medium and high density residential development, an elementary school site, and potential locations for other public uses. Exhibits 9.10b and 9.10c show how this village center might be implemented. These plans, however, represent only one of several possible approaches that might be taken to the design of this center, and they are subject to modification.

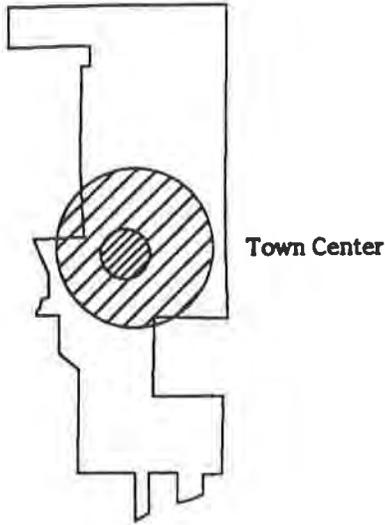


EXHIBIT 9.10A
NORTH PEAK VALLEY VILLAGE
CENTER

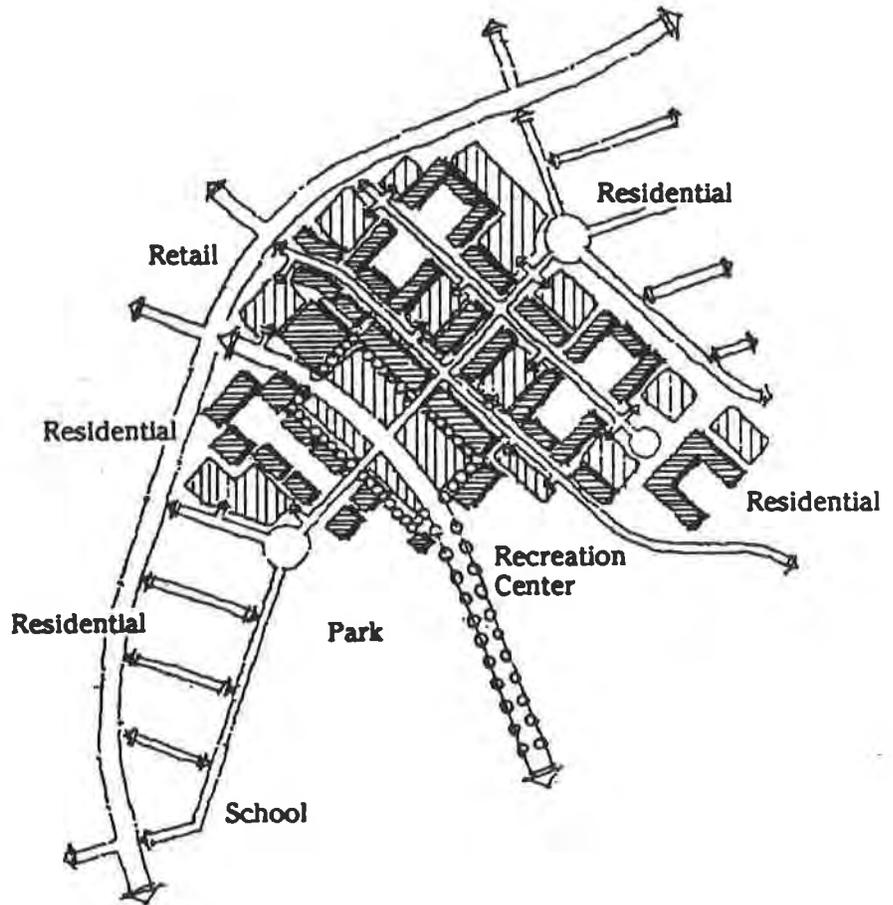


EXHIBIT 9.10b
NORTH PEAK VALLEY VILLAGE CENTER CONCEPT

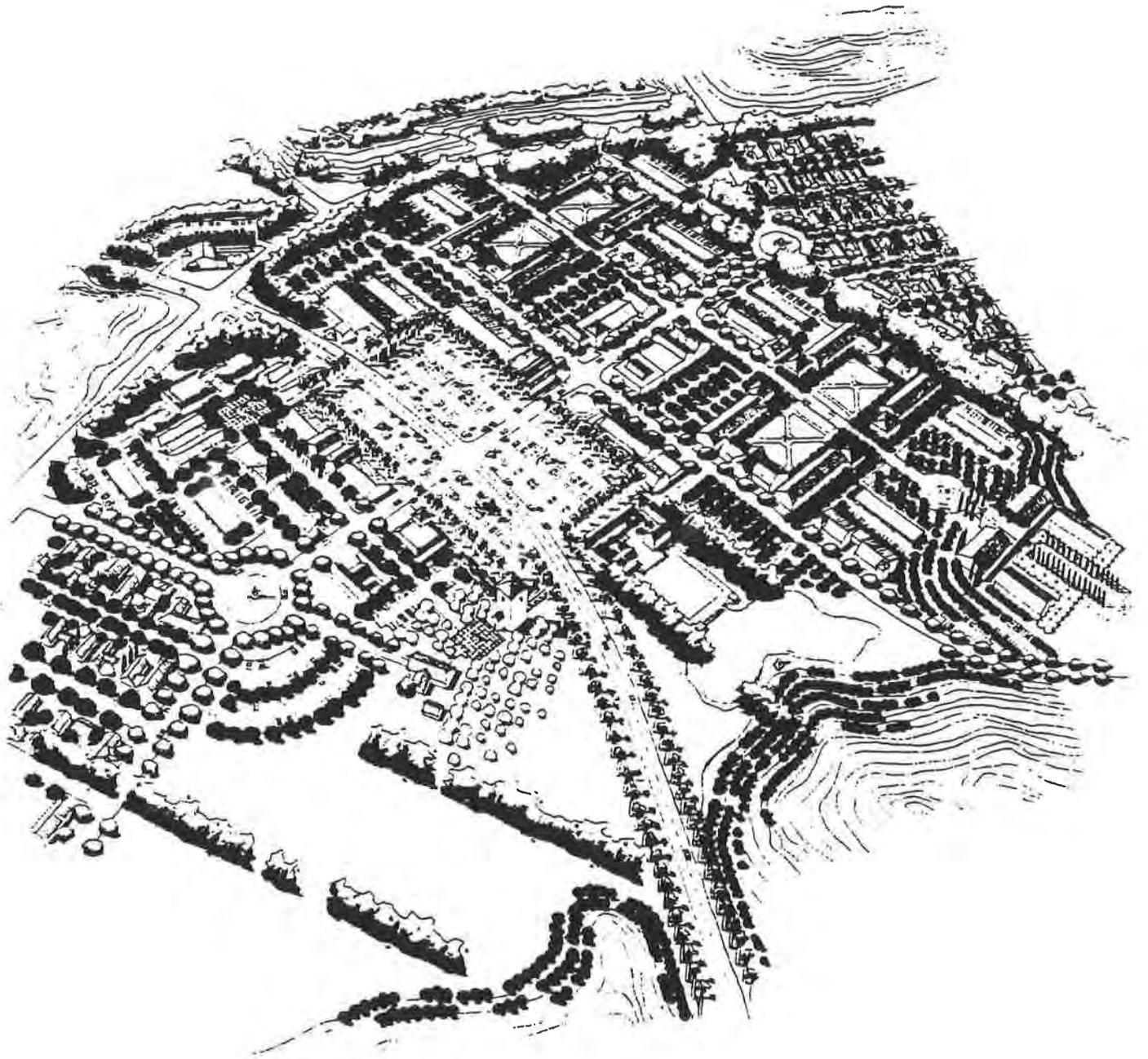


EXHIBIT 9.10c
NORTH PEAK VALLEY VILLAGE CENTER

10.1**CIRCULATION PLAN**

The circulation system for North Peak will consist of roads and streets of various sizes, depending on their location and function, as well as trails intended for pedestrians, equestrians, and bicycles. The overall intent is to accommodate many forms of movement within the community.

10.2**VEHICULAR SYSTEM**

The vehicular system will be designed to provide convenient automobile access throughout the community. The system will be hierarchical with five types of roads: 4-lane arterial, 4-lane secondary, 2-lane collector, local public street, and local private street. In keeping with the overall community goal of evoking traditional California community development, the paved travel lanes will be built at the minimum width consistent with safe roadway design. In addition, special consideration will be given to streets and alley design in the village centers in order to promote the design objectives of this overlay district.

4-LANE ARTERIAL ROADS

The 4-lane arterial road designation is based on the Major Road designation of the Lake Elsinore General Plan. This road consists of a minimum of 2, 13-foot travel lanes for each direction of travel separated by a landscaped median that may range in width from 14 to 24 feet. Where necessary, right or left turn lanes may be added and the median reduced in width to 4 feet. Paved shoulder lanes will not be included within the paved travel way, but adequate space will be provided within the cross section to add them at a later date if they become necessary. The total right-of-way will be a minimum of 100 feet, but may be wider to accommodate additional travel or turning lanes, areas of more extensive landscape or manufactured slopes. No parking will be allowed on 4-lane arterials and the minimum parkway width will be 7 feet. A minimum 4-foot sidewalk will be provided on each side of the road. Two roads in North Peak will be built to this standard: El Toro Road from Highway 74 to the intersection with the extension of Nichols Road, and the extension Nichols Road to its intersection with El Toro Road. The following cross-section illustrates the typical conditions.

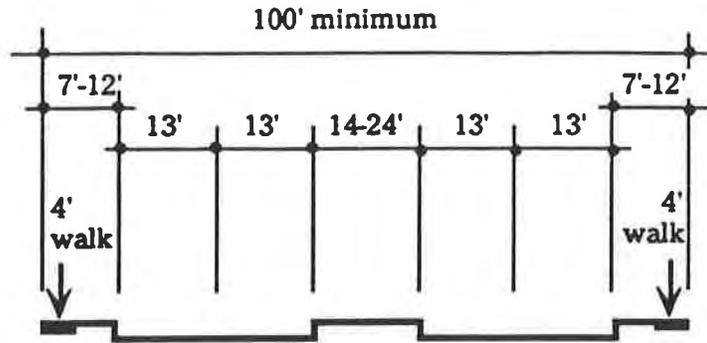


EXHIBIT 10.2A
4-LANE ARTERIAL ROAD CROSS SECTION

4-LANE SECONDARY ROAD

The 4-lane secondary will be used in limited areas as a transition between the 4-lane arterial and 2-lane collector. This road is based on the secondary road standard of the Lake Elsinore General Plan. At this time, the only area served by this road type will be El Toro Road between the intersection with Nichols and the village center in North Peak Valley. The right-of-way for this road type will contain 4, 13-foot travel lanes. The right-of-way will be a minimum of 80 feet wide but may be more depending on the adjacent landscape or the need for turning lanes. No parking will be allowed on 4-lane secondary roads and the parkway strip will be a minimum of 8 feet (including the sidewalk). A minimum 4-foot sidewalk will be provided on each side of the road. Paved shoulder lanes will not be included within the paved travel way, but adequate space will be provided within the cross section to add them at a later date if they become necessary. The following cross-section illustrates the basic condition.

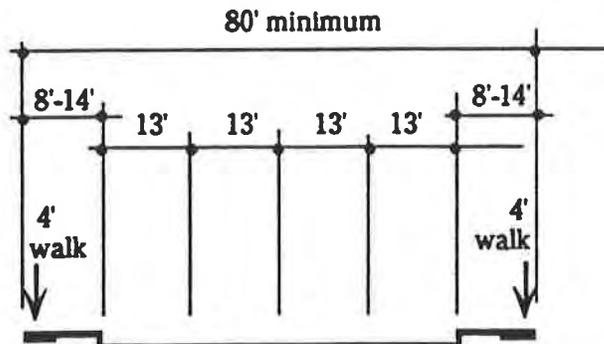


EXHIBIT 10.2B
4-LANE SECONDARY ROAD CROSS SECTION

2-LANE COLLECTOR

The primary purpose of this type of road is to collect traffic from individual neighborhoods and carry it to the arterials and secondary roads. It is based on the collector road standard of the Lake Elsinore General Plan. This road type will occur in all three villages and is shown on the land use and circulation plans. This road has two travel lanes within a minimum 36-foot paved travel way. Parking is permitted on the 2-lane collector. The total right-of-way will be at least 60 feet wide but may be more to accommodate turn lanes, or other special conditions. A minimum 4' walk will be provided on each side of the street. The following cross-section shows the typical condition.

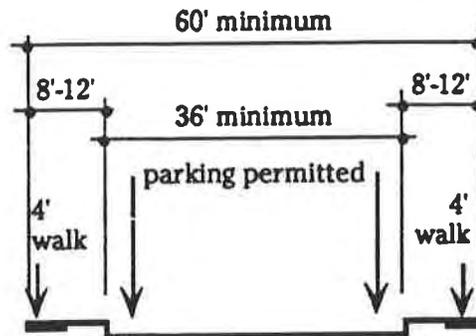


EXHIBIT 10.2c
2-LANE COLLECTOR ROAD CROSS SECTION

LOCAL PUBLIC STREETS

Local public streets are not shown on the land use or circulation plans, but will comprise the largest number of streets in North Peak. They are intended to serve local neighborhoods and will be publicly owned and maintained. The width of the street and the right-of-way will depend on a variety of factors including the need for on-street parking, the number of dwellings served by the street, and the degree to which the street serves through-traffic. The following cross-sections illustrate the various conditions that might be found on these public streets.

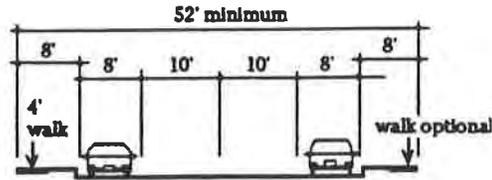


EXHIBIT 10.2D
LOCAL PUBLIC STREET, PARKING BOTH SIDES

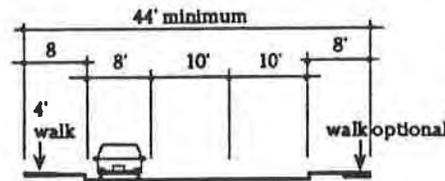


EXHIBIT 10.2E
LOCAL PUBLIC STREET, PARKING ONE SIDE

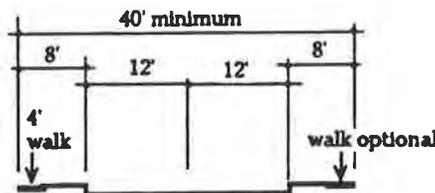


EXHIBIT 10.2F
LOCAL PUBLIC STREET, NO PARKING

PRIVATE STREETS

Private streets will be permitted in North Peak and may be built to a variety of standards depending on the specific need. Any street built to public street standards may become a private street, providing adequate provision is made for long-term maintenance. Private streets designed to special standards will be evaluated at the time they are proposed. Among the standards that might be considered for private streets in North Peak

are: 1) special curb treatments, including rolled curbs, battered curbs and no curbs; 2) reduced pavement widths for streets and drives that serve less than 12 dwellings; and 3) special paving treatments, including stamped concrete or unit pavers. Maintenance of private streets will be by a home owners association or other entity set up for that purpose. Special emergency access routes may be necessary where private streets are not readily available to all vehicles. Specific solutions will be reviewed and approved during tentative map review.

ALLEYS

Alleys will be permitted within North Peak and may be either public or privately owned and maintained depending on the specific situation. Alleys will be paved with an all-weather surface such as concrete or asphalt. The minimum width of an alley serving six dwellings or less may be 12 feet; alleys serving more than six dwellings will be a minimum of 20 feet wide. The minimum unobstructed distance behind a garage will be 24 feet. The following cross section illustrates these conditions.

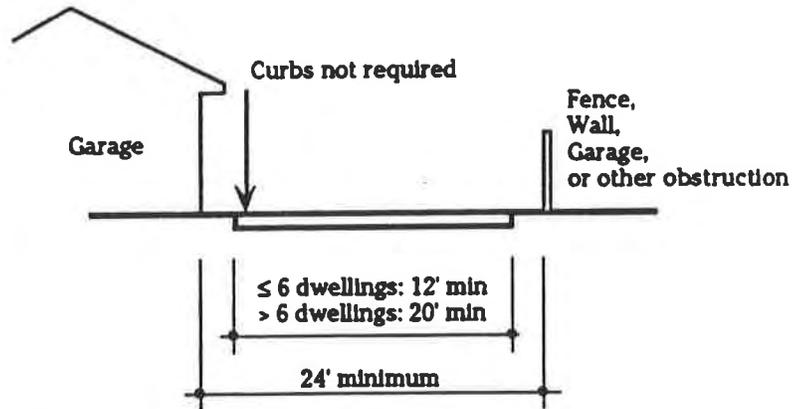


EXHIBIT 10.2c
ALLEYS, PUBLIC OR PRIVATE

10.3

BICYCLE CIRCULATION

Generally, bicycles will be accommodated on 2 lane collector roads or within local public streets. An exception to this will be along the new alignment of El Toro Road, where an 8-foot combination pedestrian/ bicycle path will be located. Because of the rugged terrain and separation of the community from regional attractions for bicyclists, separated, dedicated bicycle paths are unnecessary within North Peak. The many existing jeep trails within the natural open space areas may

eventually become an attraction for mountain bicyclists.

10.4

PEDESTRIAN CIRCULATION

Pedestrians will be accommodated within North Peak in a variety of ways that includes sidewalks, trails, and special intersection treatments in the village centers.

SIDEWALKS

All public roads will have a 4-foot pedestrian walk on at least one side; most will have sidewalks on both sides. To promote a sense of safety and encourage walking, separation of sidewalks from the automobile travel lanes by a landscaped parkway will be encouraged. Neighborhoods that have private streets may have alternative pedestrian circulation systems, if warranted by the neighborhood design.

PEDESTRIAN TRAILS

The rugged beauty of the natural open space to be retained in North Peak will be accessible to pedestrians by a system of trails that are based on the existing pattern of jeep trails that currently exist on site. These trails will require a minimum of maintenance and are intended to reinforce the rustic character of the community.

10.5

REGIONAL TRAILS

The Riverside County Parks department has established a regional trail network in the Lake Elsinore area. The trails are intended to serve equestrian, as well as other trail users, and will connect North Peak to other parts of the County. North Peak will accommodate the County's plan by setting aside 20-foot easements for these regional trails.

10.6

CIRCULATION MASTER PLAN

Exhibit 10.6a shows the principal components of the circulation master plan for North Peak.

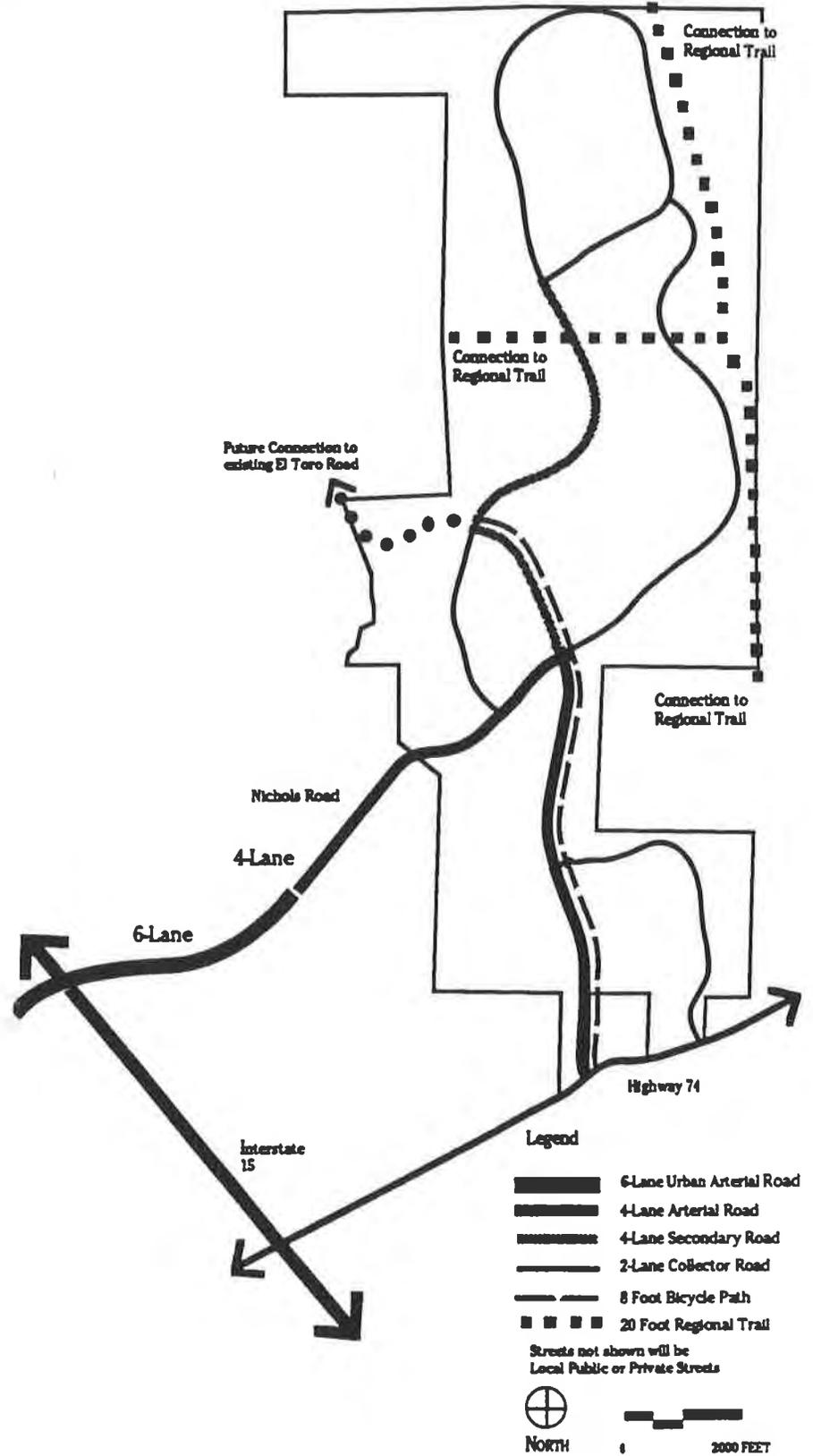


EXHIBIT 10.5A
CIRCULATION MASTER PLAN

11.1

PUBLIC FACILITIES AND CIVIC USES

Public facilities and civic uses include parks, schools, fire protection, and police services, as well as churches, libraries, and other public gathering spaces. It is the intent of the Plan to provide adequate services in these categories, and the following paragraphs describe how these needs will be accommodated.

11.2

PARKS

The City of Lake Elsinore requires dedication of park land or payment of fees in lieu of park dedication for all residential development within the City. The City has established a standard of 5.0 acres of park land for each 1,000 residents. In order to calculate the amount of required park land, the City has also established factors which estimate dwelling occupancy based on the type of dwelling. Single family homes, such as those in North Peak's rural residential, estate residential, and single family residential districts, have been assigned a factor of 3.6 persons per dwelling unit. Attached dwellings, such as those in the garden residential district have been assigned a factor of 3.1 persons per dwelling, and apartment dwellings, such as those in the village residential and multifamily districts have been assigned a factor of 1.8 persons per dwelling. Based on these factors and the number of dwellings projected for North Peak, the total park requirement will be 74.3 acres. The following table summarizes these calculations:

<i>Residential District</i>	<i>Total Dwellings</i>	<i>Occupancy Factor</i>	<i>Estimated Population</i>
Rural Residential	50	3.6	180
Estate Residential	1085	3.6	3,906
Single Family	1998	3.6	7,193
Garden Residential	693	3.1	2,148
Village Residential	375	1.8	675
Multifamily	420	1.8	756
Totals	4621		14,858 persons
Park land required at 5.0-acres per 1000 persons:			74.3 acres

TABLE 11.2A
PUBLIC FACILITIES - PARK LAND REQUIREMENTS

11.0 PUBLIC FACILITIES PLAN

The North Peak Plan will satisfy this requirement by providing 49.0 acres of active-use park in the form of general use parks near each village center, 15.0 acres of active-use park associated with three elementary schools, and 230.0 acres of passive-use nature park in the two northern villages. The nature park is proposed to receive park credit for 20% of the area provided, or a total of 52.0 acres. The following table summarizes the park land provided in North Peak.

<i>Park Type</i>	<i>Size</i>
Active Use Parks	49.0 Acres
Parks adjacent to schools	15.0 Acres
Nature Parks, at 20% credit	46.0 Acres
Total Park Provided	110.0 Acres
	Acres
Total Park Required	74.3 Acres
Excess Park Provided	35.7 Acres

TABLE 11.2B
PUBLIC FACILITIES - PARK LAND PROVIDED

11.3

SCHOOLS

The North Peak planning area is located within three school districts. The southern third of the site is served by the Lake Elsinore Unified School District (LEUSD), which provides both elementary and high school education to district students. Currently, the northern two thirds of the site is located in the Perris School District, which provides elementary and middle school education, and the Perris Union High School District.

The North Peak Plan proposes three elementary schools within the community, one in each of the three villages. Each school site will be 10 acres. Within the 10 acres will be a 5-acre public park intended for joint use with the school.

11.4**FIRE PROTECTION**

Fire protection will be provided by the Riverside County Fire Department. Currently, their standard for provision of adequate fire protection is based on one fire protection substation for approximately 8,500 dwellings. Based on the fire department's criteria, North Peak alone will not create enough demand for another station. However, it is possible that a future station may be located within the North Peak planning area. Two locations are being considered: in North Peak Hills near Highway 74 or in North Peak Valley in the village center.

11.5**POLICE PROTECTION**

Police Protection for the North Peak community will be provided from the existing Lake Elsinore Police Station at 117 South Langstaff Street in Lake Elsinore. Response time is 6 minutes for life threatening emergencies and 26 minutes for non-emergencies. The Langstaff Station supports 60 sworn officers.

11.6**OTHER PUBLIC FACILITIES**

Other public facilities include churches, public meeting halls, and similar uses. Since demand for these uses is difficult to establish, they will be accommodated as the need for them arises, as plans for each village are developed in greater detail.

11.7**PUBLIC FACILITIES MASTER PLAN**

Exhibit 11.7a shows the location of public facilities within North Peak.

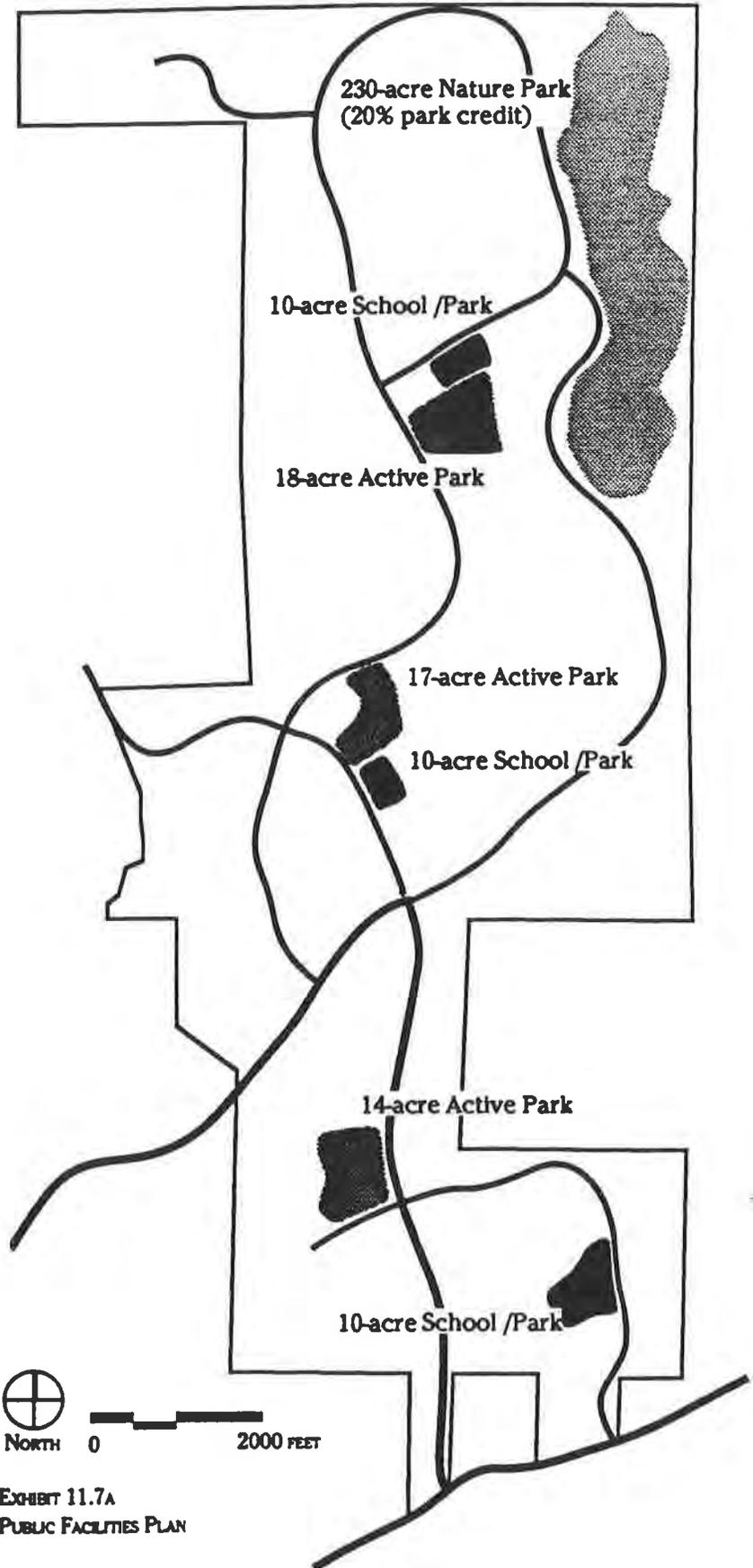


EXHIBIT 11.7A
PUBLIC FACILITIES PLAN

12.1

GRADING MASTER PLAN

In order to prepare the North Peak site for development, large areas will be graded. The conceptual grading plan in Exhibit 12.1a shows the proposed condition of the site following the grading operations.

PURPOSE

There are three main purposes to be served by the grading that will occur at North Peak. First, grading will create buildable areas from terrain which is, in many cases, too steep to safely build on or provide access to. Second, grading will stabilize areas that are currently unstable or hazardous because of landslides or, in the northern part of the site, the potential for rock slides. Finally, grading will serve aesthetic purposes, especially in parks and other public use areas where the manipulation of landform can benefit the overall visual impact. For example, special attention has been given to the visibility of the project from Highway 74; significant areas of existing landforms will be retained to enhance the appearance of the community from this highway.

GOALS OF THE GRADING PLAN

The main goals of the grading plan are to minimize cut and fill while maximizing buildable area, and to imitate natural conditions whenever feasible to enhance the appearance of manufactured slopes. Another important goal is to retain natural slopes in highly visible areas. For example, several significant landforms that are visible from Highway 74 will remain essentially intact and will visually shield the community from this important visual corridor. Minimizing cut and fill will also help assure the retention of significant areas of undisturbed open space.

In visually important areas, such as the parks that occur in each village center, special attention will be given to transition areas between existing and man-made grades to sustain the illusion that the grades are natural. Also, building mass and planting will be used to conceal man made slopes while enhancing the community. For example, placing buildings in front of densely planted slopes conceals the slopes while providing a dramatic living backdrop for the architecture.

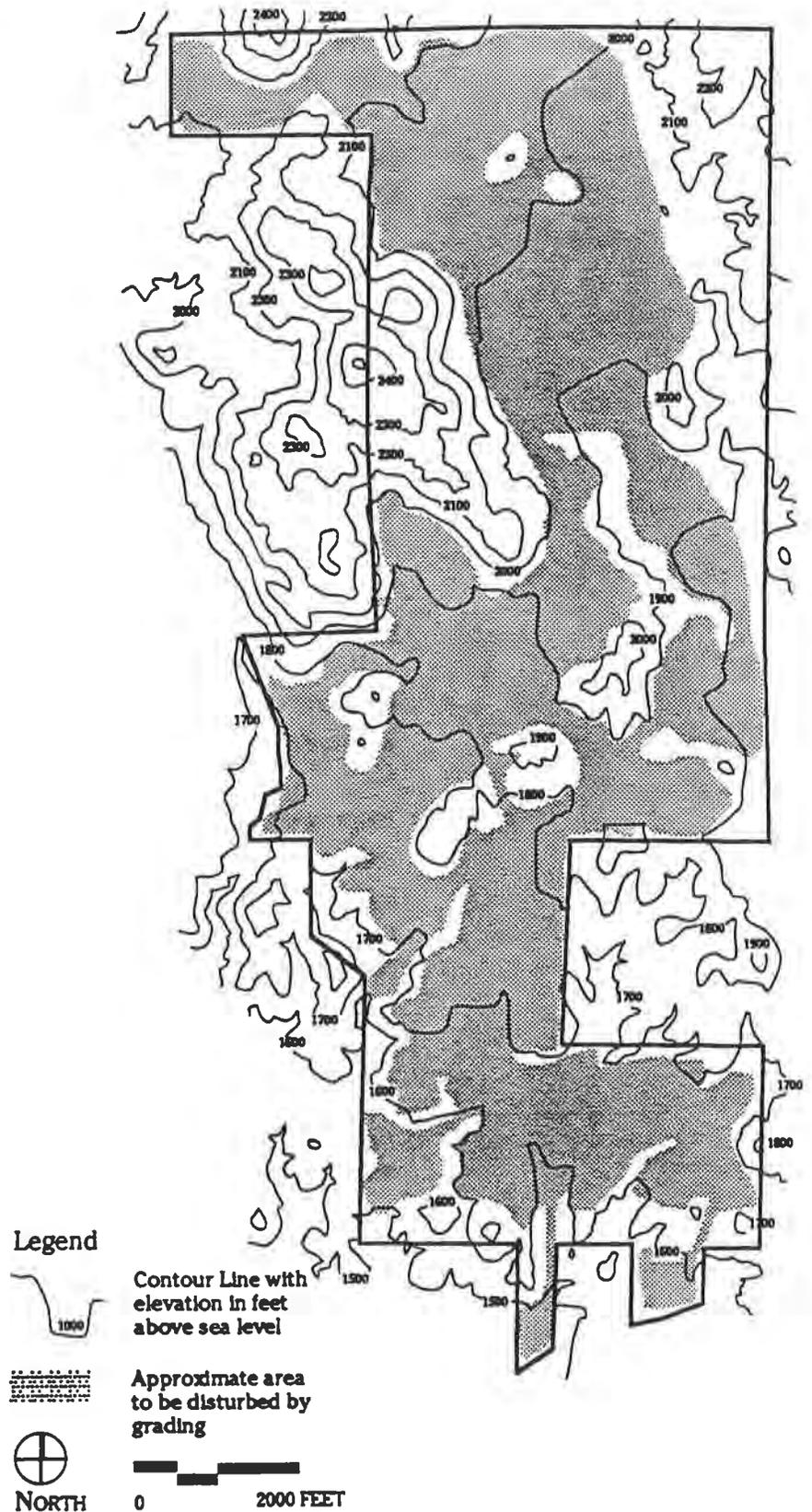


EXHIBIT 12.1A
CONCEPTUAL GRADING PLAN

12.2

GENERAL GRADING GUIDELINES

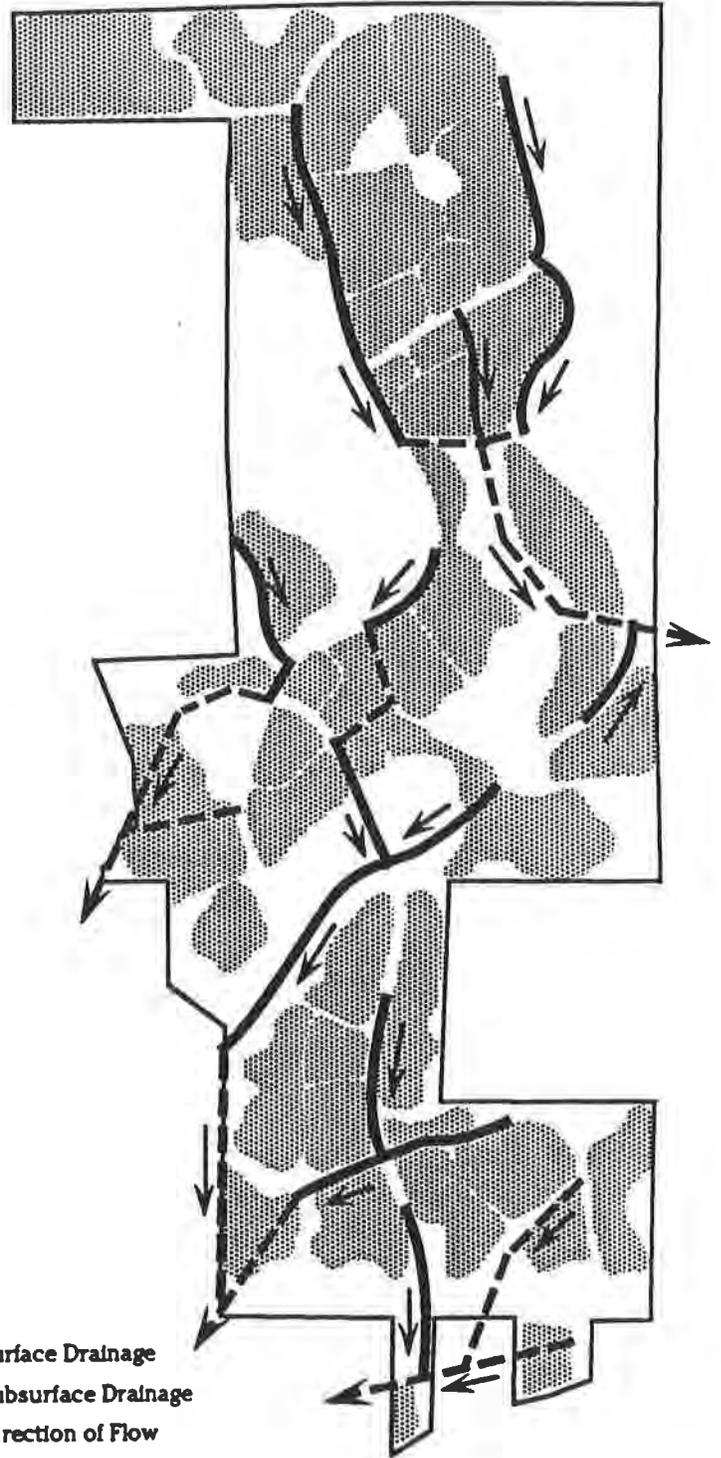
The following guidelines pertain to the grading of North Peak.

- Where two cut or fill slopes intersect, the ends of each should be horizontally rounded and blended with a minimum radius of twenty five feet.
- Where any cut or fill slope meets the natural grade, the ends of each slope should be vertically and/or horizontally rounded and blended with the natural contours so as to present a natural slope appearance.
- Where any cut or fill slope exceeds 200 feet in horizontal length, the slope should be curved in an undulating fashion that resembles the natural terrain.
- Landscape, including trees, shrubs and ground cover, will be installed for all cut or fill slopes in excess of four feet in height.
- Landscape, including trees, shrubs and ground cover, and irrigation will be installed on all slopes in excess of ten feet in height.
- The planting and irrigation of all slopes in excess of fifteen feet in height will be commenced in accordance with the approved plans within 60 days of the completion of the rough grading operations.
- The planting and irrigation of all slopes less than fifteen feet in height will be commenced in accordance with the approved plans within 60 days of the completion of the finish grading operations.
- The developer will be responsible for the maintenance and upkeep of all slope plantings and irrigation systems until the individual properties and dwellings are occupied or until a homeowner's association accepts the responsibility to maintain the landscaping in common areas.
- Unless otherwise indicated on the approved grading plan, drainage facilities and terracing will conform to the provisions of this section for cut or fill slopes steeper than 3 horizontal to 1 vertical.12.2

12.3

DRAINAGE MASTER PLAN

The purpose of the drainage plan is to describe the facilities that will be necessary to collect storm water from North Peak and safely convey it to off-site facilities. Exhibit 12.2a shows the drainage plan for North Peak.



Legend

- Surface Drainage
- - - Subsurface Drainage
- ← Direction of Flow



EXHIBIT 12.2A
CONCEPTUAL DRAINAGE PLAN

Basically, the Plan provides for facilities that protect North Peak in the 100-year storm. The 100-year storm is defined as the storm that has a 1% statistical chance of occurring in any particular year. Whenever possible, storm drain facilities will be located on the surface; only when no other choice is feasible will subsurface facilities be considered. Factors that are likely to affect this decision include topographic conditions and the density of development to be served by the proposed facilities, as well as cost. Generally, underground storm drains will be sized to convey the 10-year storm; peak flows will be on the surface within road rights-of-way.

12.4

TERRACES

- Terraces at least 6 feet in width will be established at not more than 30-foot vertical intervals on all cut or fill slopes to control surface drainage and debris except that where only one terrace is required, it will be at midheight.
- For cut or fill slopes greater than 60 feet, and up to 120 feet in vertical height, one terrace at approximately midheight will be 12 feet in width.
- Terrace widths and spacing for cut and fill slopes greater than 120 feet in height will be designed by the civil engineer and approved by the chief building official.
- Suitable access will be provided to permit proper cleaning and maintenance.
- Swales or ditches on terraces will have minimum gradient of 5 percent and must be paved with reinforced concrete not less than 3 inches in thickness or an approved equal paving. They will have minimum depth at the deepest point of 1 foot and a minimum paved width of 5 feet.
- A single run of swale or ditch will not collect run off from a tributary area exceeding 13,500 square feet (projected) without discharging into a down drain.
- Slopes having a horizontal to vertical ratio of greater than three to one will not require terraces.

12.5

CUTS

Unless otherwise recommended in the approved soils engineering and/or engineering geology report, cuts will conform to the provisions of this section.

The slope of cut surfaces will be not steeper than is safe for the intended use and will be no steeper than 2 horizontal to 1 vertical unless the owner furnishes a soils engineering or an engineering geology report, or both, stating that the site has been investigated and giving an opinion that a cut at a steeper slope will be stable and not create a hazard to public or private property.

12.6

FILLS

Unless otherwise recommended in the approved soils engineering report, fill will conform to the provisions of this section.

FILL LOCATION

Fill slopes will not be constructed on natural slopes steeper than two to one.

FILL MATERIAL

Detrimental amounts of organic material will not be permitted in fills. Except as permitted by the chief building official, no rock or similar irreducible material with a maximum dimension greater than 12 inches will be buried or placed in fills.

The chief building official may permit placement of larger rock when the soils engineer properly devises a method of placement, continuously inspects its placement and approves that fill stability. The following conditions will also apply:

- Prior to issuance of the grading permit, potential rock disposal areas will be delineated on the grading plan.
- Rock sizes greater than 12 inches in maximum dimension will be 10 feet or more below grade, measured vertically.
- Rocks will be placed so as to assure filling of all voids with fine soil.
- The slope of fill surfaces will be not steeper than is safe for the intended use. Fill slopes will be no steeper than two horizontal to one vertical.

12.7

SETBACKS

Cut and fill slopes will be set back from site boundaries in accordance with this section. Setback dimensions will be horizontal distances measured perpendicular to the site boundary. Setback dimensions will be as shown in Exhibit 12.7a

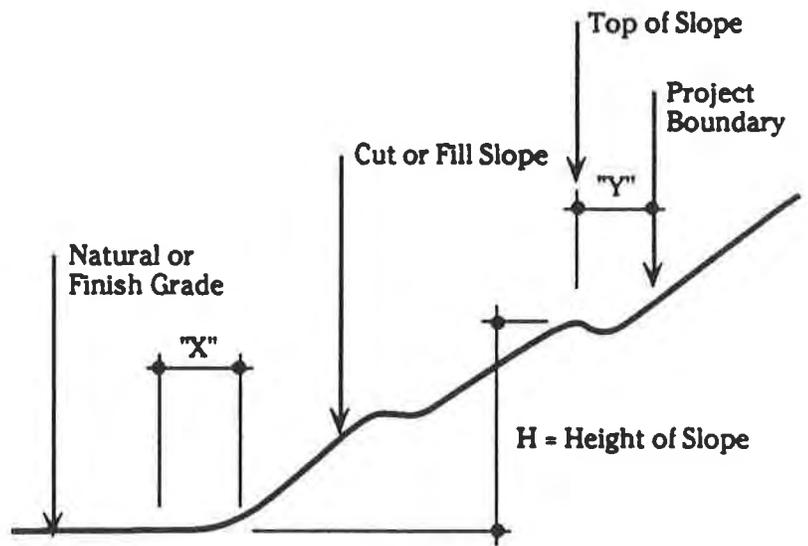
TOP OF CUT SLOPE

The top cut slopes will be made not nearer to a site than one-half of the height with a minimum of two feet, and a maximum of 10 feet. The setback may need to be increased for any required interceptor drains.

TOE OF FILL SLOPE

The toe of fill slope will be made not nearer to the site boundary line than one-half the height of the slope with a minimum of 2 feet and a maximum of 20 feet. Where a fill slope is to be located near the site boundary and the adjacent off-site property is developed, special precautions will be incorporated in the work as the chief building official deems necessary to protect the adjoining property from damage as a result of such grading. These precautions may include but are not limited to:

- Additional setbacks.
- Provisions for retaining or slough walls.
- Mechanical or chemical treatment of the fill slope surface to minimize erosion.
- Provisions for the control of surface waters.



$X = H/2$, but 2' minimum and 20' maximum

$Y = H/5$ but 2' minimum and 10' maximum

EXHIBIT 12.7A
GRADING SETBACKS

13.1**UTILITY PLANS**

All conventional urban utilities will be provided in North Peak including water, sewer, electricity, gas and cable television. The following paragraphs describe in a general way how these systems will be organized and how they will function. More precise plans will be prepared as each village is developed.

13.2**WATER SERVICE**

Water service within North Peak will be provided by the Elsinore Valley Municipal Water District (EVMWD). The North Peak master Plan calls for new reservoirs and supply lines to be constructed. Once completed, adequate domestic water supply and water for fire protection would be available throughout the developed portions of the community. The proposed water service plan is shown in Exhibit 13.2a. A more detailed version of this plan is available in the *North Peak Specific Plan Technical Appendix*.

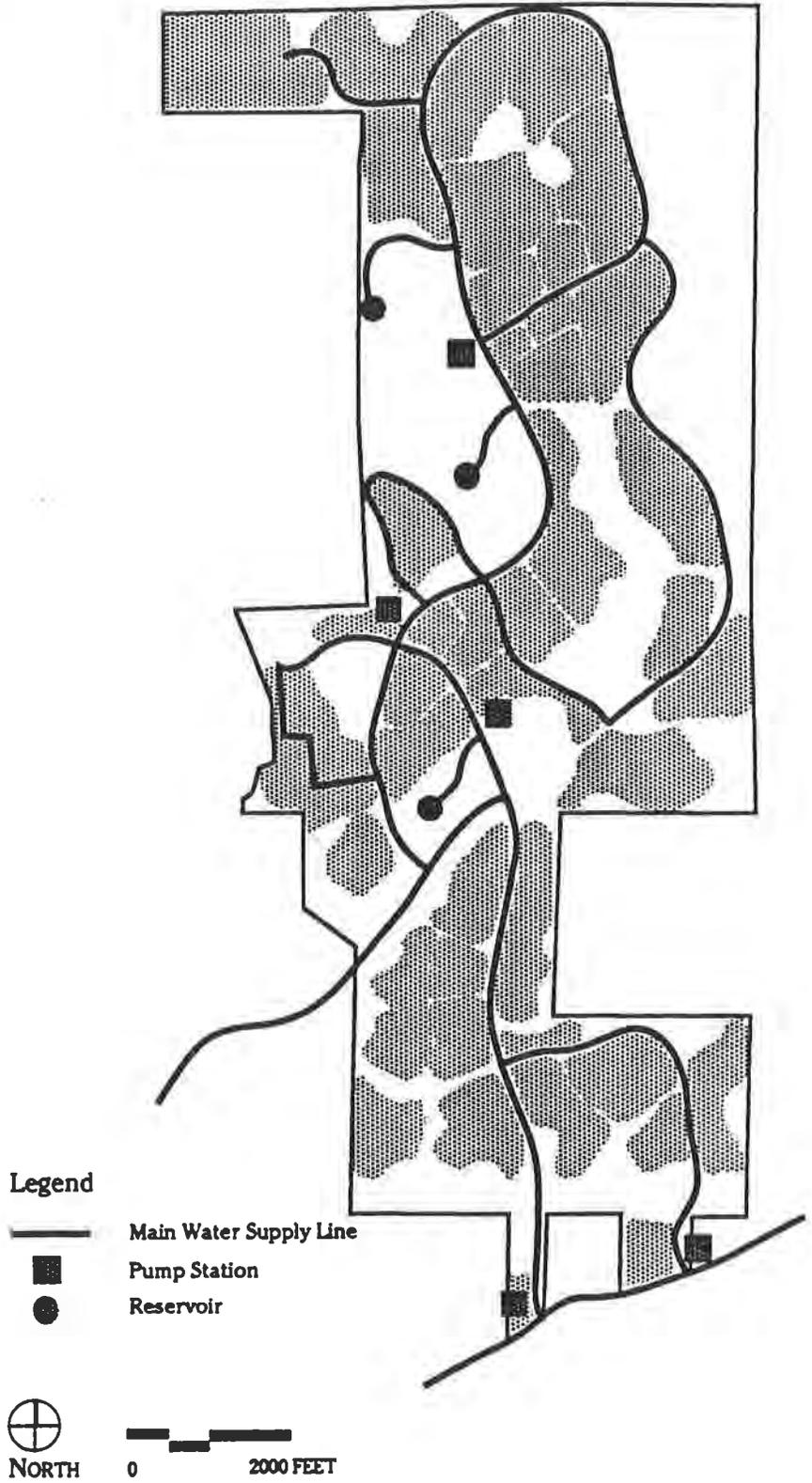


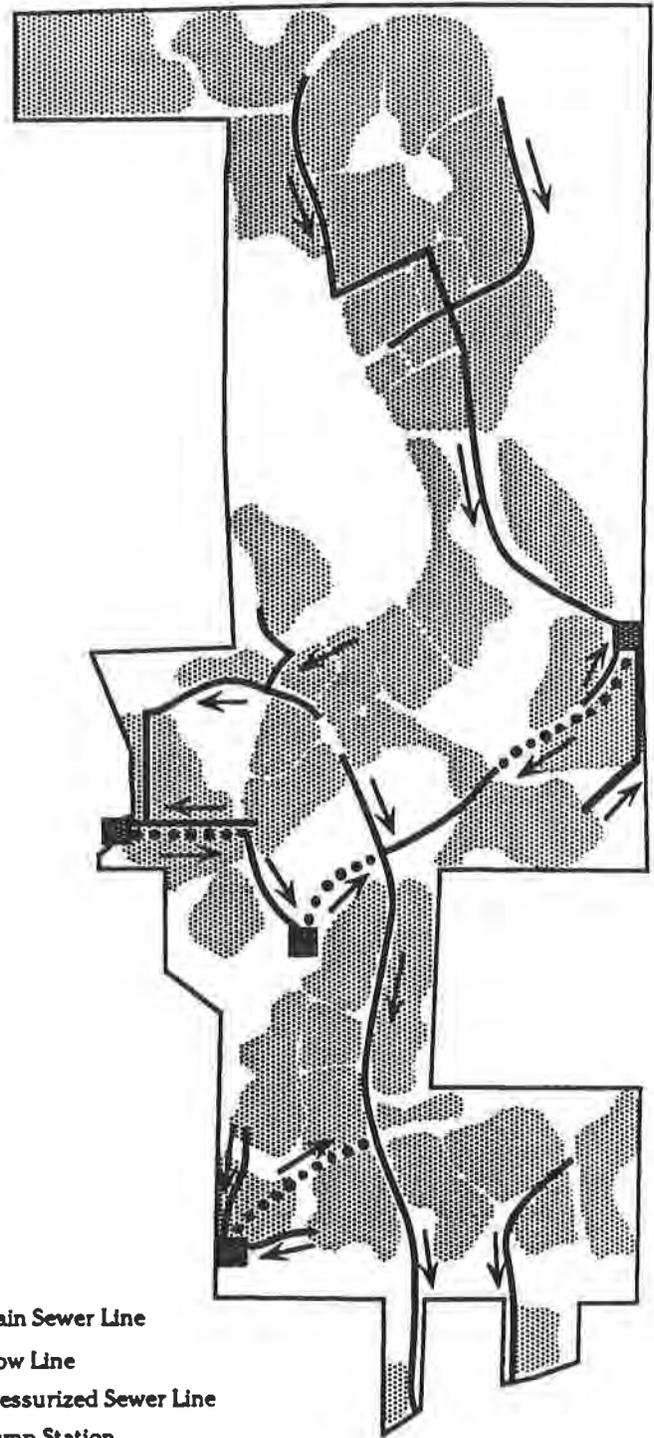
EXHIBIT 13.2A
WATER SERVICE PLAN

13.3**SEWER SERVICE**

The North Peak sewer system will also be part of the EVMWD. Planned improvements to the wastewater treatment plant approximately 2 miles from the site will eventually have the capacity to meet the needs of North Peak, as well as those of many other projects within the City of Lake Elsinore.

On-site sewage collection will be accomplished with a main line system and lift stations. All sewage generated by the community will be discharged into a proposed off-site sewer main in Highway 74. This sewer will flow south along Highway 74 to an existing sewer that flows directly to the EVMWD wastewater treatment plant.

The sewer master plan shown in Exhibit 13.3a identifies the sewer line locations, flow lines, and pump stations necessary to provide sewer service to the community.



Legend

- Main Sewer Line
- ← Flow Line
- • • • Pressurized Sewer Line
- Pump Station



NORTH



0 2000 FEET

EXHIBIT 13.3A
SEWER SERVICE PLAN

13.4

ELECTRICAL SERVICE

Southern California Edison (SCE) will provide electrical service to North Peak. They maintain electric service lines along Hammack Avenue, Crumpton Road, and Rachel Way, as well as a 12kv pole line along Highway 74. SCE owns a 200-foot right-of-way that bisects the northern third of the site. There are 500kv lines in that right-of-way, though they would not directly serve the site. They also own an unused 350-foot right-of-way that divides the southern third of the site.

Future electric service for North Peak will be provided from the existing Highway 74 facilities which will be upgraded by SCE. According to SCE Company representatives, a 12kv underground line will be adequate to provide electrical service to North Peak.

13.5

GAS SERVICE

Southern California Gas Company (SCG) will provide natural gas to North Peak. The nearest gas source is at Conard Avenue near Highway 74 and continues on Conard to Nichols Road, then north on El Toro Road for 500 feet. The SCG Company indicates that the existing lines on Lake Street and Robb Road are available for local service needs.

13.6

TELEPHONE SERVICE

General Telephone (GTE) will provide telephone service to the site. They have facilities on the SCE electric poles mentioned earlier. Their primary source will be from Highway 74, which they will upgrade to handle the increased demand. A 50-foot by 75-foot site will be required within North Peak to house a Remote Switching Unit (RSU) for phone line switching and service capabilities.

13.7

CABLE TELEVISION

King Video (CATV) currently maintains distribution cables on the SCE pole lines on Highway 74. Cable TV lines for the project would follow the same configuration of the underground electric lines. To adequately serve the project, however, they may need to install a new reception facility. Proposals from other cable television companies may be solicited prior to the development of phase one improvements.

SECTION IV: COMMUNITY DESIGN

14.1

A TRADITIONAL SOUTHERN CALIFORNIA LANDSCAPE

Together with the open space plan, the landscape plan for the North Peak community is a central element in the establishment of the overall community identity. The landscape concept for North Peak is to create an environment that derives its character from the Southern California landscape tradition. This landscape is one which emphasizes rustic landscape qualities, and reflects the Southern California agricultural tradition by incorporating traditional agricultural elements, such as orchards and windrow plantings, into the plan.

Exhibit 14.1a shows some of the elements that are characteristic of the traditional Southern California landscape.



EXHIBIT 14.1A

SINCE THE END OF THE LAST CENTURY, SOUTHERN CALIFORNIA HAS BEEN PORTRAYED AS LAND OF "SUNSHINE, FRUIT AND FLOWERS." POSTCARDS HAVE PLAYED AN IMPORTANT ROLE IN DEFINING SOUTHERN CALIFORNIA'S IMAGE OF ITSELF.

14.2**CONSERVING RESOURCES**

In addition to the overall idea of abstracting traditional Southern California agricultural elements, the plan incorporates several important ideas that are essential to the long term viability of the landscape. First, landscape development will be efficient. That is, it will concentrate resources in those areas receiving the most intense human use, such as parks and recreation facilities. Areas intended primarily for passive or visual amenity will require fewer resources. Second, landscape areas will be designed with the objective of reducing long-term water use to a minimum. Maintenance standards will be directed to gradually weaning plants from watering as they mature, so that eventually, water use can be significantly reduced. Third, the landscape will be designed to minimize long-term maintenance for the majority of landscaped areas. This will be achieved by limiting areas of turf, clipped hedges, and exotic ornamentals to a few, very special places where they can be emphasized. All these objectives are consistent with the design style of the traditional Southern California landscape.

To better describe the intent of the master landscape plan, four distinct zones have been created which exemplify the essence of the Southern California landscape tradition: the native landscape zone, the eucalyptus forest plantation, the orchard landscape, and the processional landscape.

14.3**THE NATIVE LANDSCAPE**

The native landscape will occur in natural open space areas. It will consist of native plant materials retained in their existing condition, and will be the largest landscape area within the community. No water other than natural rainfall will be applied, and maintenance will consist only of trail maintenance and occasional maintenance to reduce fire danger. Figure 14.3a shows the native landscape at North Peak.



EXHIBIT 14.3A
THE NATIVE LANDSCAPE AT NORTH PEAK

14.4

EUCALYPTUS FOREST PLANTATION

Eucalyptus plantations are an integral part of the Southern California landscape tradition. Within North Peak, similar planting will occur over large areas, primarily adjacent to public rights-of-way and on manufactured slopes. The primary functions of this landscape will be to: 1) provide a transition between the native landscape and more intensely cultivated areas; 2) create a unifying landscape element found throughout the community; and 3), provide a landscape backdrop to special landscape treatments.

The dominant tree will be eucalyptus trees planted in small sizes in much the way they were planted throughout California at the turn of the century. Because of their graceful character, the forest will be perceived from a distance as an informal planting; only upon closer examination will one be able to see the regularity of the planting.

The ground plane will consist primarily of drought tolerant spreading shrubs and ground covers, such as *Acacia redolens*, or *Lantana* spp.

Figure 14.4a shows the kind of character created by this type of landscape treatment.



EXHIBIT 14.4A

EUCALYPTUS TREES HAVE BEEN AN IMPORTANT ELEMENT IN SOUTHERN CALIFORNIA LANDSCAPES SINCE THEIR INTRODUCTION IN THE 19TH CENTURY

14.5

THE ORCHARD LANDSCAPE

Since before 1900, the lure of Southern California has included orchards stretched out beneath snow-capped mountains. Citrus orchards, in particular, have fueled this vision, but olives, date palms and avocados have also been admired. At North Peak, an orchard-like landscape will be created in areas which, because of their visual prominence or intense use, are important elements in the community. These areas would include the village centers, parks, recreation facilities, and edges of major public roads. Plant material to be used in these areas may include Citrus (*Citrus spp.*), Olive (*Olea europaea*), Date Palms (*Phoenix dactylifera*), or other ornamentals which have the character of an orchard tree. Trees will be drip irrigated in many areas, and the ground plane will include seasonal grasses and flowers with only limited overhead irrigation during the summer.

In areas where year-round turf is required for recreation activities, this landscape zone will be modified to accommodate these uses.

Exhibit 14.5a is a postcard that shows a traditional orchard landscape in Southern California. Images like this inspired the use of orchard-like plantings in North Peak.



EXHIBIT 14.5A

THE IMAGE OF ORANGES GROWING IN THE SHADOW OF SNOW-CAPPED PEAKS IS WELL-KNOWN THROUGHOUT THE UNITED STATES AND THE WORLD.

14.6

THE PROCESSIONAL LANDSCAPE

The processional landscape will be a formal planting of trees intended to create special visual statements in areas that are particularly important to the overall community identity. It will be used sparingly to increase its dramatic effect. An important use of this landscape type will be the section of El Toro Road connecting the North Peak Hills Village Center with the North Peak Valley Village Center. Here, stately palms will line the edges of the road to create a grand procession from one village to another. Other uses of this landscape type will occur in the village centers and at entries to individual neighborhoods.

Exhibit 14.6a is from an old postcard that shows a traditional processional landscape.



EXHIBIT 14.6A

FORMAL ROWS OF PALMS HAVE BEEN AN IMPORTANT FEATURE IN TRADITIONAL SOUTHERN CALIFORNIA LANDSCAPES

14.7

LANDSCAPE MASTER PLAN

Exhibit 14.7a shows the location of typical cross-sections that illustrate in more detail the landscape concept. Exhibits 14.7c through 14.7e are typical landscape cross-sections.

The plan in Exhibit 14.7b is a diagrammatic landscape plan that shows how the various landscape types interact with one another to create the overall community landscape identity.

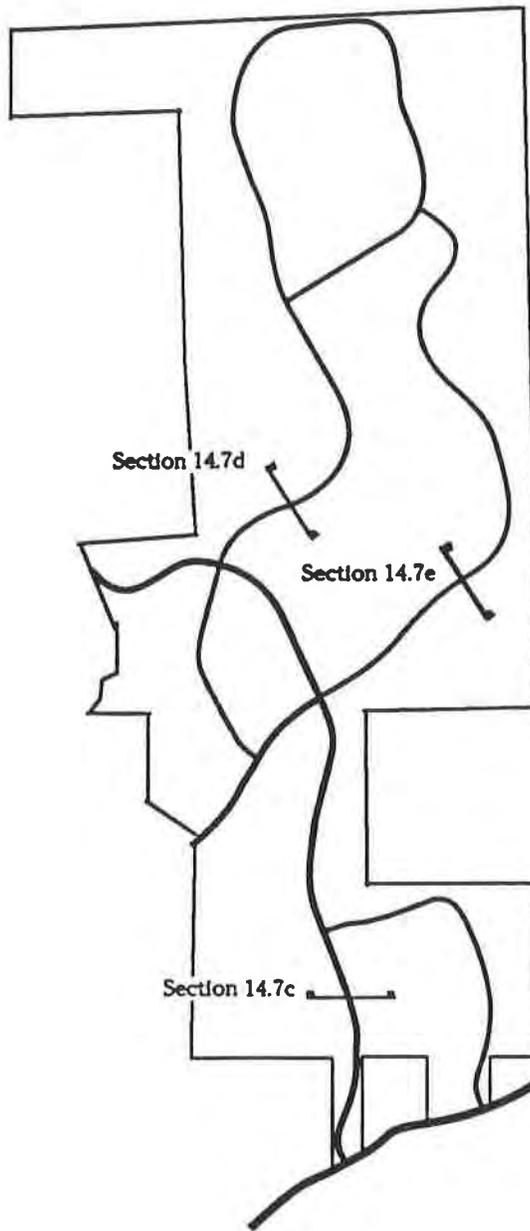
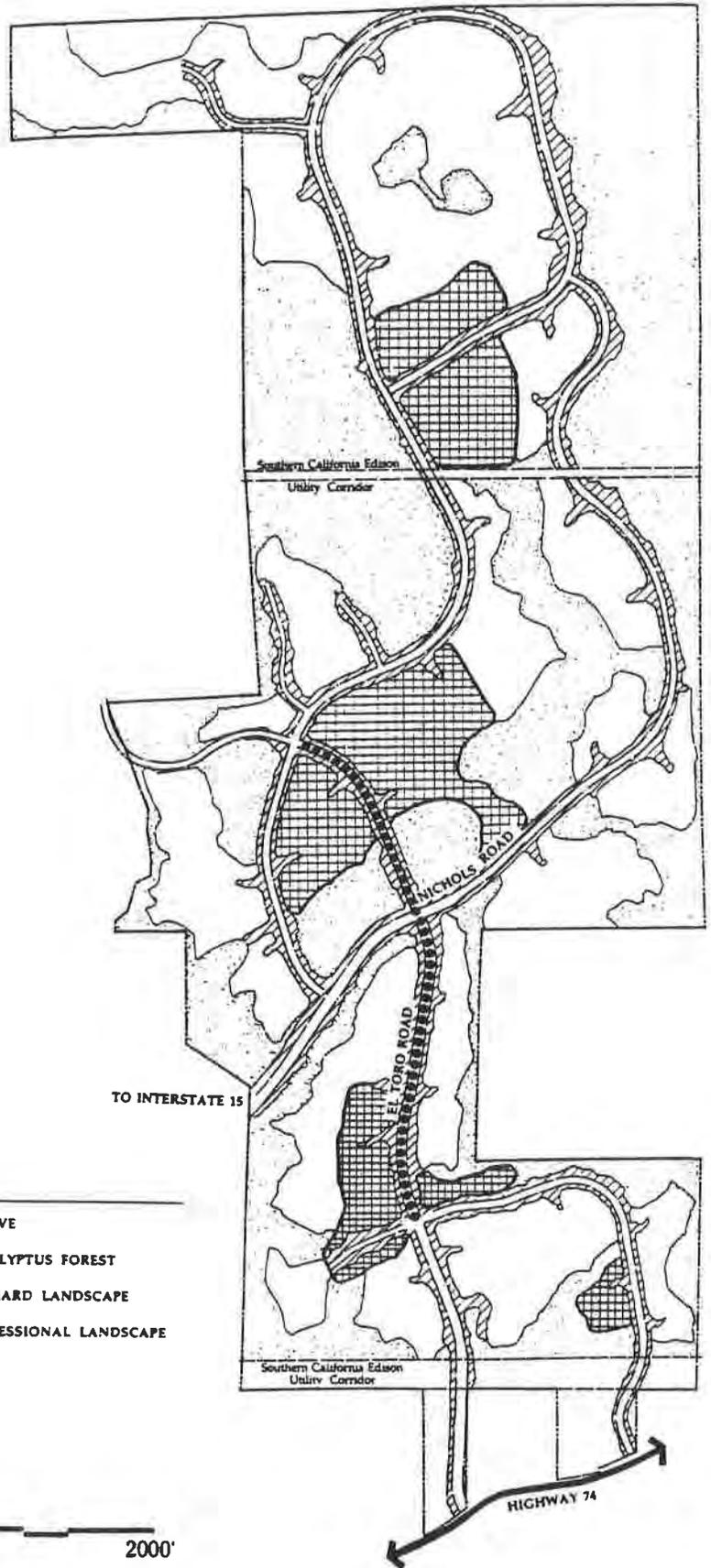


EXHIBIT 14.7A
CROSS SECTION KEY

- LEGEND**
-  NATIVE
 -  EUCALYPTUS FOREST
 -  ORCHARD LANDSCAPE
 -  PROFESSIONAL LANDSCAPE



EXHIBIT 14.7b
LANDSCAPE MASTER PLAN



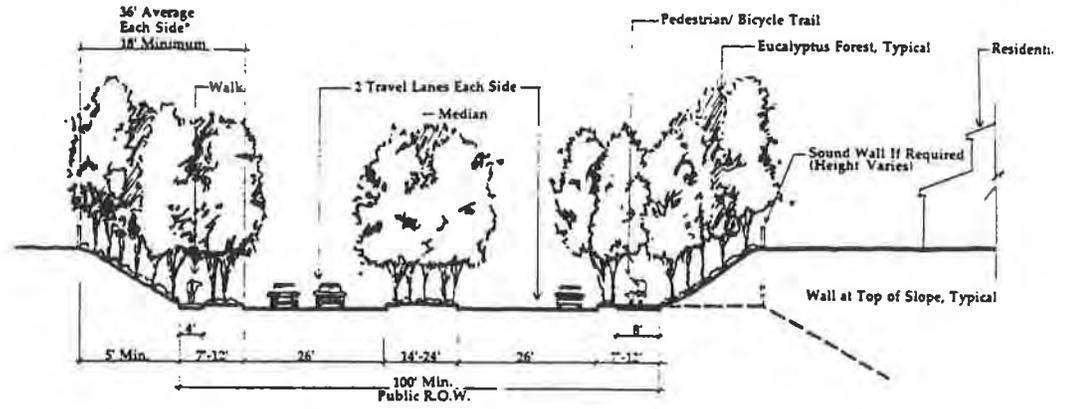


EXHIBIT 14.7c
CROSS SECTION
4-LANE ARTERIAL ROAD AND ASSOCIATED LANDSCAPE (BASED ON GENERAL PLAN "MAJOR")

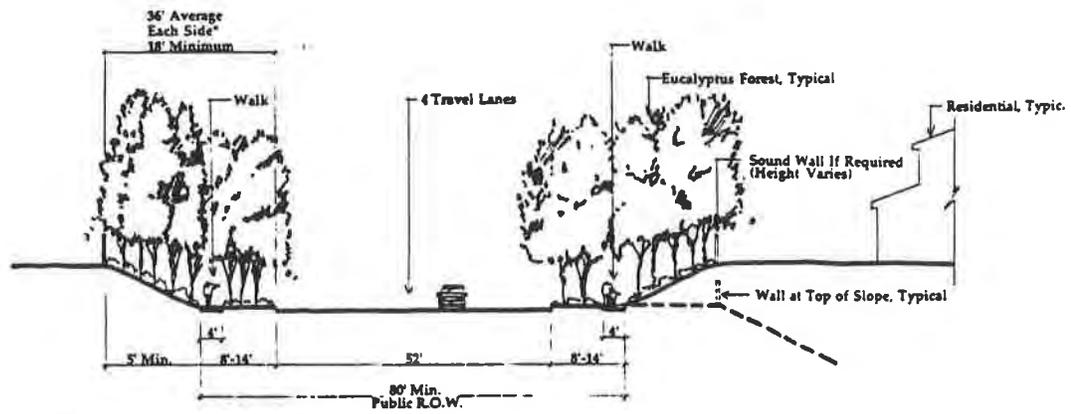


EXHIBIT 14.7D
CROSS SECTION
4-LANE SECONDARY AND ASSOCIATED LANDSCAPE (BASED ON GENERAL PLAN "SECONDARY")

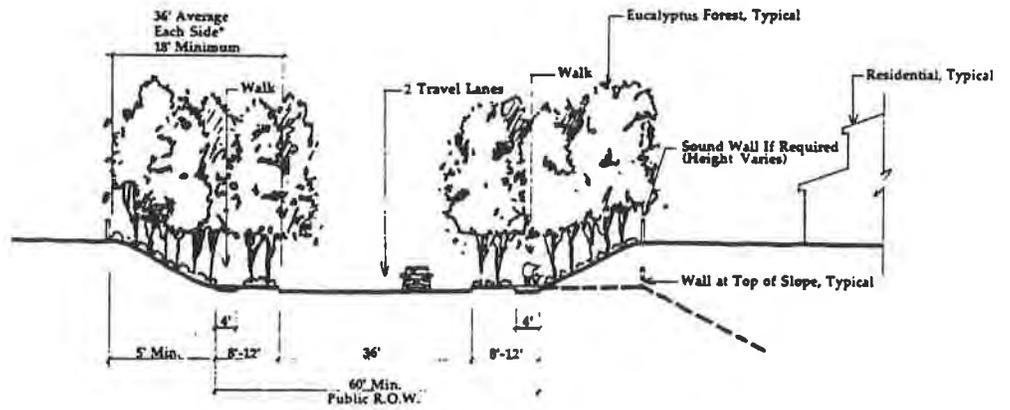


EXHIBIT 14.7E

CROSS SECTION

2-LANE COLLECTOR ROAD WITH ASSOCIATED LANDSCAPE (BASED ON GENERAL PLAN "COLLECTOR")

14.8

MAINTENANCE

There will be levels of maintenance responsibilities: community-wide and project. Maintenance for community-wide landscape areas will be shared by a master homeowners association, a special assessment district administered by the City, and the City itself. Landscape areas within an individual neighborhood will be provided by a homeowners association set up for that individual neighborhood. The following is a summary of maintenance responsibilities for each group.

Natural areas will require very little maintenance, and will be maintained by the master home owners association.

Except for manufactured slopes clearly within a specific project area, manufactured slope areas, and those slopes which are visually important to the whole community, will also be maintained by the master home owners association.

Landscape areas within the public right-of-way will be maintained by a special assessment district to which each North Peak resident will contribute funds through their property tax payment. This special assessment district will be administered by the City.

Public parks, including active parks and shared park facilities, will be maintained by the City of Lake Elsinore.

Community-wide private recreation facilities will be maintained by the master homeowners association; private facilities intended for a limited neighborhood group will be maintained by the group that receives the benefit.

15.1

INTRODUCTION

The criteria described in the following paragraphs and exhibits will guide development within the residential districts within North Peak. They are intended to assure a high quality of development within the community without discouraging individually innovative or imaginative residential development. The diagrams shown represent the average lot size and conditions, although the text refers to the minimums.

15.2

TRADITIONAL AND CONTEMPORARY SUBDIVISION DESIGN

Within the single family, garden residential, village residential, and village residential districts, both traditional and contemporary subdivision design will be permitted. The following diagrams, in combination with the table in Section 4.5, outline the differences between these two approaches to residential subdivision design. None of the diagrams which illustrate typical neighborhood clusters is intended to be a final subdivision design for a particular area of North Peak; each is simply intended to graphically illustrate general design principles.

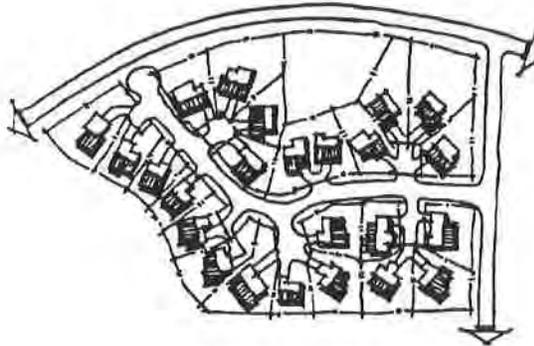


EXHIBIT 15.2A
CONTEMPORARY SUBDIVISION

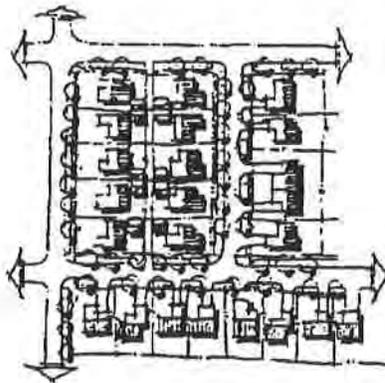


EXHIBIT 15.2B
TRADITIONAL SUBDIVISION

15.3

RURAL RESIDENTIAL

The rural residential district is intended for development of custom single family residences on lots that average 1 acre. The character of this district should be distinctly rustic with narrow streets, minimal grading, and a variety of architectural treatments. The following diagrams illustrate the criteria that should be considered in the development of this district.

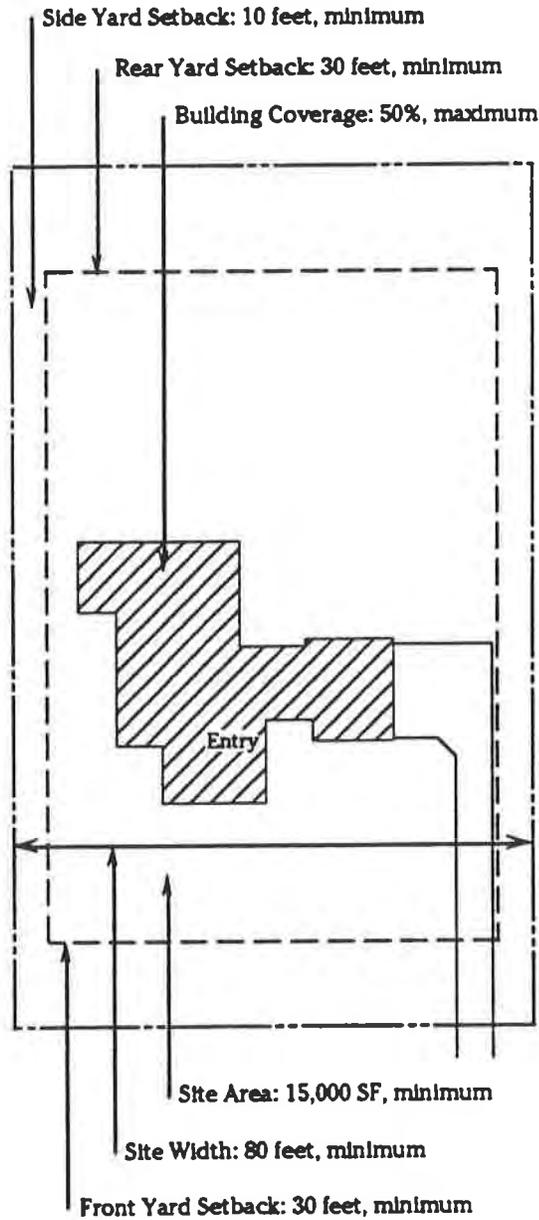
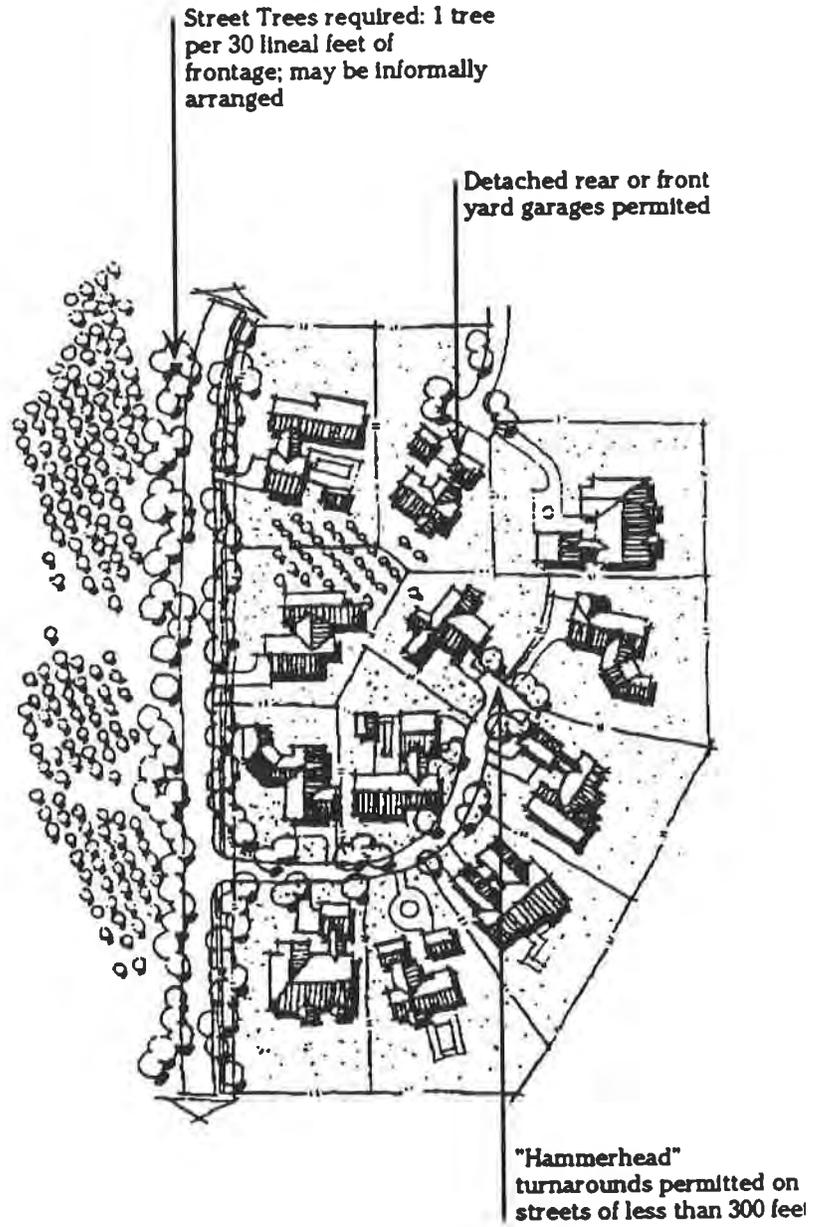


EXHIBIT 15.3A
TYPICAL RURAL RESIDENTIAL (RR) LOT



15.3b
EXHIBIT 15.3b
TYPICAL RURAL RESIDENTIAL (RR) SUBDIVISION

15.4

ESTATE RESIDENTIAL

The estate residential district will be characterized by residential development on lots that range in size from 8,000 to more than 10,000 square feet. Generally, this development will occur in contemporary, rather than traditional, neighborhoods (see comparison Table, Section

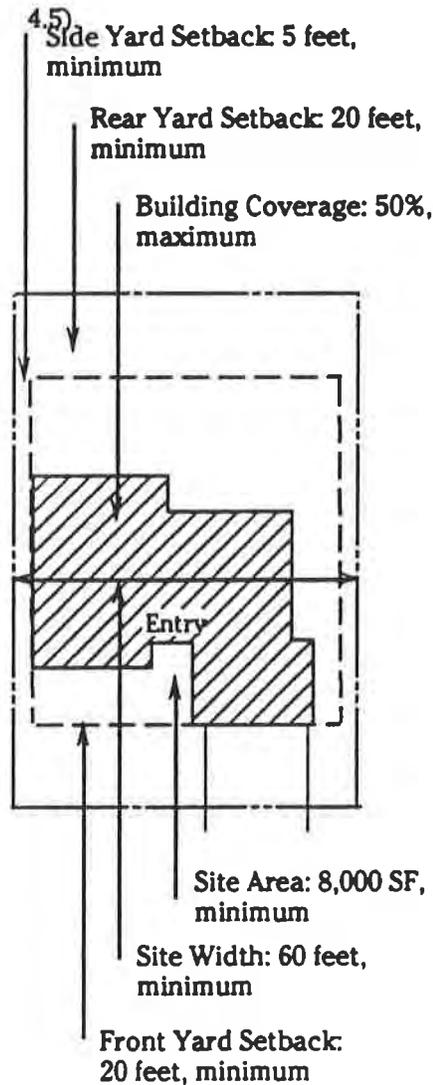


EXHIBIT 15.4A
TYPICAL ESTATE RESIDENTIAL (ER) LOT

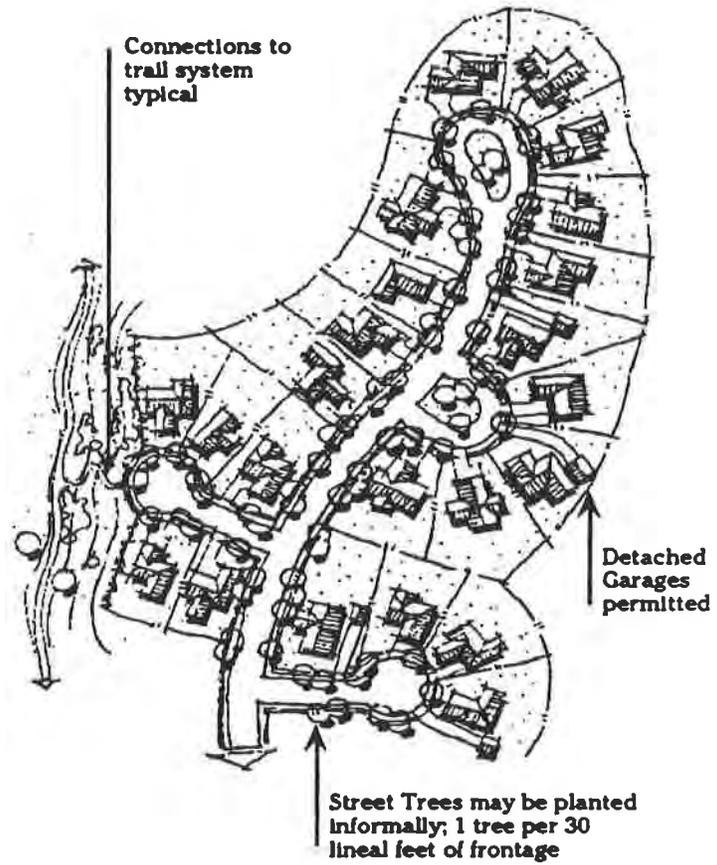


EXHIBIT 15.4B
TYPICAL CONTEMPORARY ESTATE RESIDENTIAL (ER) SUBDIVISION

15.5

SINGLE FAMILY RESIDENTIAL

Single family development on lots ranging from 5,000 square feet to more than 7,200 square feet will be the primary use in this district. Both traditional and contemporary subdivision design will be permitted, but it is likely that most development will be based on the contemporary model. In addition to single family detached development, it is possible that several areas might be developed with either attached dwellings, or that there may be common area landscape developed in conjunction with detached dwellings. The following exhibits illustrate various development possibilities within this district.

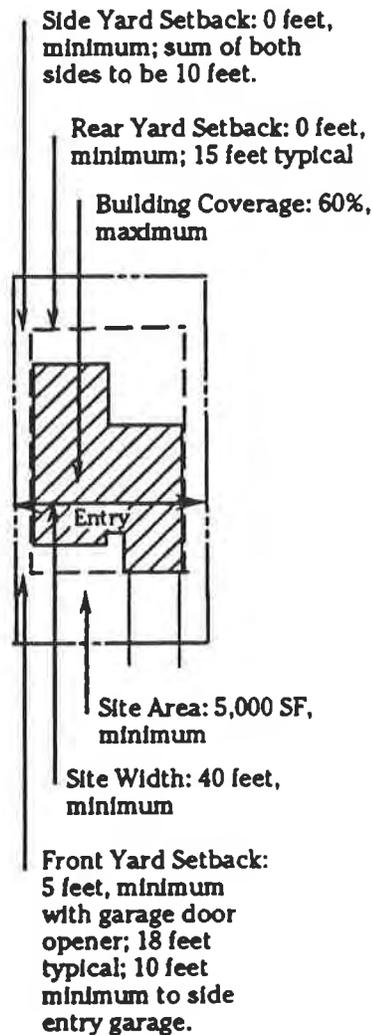


EXHIBIT 15.5A
TYPICAL SINGLE FAMILY (SF) RESIDENTIAL
LOT

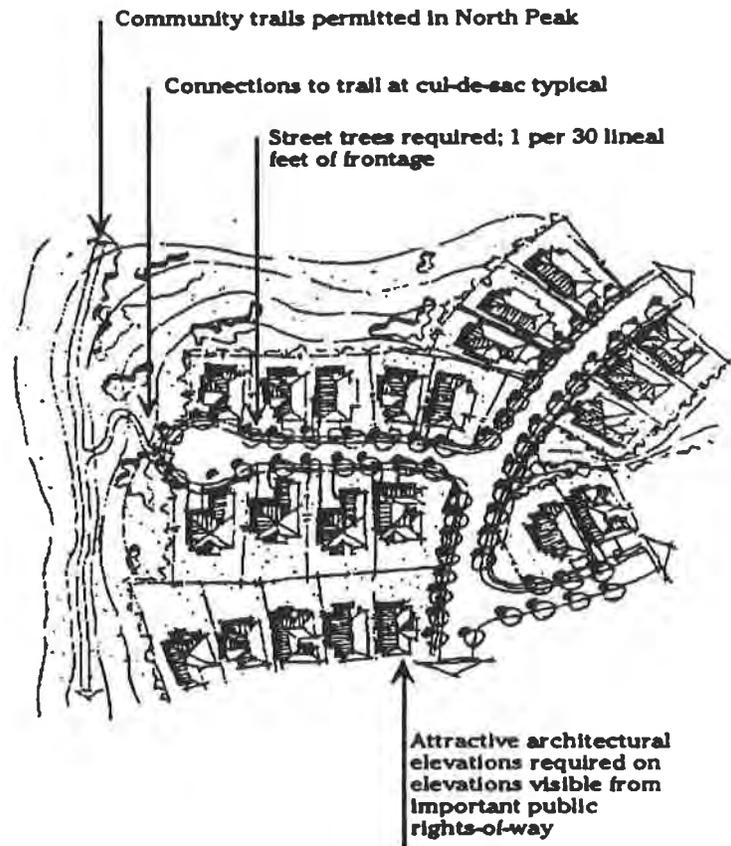


EXHIBIT 15.5a
TYPICAL CONTEMPORARY SINGLE FAMILY SUBDIVISION

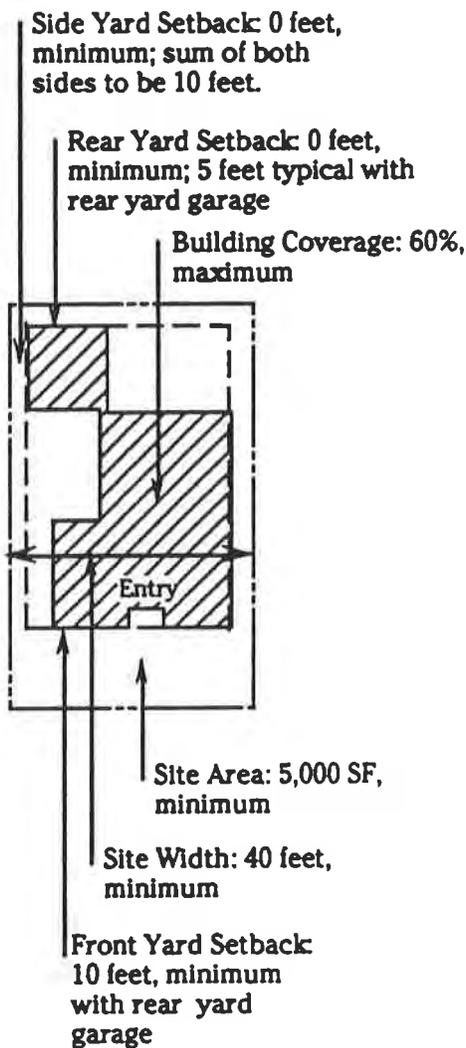


EXHIBIT 15.5c
TYPICAL SINGLE FAMILY (SF) LOT WITH GARAGE IN REAR YARD

15.6

GARDEN RESIDENTIAL

Development in this district will consist primarily of single family attached dwellings, though it is possible that some detached dwellings on lots of 3,000 to 5,000 square feet may be included. Common area landscape and private recreation facilities may be provided in association with development in this district, though it is not required. Homeowners associations will be established to maintain common area facilities. Both traditional and contemporary subdivision design will be permitted, but neither is required.

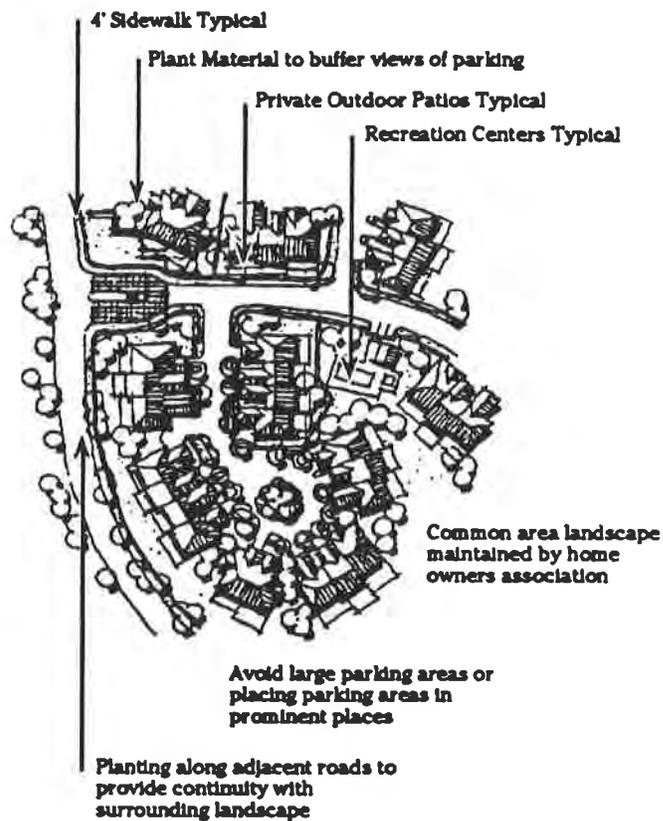


EXHIBIT 15.6A

TYPICAL GARDEN RESIDENTIAL (GR) TOWN HOUSE DEVELOPMENT
(CONTEMPORARY)

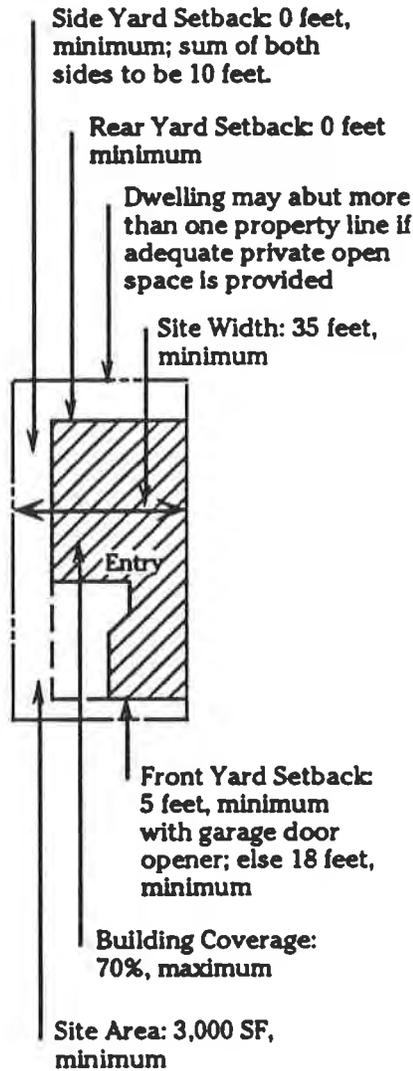


EXHIBIT 15.68
TYPICAL GARDEN RESIDENTIAL (GR) ZERO LOT LINE LOT

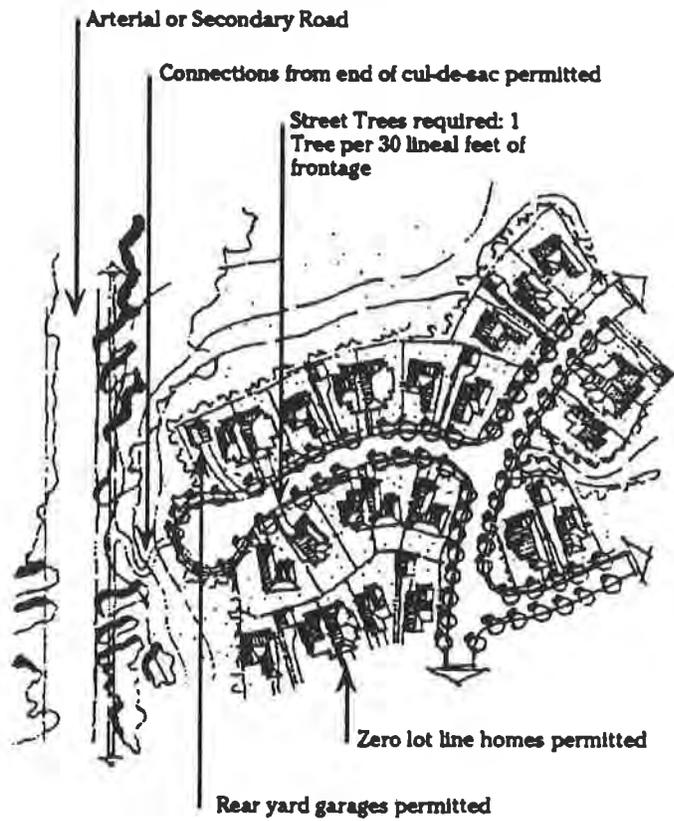


EXHIBIT 15.6c
TYPICAL GARDEN RESIDENTIAL (GR) ZERO LOT LINE NEIGHBORHOOD

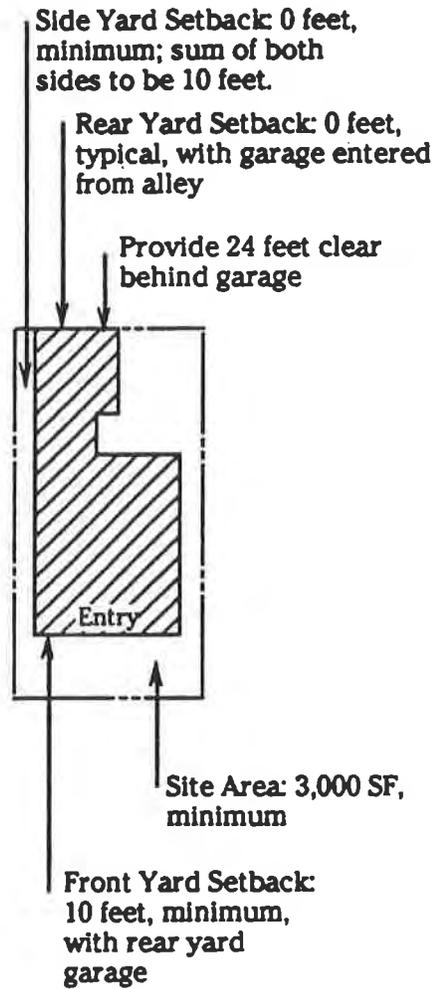


EXHIBIT 15.6D
TYPICAL GARDEN RESIDENTIAL (GR) SINGLE FAMILY DWELLING WITH ALLEY ACCESS

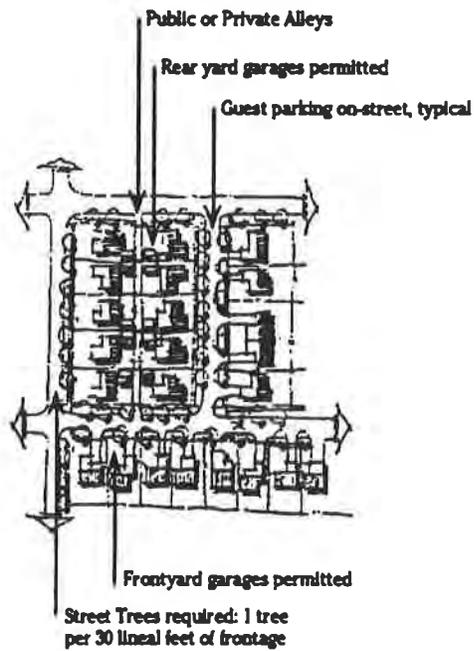


EXHIBIT 15.6E
TYPICAL GARDEN RESIDENTIAL (GR) SINGLE FAMILY NEIGHBORHOOD
WITH ALLEY ACCESS TO GARAGES (TRADITIONAL FORM)

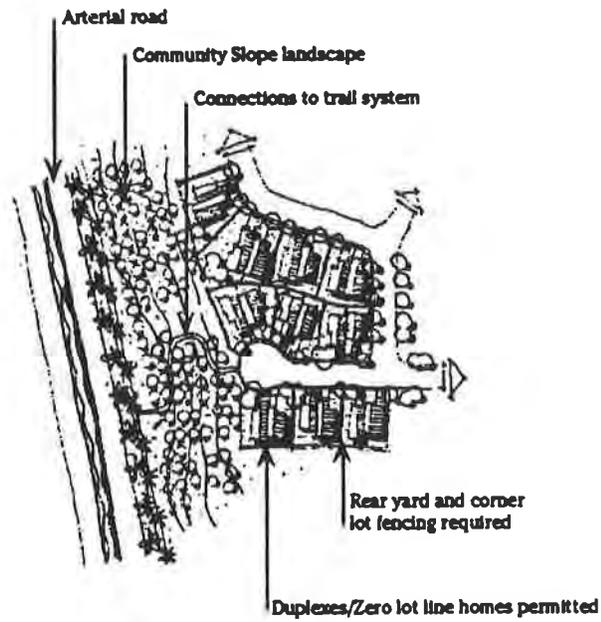


EXHIBIT 15.6f
TYPICAL GARDEN RESIDENTIAL (GR) DUPLEX NEIGHBORHOOD (CONTEMPORARY FORM)

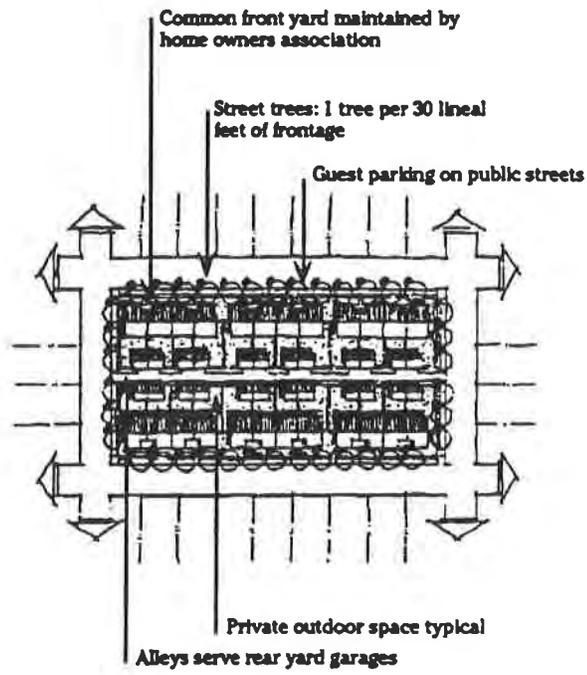


EXHIBIT 15.6G
TYPICAL GARDEN RESIDENTIAL (GR) TOWNHOUSE NEIGHBORHOOD (TRADITIONAL FORM)

15.7

VILLAGE RESIDENTIAL

In the village residential district, all dwellings will be either attached or stacked. Typically, dwellings will be two stories, but some loft type of development in 2-1/2 story buildings may also be accommodated. For-sale and for-rent development projects will be permitted, and shared landscape and recreation improvements will be common. The following diagrams illustrate potential development within this district.

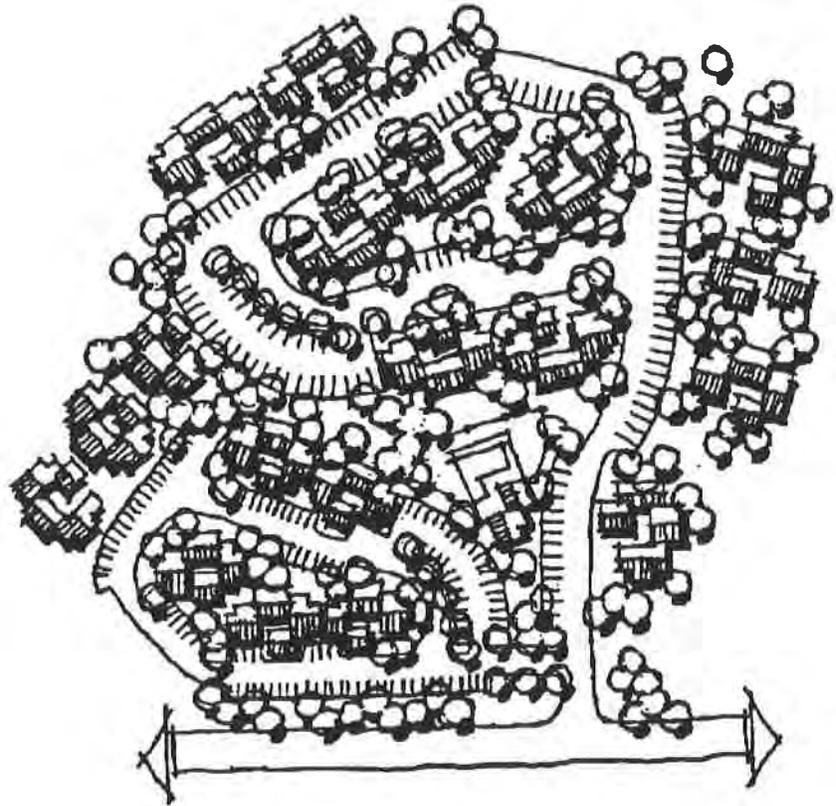


EXHIBIT 15.7A
TYPICAL VILLAGE RESIDENTIAL (VR) TOWNHOUSE NEIGHBORHOOD
(CONTEMPORARY)

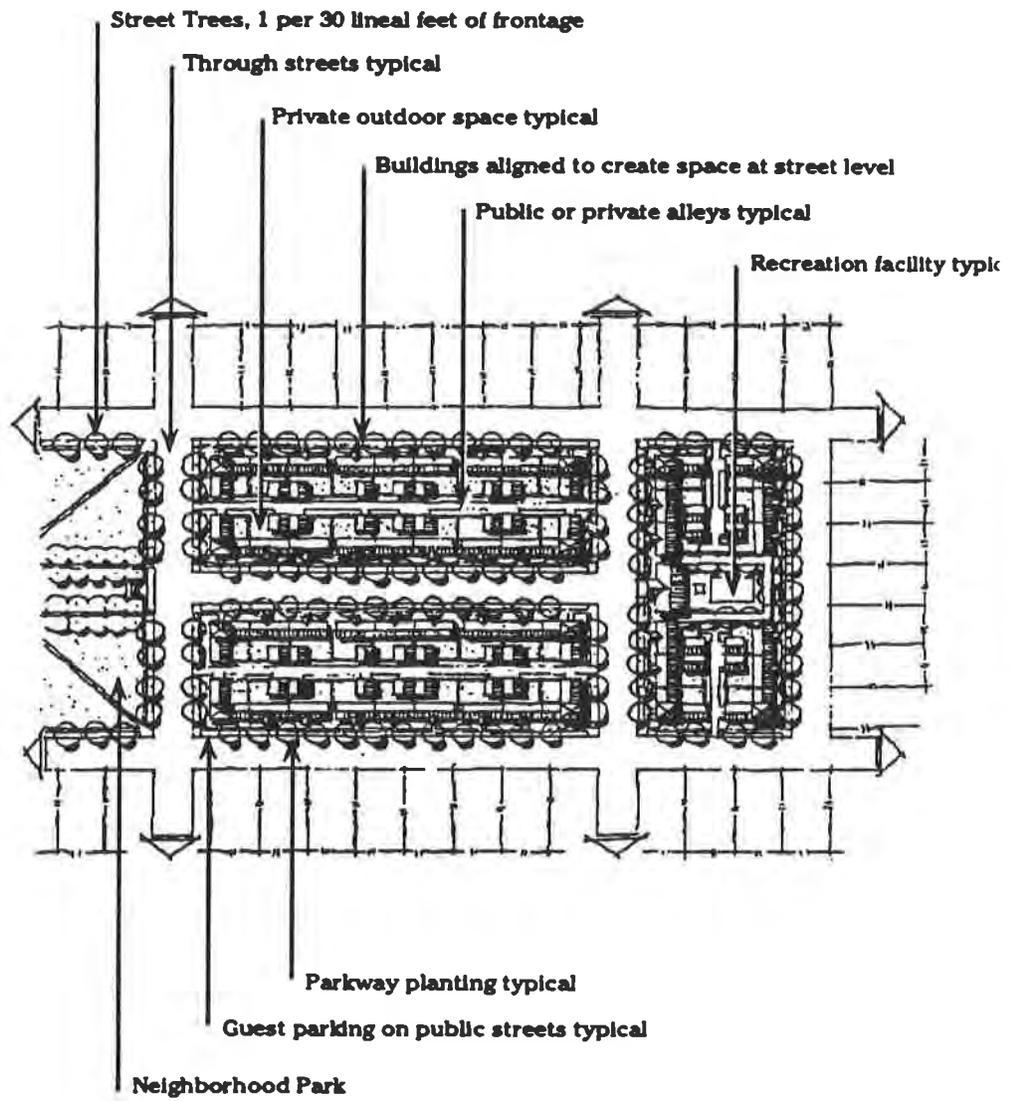


EXHIBIT 15.7b
TYPICAL VILLAGE RESIDENTIAL (VR) TOWNHOUSE NEIGHBORHOOD
(TRADITIONAL)

15.8

MULTI-FAMILY RESIDENTIAL

The multi-family district may include both for-sale and for-rent projects. Typical dwellings will be stacked two or three stories high, but other, innovative architectural treatments may also be possible in this district. The following diagram is only one of many possible design solutions which would be appropriate within this district.

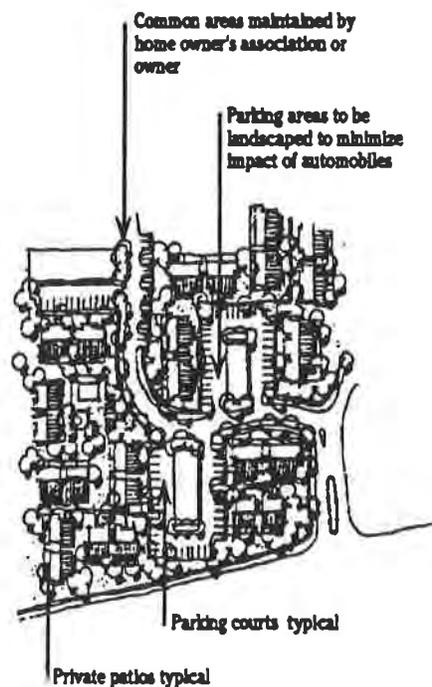


EXHIBIT 15.8A

TYPICAL MULTIFAMILY (MF) RESIDENTIAL NEIGHBORHOOD (CONTEMPORARY)

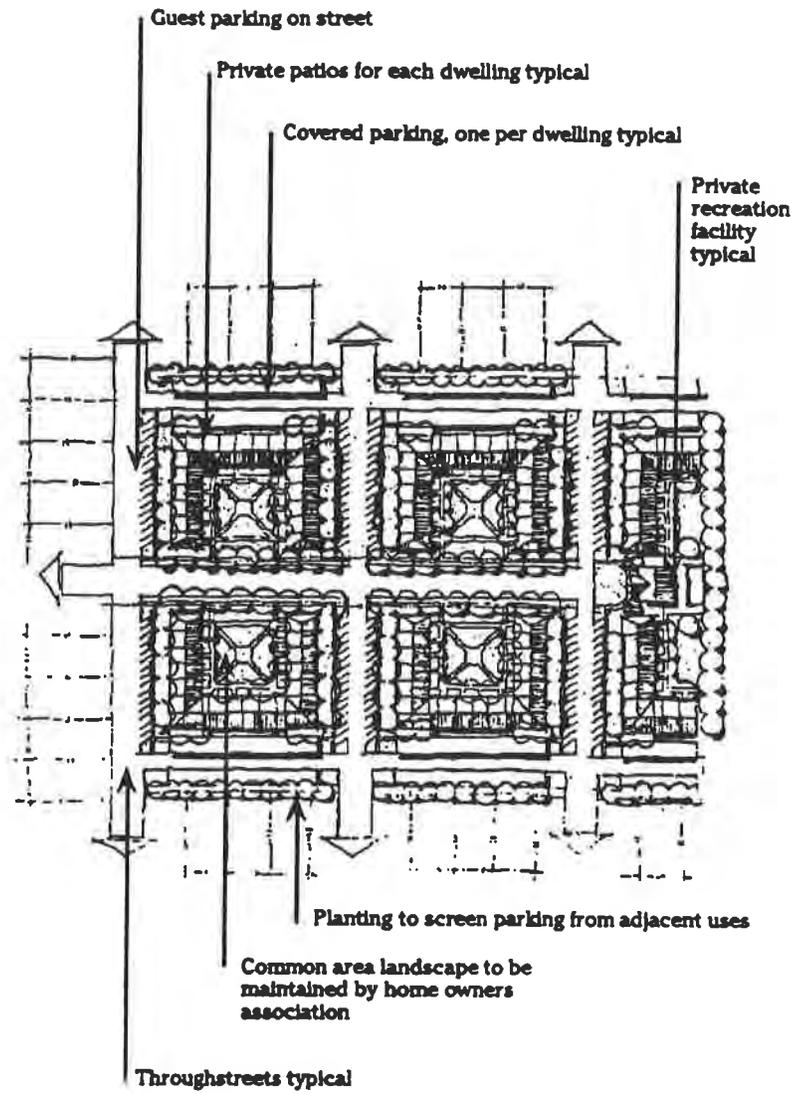


EXHIBIT 15.8a
TYPICAL MULTIFAMILY RESIDENTIAL NEIGHBORHOOD (TRADITIONAL)

15.9

GUIDELINES FOR SMALL LOT SUBDIVISIONS

The conceptual site plan below and sketches on the following pages depict design solutions intended to enhance the quality of small lot single family subdivisions. The main concepts are to provide easy access to nearby open space, improve the streetscene by reducing the visual impact of garage doors and driveways, and provide tree-lined streets to encourage pedestrians and further enhance the neighborhood appearance.

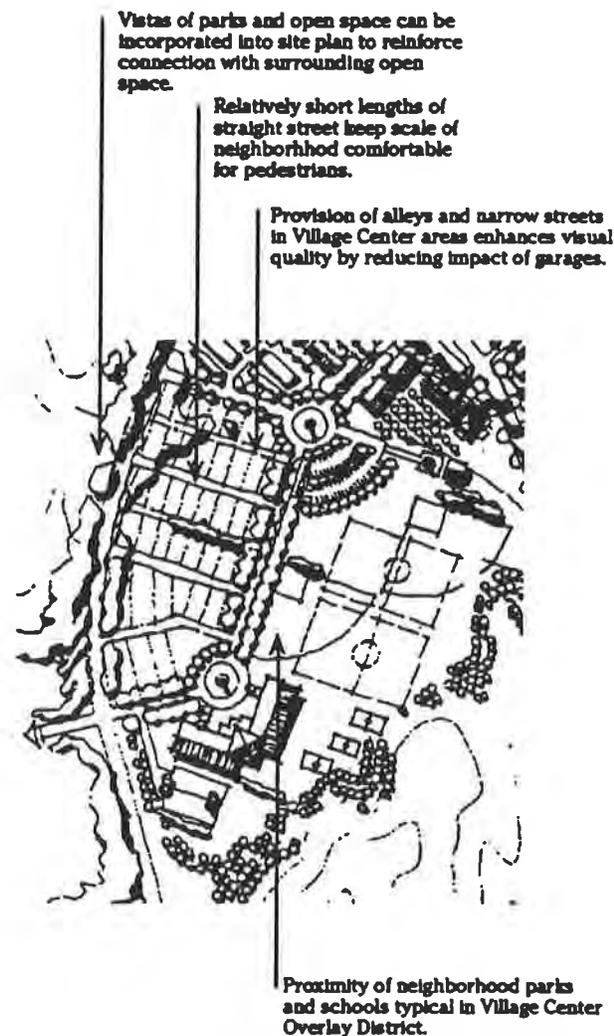


EXHIBIT 15.9A
SITE PLANNING GUIDELINES

This conceptual site plan depicts a traditional single family neighborhood within the North Peak Valley Village Center. It shows several ideas which are important to the successful implementation of small lot subdivisions within North Peak.

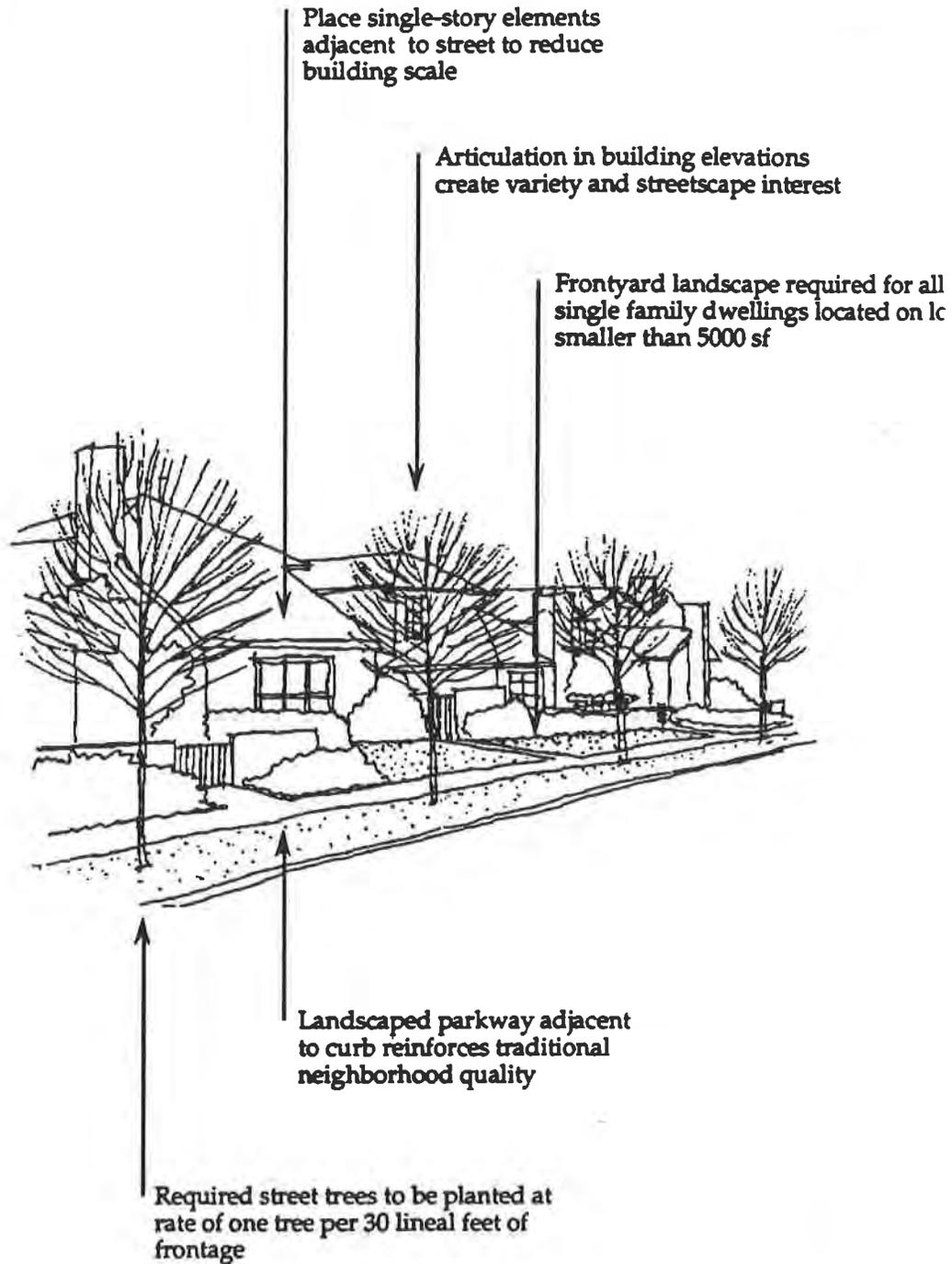
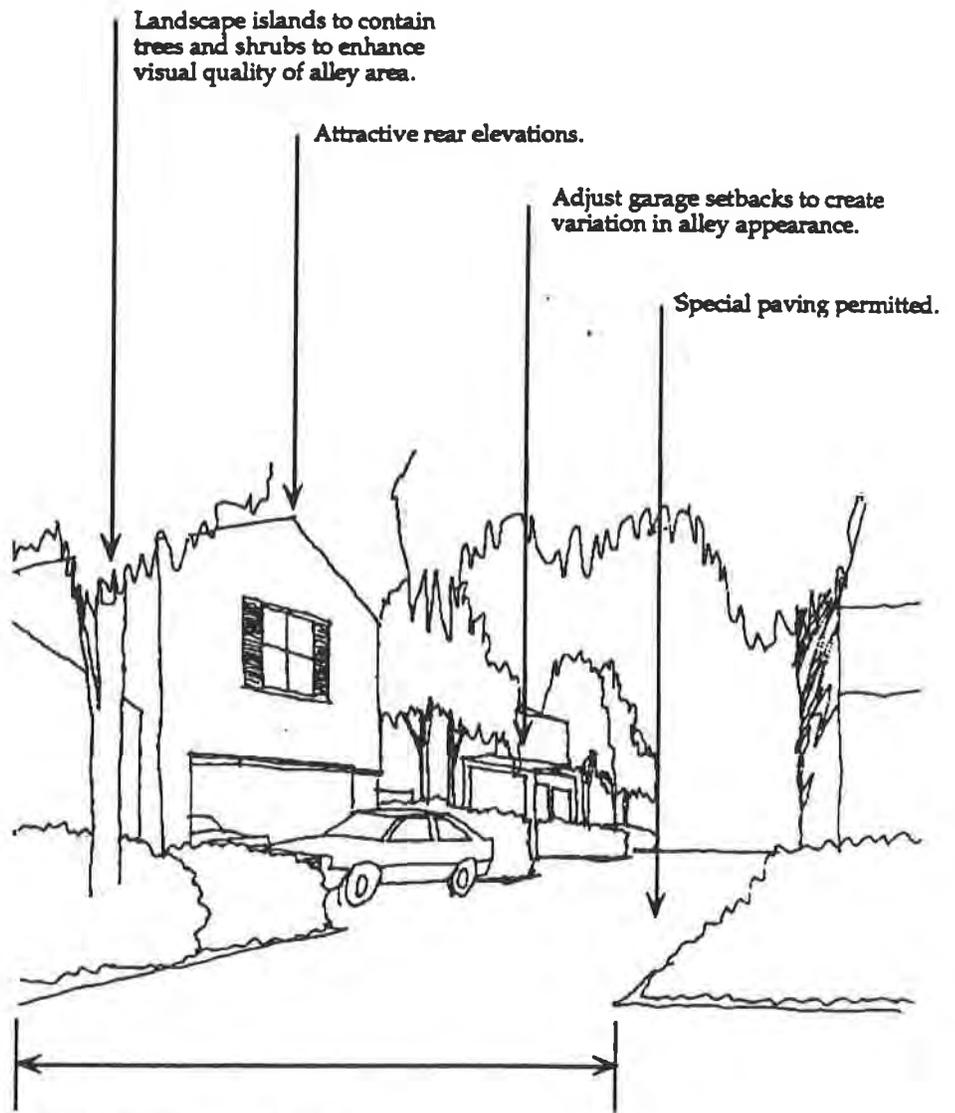


EXHIBIT 15.9b

STREETSCENE: TRADITIONAL SUBDIVISION WITH ALLEY-LOADED GARAGES

This type of development would occur in neighborhoods where the smallest lots are less than 5,000 square feet, and would be found near village centers and public open space.



Narrow travelways with adequate backup space between garage faces.

Additional parking may be provided with alley access to permit reduction of paved street width in subdivisions with alley-loaded garages.

EXHIBIT 15.9c

ALLEY VIEW: TRADITIONAL SUBDIVISION WITH ALLEY-LOADED GARAGES

This type of development would occur in neighborhoods where the smallest lots are less than 5,000 square feet, and would be found near village centers and public open space.

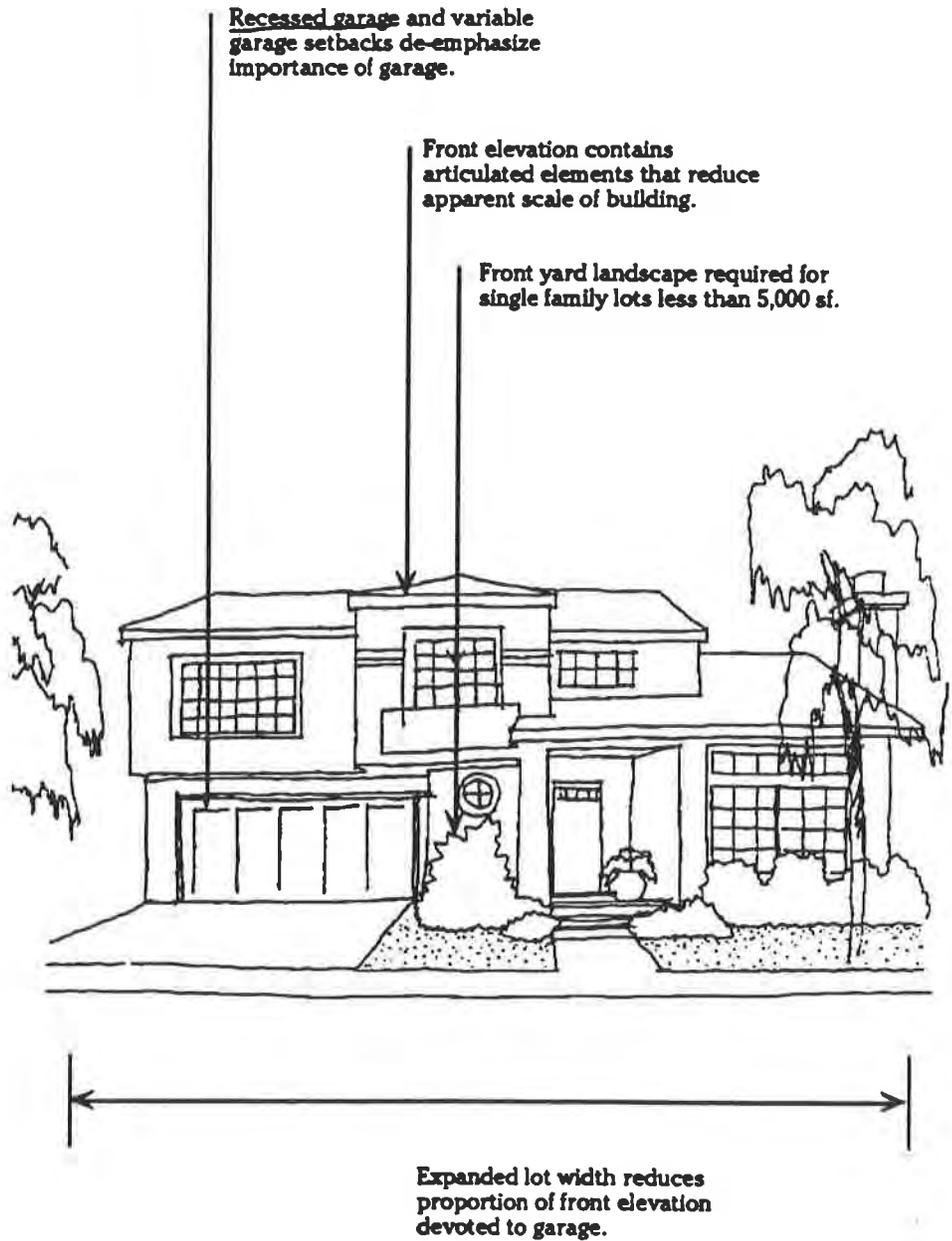


EXHIBIT 15.9d

STREETSCENE: SUBDIVISION WITH WIDE-SHALLOW LOTS

This type of development may occur in either traditional or contemporary neighborhoods where the smallest lots are less than 5,000 square feet. In typical situations, the width and depth of the lot are the same; the rear or side yards may include zero-lot-line conditions.

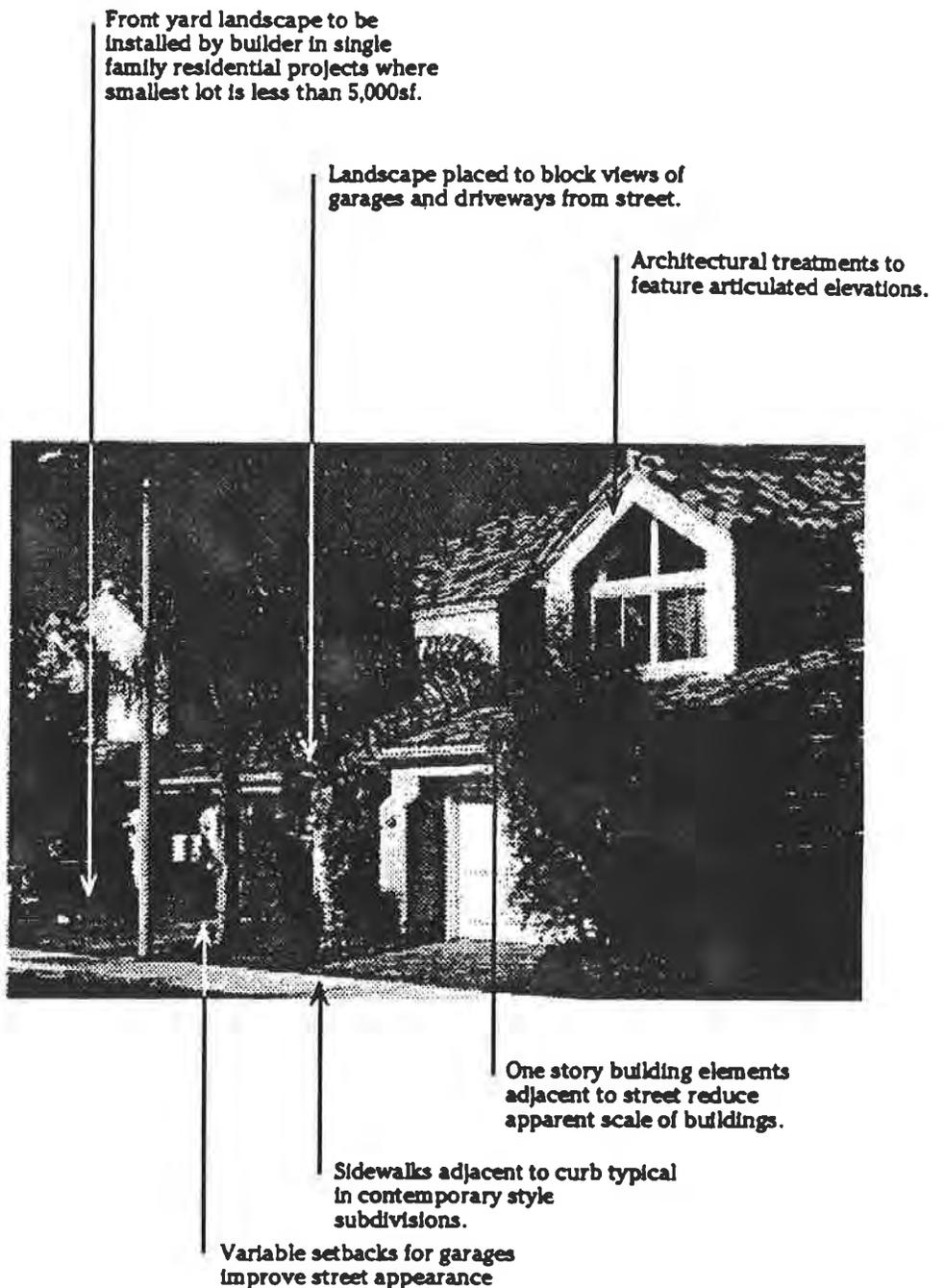


EXHIBIT 15.9e
STREETSCENE: CONTEMPORARY SUBDIVISION WITH STREET-LOADED GARAGES

This type of development may occur in any residential neighborhoods where the lots are between 5,000 and 6,000 square feet. On-street parking is typical, though special areas may be set aside for guest parking in order to reduce the overall paved street width.

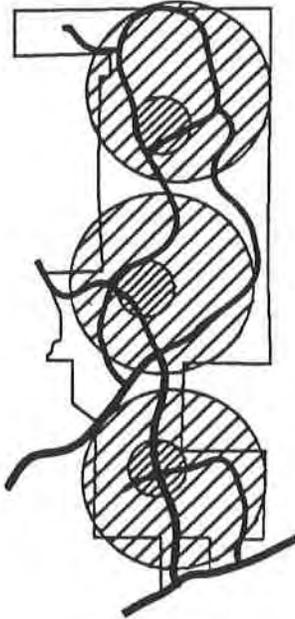


EXHIBIT 16.1A
VILLAGE CENTERS SERVE SURROUND-
ING NEIGHBORHOODS

16.1

VILLAGE CENTER DESIGN CRITERIA

The following design criteria are intended to assist the planners and architects of North Peak's three village centers by providing general guidelines for the design of the Centers. These guidelines describe both overall goals as well as specific design solutions, and are primarily proposed to convey overall design intent. Good judgment in applying these criteria is an essential ingredient in the design of the village centers. (Refer to Section 24.0 for Village Center Design Regulations.)

16.2

THE VILLAGE CENTER SITE

North Peak's three village centers are designated on the land use plan, Exhibit 5.1a. The village centers contain a mix of land uses and the design guidelines contained in this section are intended to apply throughout the village center overlay district defined on the land use plan.

LOCATION

The location of the village centers is established on the land use plan. They are centrally located in each village and are intended to be readily accessible by automobiles, pedestrians and bicycles.

SIZE

The size of the individual village centers is established on the land use plan. They vary in size from approximately 40 acres to 100 acres. The village center in North Peak Valley will be the largest because of its central location in the community and because it will contain more retail development than the other centers. North Peak Hills' and North Peak Highlands' centers will be smaller than the Valley's center, and similar in size to each other. The area in acres included in the village center for a particular village may increase or decrease up to 20%, assuming the overall density of the entire Village remains essentially unchanged. This is to allow flexibility in the final site planning of each center.

16.3

VILLAGE CENTER SPATIAL STRUCTURE

The spatial structure of the village center is one of the most important factors in the design of the centers because it establishes the overall scale and character of the center. Spatial structure is established by the size and location of buildings, open spaces and landscape elements. The arrangement of these elements within the village centers should create a spatial structure that has an intimate, personal quality.

HIERARCHY OF SPACES

A hierarchy of spaces should be created by the organization and placement of structures and landscape elements.

a. Large Spaces

Spaces which have a role in creating the overall identity of the village or the community should be the most grand in scale and most visually prominent; generally, these spaces will be associated with public use, and will have a strong tie to main circulation routes. Exhibit 16.3a shows how a successful large space might be configured in a village center.



EXHIBIT 16.3A

MALAGA COVE SHOPPING CENTER, PALOS VERDES, A LARGE SPACE UNIFIED BY A CENTRAL FOUNTAIN/ SCULPTURE

b. Intimate Spaces

These spaces will conform in scale to uses which are intended for small gatherings; they should convey a sense of safety and enclosure. Exhibits 16.3b and 16.3c are examples of successful intimate spaces in existing Southern California village centers.



EXHIBIT 16.3b
EL PASEO, SANTA BARBARA, AN INTIMATE SPACE HIGHLIGHTED BY A SMALL FOUNTAIN



EXHIBIT 16.3c
COURTYARD IN RANCHO SANTA FE

ENCLOSURE

Buildings and landscape elements should create a sense of enclosure in the village centers. Vast, unenclosed, and characterless spaces are to be avoided.

a. Building Height vs. Building Separation

The height of buildings next to large spaces should generally be taller than buildings next to small spaces in order to create a comfortable sense of enclosure. Proportions of 3:1 are ideal (see Exhibits 16.3d and e); proportions greater than 6:1 should be avoided, if possible. Landscape elements should be considered in assessing spatial scale. Large trees or palms are especially important because they can create the proper scale when buildings are smaller or further apart.

b. Continuity of Enclosure

Generally, enclosure should be continuous; large breaks in continuity are to be avoided. Continuity of enclosure is particularly important along circulation routes

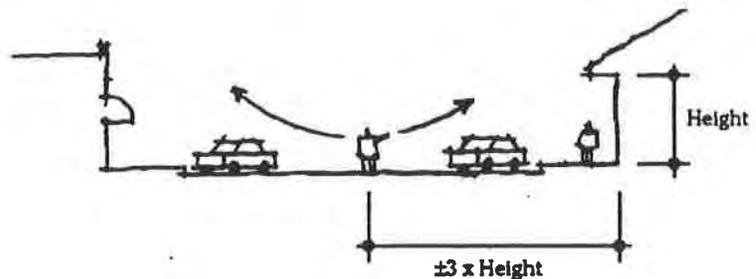


EXHIBIT 16.3D
BUILDINGS CREATE SPACE WHEN PROPORTIONS ARE SUITABLE

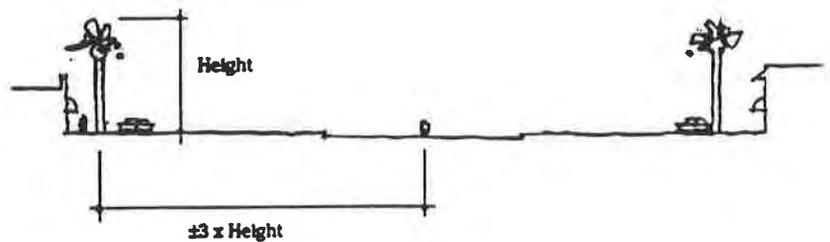


EXHIBIT 16.3E
LANDSCAPE ELEMENTS CAN CREATE SPACE WHEN BUILDINGS LACK HEIGHT

STRONG LINKAGES

Links between important village center elements should be reinforced by building and landscape elements. In the case of landscape elements, these links may be made with both hard-scape (such as paving), or softscape (such as areas of planting).

HUMAN SCALE

Human scale is a term used to describe the appropriateness of the size of architectural and spatial elements relative to the size and capabilities of human beings. The scale of spaces within the village center should convey a feeling of comfort and security throughout the center so that people will not be overwhelmed by vast unenclosed spaces, massive buildings without articulated elevations, and speeding automobiles.

16.4

VILLAGE CENTER LAND USE

Land use within the village center should be fine-textured and varied. The intent is to promote a richness more commonly found in older, established towns than is typical in modular development patterns. (Exhibit 16.4a below illustrates this distinction) For this reason, flexibility in the precise location of specific uses within the village center is of prime importance.

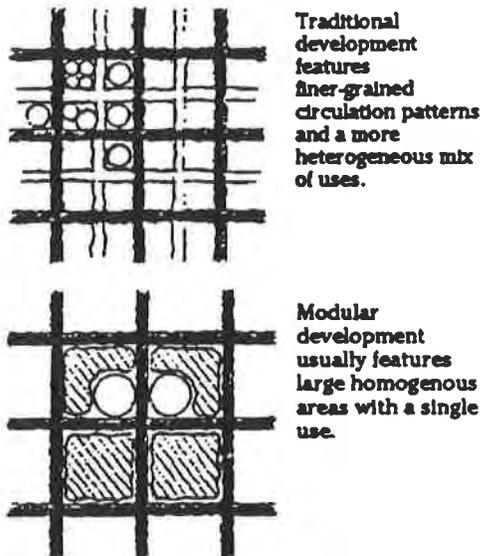


EXHIBIT 16.4A
MODULAR VS. HETEROGENEOUS URBAN FABRIC

16.5

VILLAGE CENTER CIRCULATION

The circulation system should be designed to provide convenient, safe, access to all parts of the village center by all means of circulation, including automobiles and pedestrians. Pedestrians are of particular importance in the village center, and the centers should be designed with their needs in mind first. Automobile circulation design should emphasize accommodation of the pedestrian.

RENEW PRIMACY OF PEDESTRIAN

Pedestrians are the life-blood of the village center; their comfort and safety, and provision of adequate pedestrian access to all parts of the village center is a primary design objective of the Plan.

a. Direct connections along roads

Pedestrian walks will be provided along all roads within the village center; adequate separation from automobile traffic, such as landscaped parkways, parking lanes and other devices should be provided to increase the pedestrians' feeling of safety.

b. Walk size

Pedestrian walks should be a minimum of 4 feet wide; they should be larger in areas of intense pedestrian uses, such as in commercial and civic uses areas (Exhibits 16.5a-c).

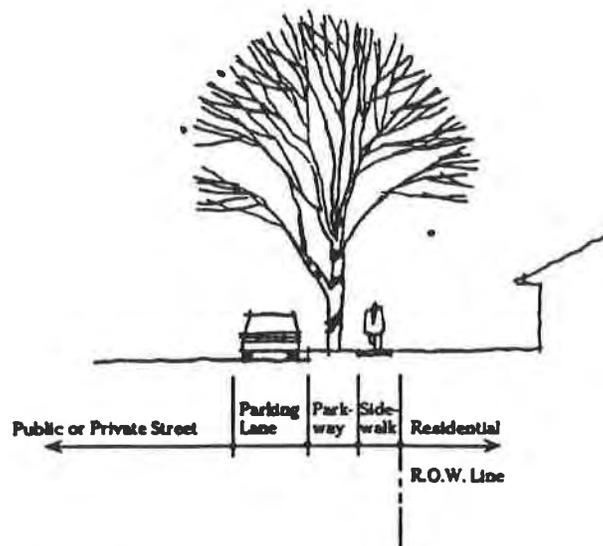


EXHIBIT 16.5A
VILLAGE CENTER STREET IN RESIDENTIAL AREA.

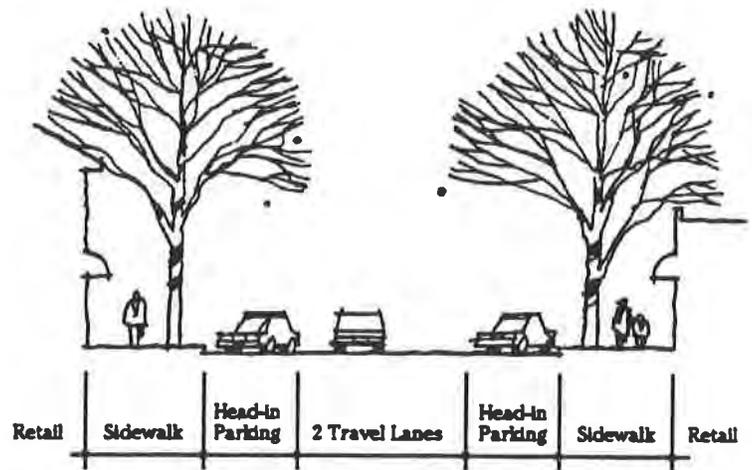


EXHIBIT 16.5b
VILLAGE CENTER STREET WITH HEAD-IN PARKING

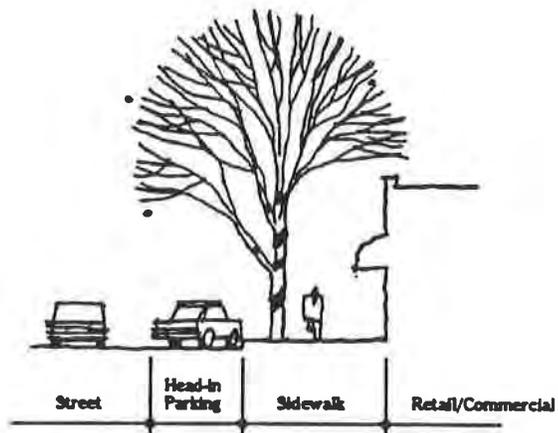


EXHIBIT 16.5c
VILLAGE CENTER CROSS SECTIONS

c. Provide for pedestrian comfort

Where appropriate and justified by pedestrian use, provide protection from the elements. Protection can include tree canopy, pergolas, arcades, or other landscape or architectural devices. Street furniture should be provided in areas likely to be gathering places for pedestrians such as bus stops, or near civic and commercial uses. Exhibit 16.5d is a sketch that shows how an arcade might support this guideline in a village center.

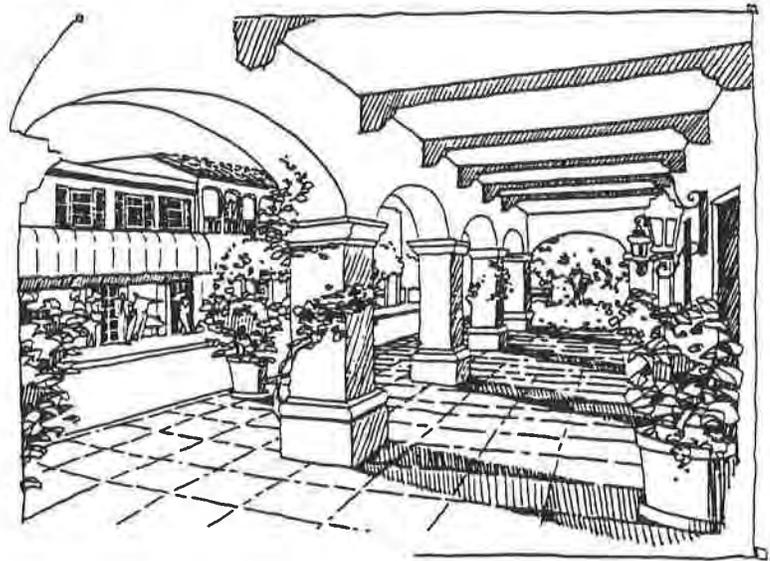


EXHIBIT 16.5d

ARCADES PROVIDE PROTECTION FROM THE ELEMENTS AND PROMOTE PEDESTRIAN COMFORT

d. Connections to public spaces

Provide direct connections to the open space elements of the village center and increase the apparent size of the public spaces. These connections will most often be adjacent to streets, but may also be in paseos.

REDUCE IMPACT OF AUTOMOBILE

The needs of automobiles should complement, rather than dominate other village center design goals. They should be considered visitors and their needs accommodated without disruption of the overall character of the centers.

a. Direct connections

Multiple, direct connections from uses within the village center to surrounding arterial circulation should be provided to minimize the importance of any single auto connection, and thereby reduce traffic impacts on village center streets. Whenever possible, direct connections should be provided into the village center from the surrounding village so that village residents have access to the Center without having to use arterial roads. Exhibit 16.5e shows how connections in the North Peak Valley village center might support this guideline.

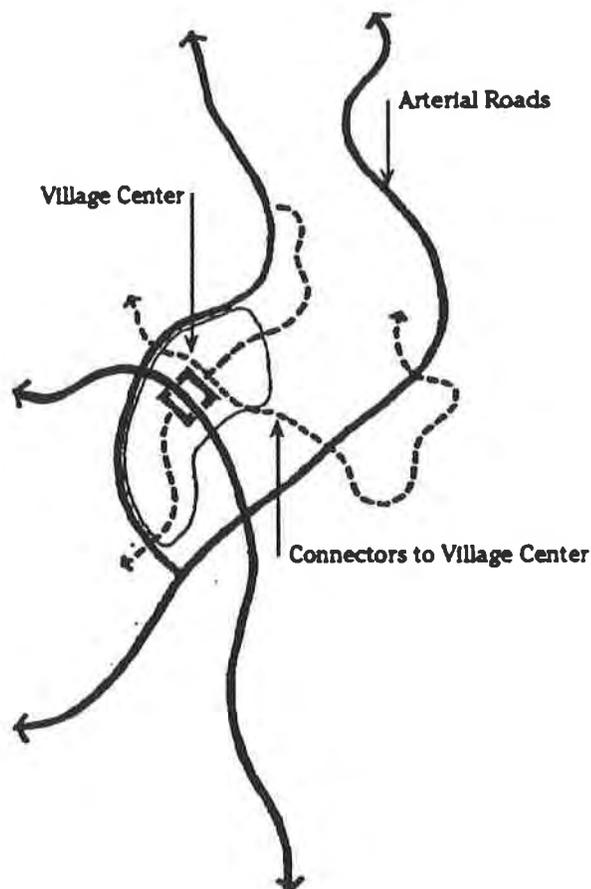


EXHIBIT 16.5e
STREET CONNECTIONS TO VILLAGE CENTER

b. Road Cross-Sections

Within the village center, special consideration will be given to road cross sections. Generally, the paved travel way will be kept as narrow as possible (consistent with safety and parking needs) in order to promote slower speeds, encourage pedestrian activity, and reinforce the more intimate scale of the village center.

c. Design Speeds

Design speeds for roads within the village center (not including arterial roads) should not exceed 20 miles per hour. These slower speeds are intended to make the village centers more attractive to pedestrians. This pedestrian friendly environment will be achieved by decreased building setbacks, lowering traffic speeds, and decreasing curb return radii in the village centers.

d. Intersection radii

The curb return radii of streets at intersections should not exceed 15 feet wherever possible (Exhibit 16.5f). This guideline is intended to make crosswalks more attractive to pedestrians by shortening the cross walk and compelling automobiles to slow before making turns.

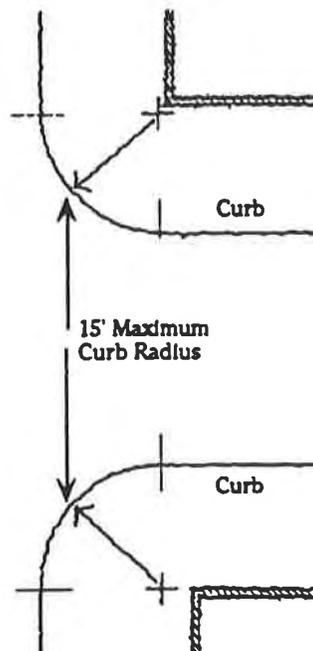


EXHIBIT 16.5f
CURB RADII SHOULD BE SMALL TO ENCOURAGE PEDESTRIANS

16.6

VILLAGE CENTER PARKING LOT GUIDELINES

Adequate parking should be provided within the village center, but recognition of the emphasis on pedestrian access and the mixed use, pedestrian character of the center should be predominant. The objective is to provide enough parking without unnecessarily duplicating parking spaces, or placing undue visual emphasis on parking lots.

OFF-STREET PARKING GUIDELINES

Off-street parking areas are subject to the same spatial structure guidelines as other elements of the village center. As such they should contribute to, rather than detract from the overall scale of the center.

a. Size of off-street lots

The size of the lot should be determined by the scale of the surrounding buildings, as well as the demand for parking spaces. Generally, they should be kept small (less than 200 cars for most parking areas), unless the spatial structure of the surrounding buildings and landscape supports a larger-scaled space.

b. Location of parking lots

Generally, parking areas should be located within several hundred feet of the uses which they are intended to serve. On streets which have uses facing the street (which will be most of the streets in the village center), parking lots should be placed in back of, or to the side of the use, rather than in front of the use (Exhibit 16.6a). The purpose of this criteria is to insure spatial enclosure and continuity along the street. Visually, parking lots should be placed where they do not interfere with the overall character and spatial quality of the center.

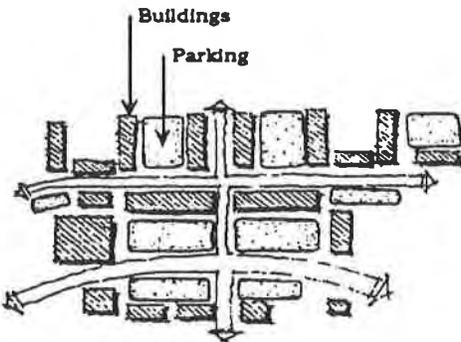


EXHIBIT 16.6A
PARKING IS CONTAINED IN LOTS
SURROUNDED BY BUILDINGS THAT
FORM SPACES

ON-STREET PARKING GUIDELINES

To promote a more traditional Southern California community character within the Village Center, on-street parking on public streets is encouraged, and will be considered when determining satisfaction of parking requirements.

a. Parallel parking

Parallel parking will be permitted on all streets of adequate width.

b. Head-in Parking

Head-in parking from public streets will be permitted on streets with adequate width. Exhibit 16.6d shows head-in parking on Main Street in Lake Elsinore sometime in the 1920's.

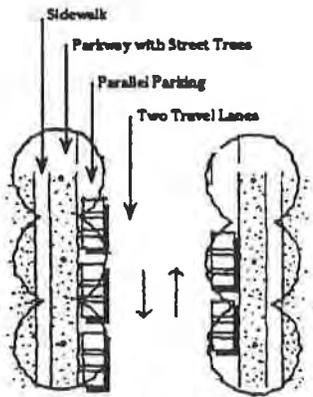


EXHIBIT 16.6b
PARALLEL PARKING STANDARDS

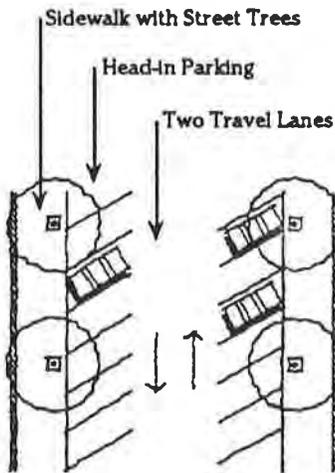


EXHIBIT 16.6c
HEAD-IN PARKING STANDARDS

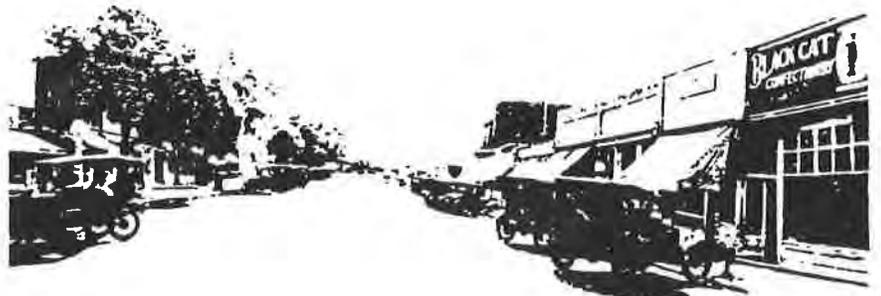


EXHIBIT 16.6d
LAKE ELSINORE COMMERCIAL DISTRICT, CIRCA EARLY 1900's, WITH HEAD-IN PARKING
(USED BY PERMISSION OF THE SMILEY LIBRARY, REDLANDS, CALIFORNIA)

16.7

VILLAGE CENTER ARCHITECTURAL CHARACTER

The architecture of the village center should reinforce the impression of the village center as a central visual element in the community. The following guidelines are intended to assist in the achievement of this objective.

COLLECTION OF RELATED STRUCTURES

Village centers should appear to be collections of small, related structures rather than as a large, complex structure, or a collection of unrelated buildings.

UNIFYING STYLE

To the maximum extent that is reasonable, a unifying architectural style should be applied to each village center. The styles of architecture traditionally found in Southern California communities between 1900 and 1930 include craftsman, spanish colonial revival, and mediterranean eclectic (based primarily on Italian and Spanish precedents).

RICHNESS OF DETAIL

There should be an obvious richness in materials and detail within the village centers. This richness can be expressed in enhanced pedestrian paving, architectural details, street furniture, and other architectural elements. Generally, the areas needing the most attention are the elements that people can actually touch. Exhibit 16.7a is an example of one way richness of detail might be added to a village center.

PROMOTE PEDESTRIAN COMFORT

To encourage pedestrian activity, special consideration should be given to providing amenities that will draw pedestrians to the village center. These amenities may include arcades, courtyards, fountains, seating, and protection from the elements. Exhibit 16.7b is an example of one way pedestrian comfort might be accommodated in the village centers.



EXHIBIT 16.7A
RICHNESS OF DETAIL



EXHIBIT 16.7B
FOUNTAINS ADD RICHNESS AND COOL
COURTYARDS

16.8

VILLAGE CENTER LANDSCAPE CHARACTER

The landscape character of the North Peak community is defined in Section 14.0 and includes a master landscape plan. Generally, the intent of the overall landscape plan is to create a community identity that is typical of Southern California from 1900 to 1930.

REINFORCE VILLAGE CENTER SPATIAL STRUCTURE

Landscape elements can be important contributors to the overall spatial structure of the village center. The placement of trees and other large scale elements, such as palm trees, are especially significant in this respect. Exhibits 16.8a and 16.8c shows how trees might be used to reinforce the spatial structure of the village center.

PROMOTE COMMUNITY LANDSCAPE IDENTITY

Within the village centers, the master landscape plan calls for the "orchard" or "processional" categories of landscape treatment. These treatments are generally formal and permit the use of more water-intensive plant materials that other landscape districts.

PROVIDE APPROPRIATE FURNISHINGS

Signs, street and building lighting, street furniture, and other village center furnishing should be in character with the ultimate style of the buildings.

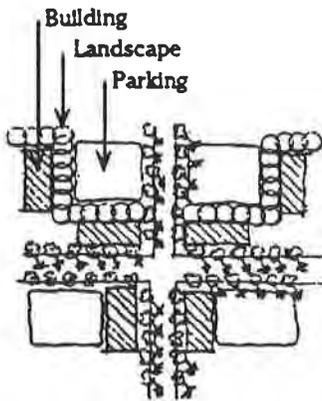


EXHIBIT 16.8A
LANDSCAPE SHOULD REINFORCE
VILLAGE CENTER SPATIAL STRUCTURE



EXHIBIT 16.8B
APPROPRIATE STREET FURNISHINGS



EXHIBIT 16.8C
EUCALYPTUS REINFORCES STREET SPATIAL STRUCTURE IN RANCHO SANTA FE



EXHIBIT 16.8d

THE SOUTHERN CALIFORNIA LANDSCAPE WAS A POPULAR TOPIC FOR ORANGE CRATE LABELS OF THE EARLY TWENTIETH CENTURY

SECTION V: IMPLEMENTATION

17.1**SPECIFIC PLAN REVIEW**

To initiate the implementation of the North Peak Specific Plan the following actions should be taken by the Lake Elsinore City Council:

- **Certify the Environmental Impact Report as adequate and complete for the project area and for the area of annexation;**
- **Annex North Peak to the City of Lake Elsinore;**
- **Amend the City General Plan to reflect the designation of North Peak as a Specific Plan Area;**
- **Pre-zone the project site to Specific Plan District (SPD);**
- **Establish the development intensity of the residential and commercial specific plan districts on the project site;**
- **Approve the North Peak Specific Plan as a nonconditional (SP) specific plan; and**
- **Approve a development agreement between the developer and the City of Lake Elsinore.**

This section contains a description of these steps and other information pertinent to the implementation of the North Peak Specific Plan.

17.2**SPECIFIC PLAN MODIFICATIONS**

Minor modification to the approved North Peak Specific Plan will be allowed at the discretion of the Community Development Director. Modifications to the Specific Plan must be consistent with the purpose and intent of the originally approved Specific Plan. All modifications or amendments to the approved Specific Plan, other than minor changes, will be processed as a specific plan amendment and are subject to the specific plan approval procedures described in Section 17.99 of the Lake Elsinore Zoning Ordinance.

The following modifications constitute minor changes to the approved Specific Plan:

PLANNING UNIT SIZE AND DESIGNATION

Planning units shown on the land use plan (Exhibit 5.1a) contain a land use designation such as SF, GR, PARK, and an acreage. The acreage or designation of a residential planning unit (including manufactured slopes, and collector, local and private streets as applicable) may vary from the designation or acreage specified on the land use plan as long as the total number of units for a particular village is not exceeded by more than 15%, the total number of units for North Peak does not exceed 4,621, and the overall planning objectives of the Specific Plan are maintained, including the objective to place higher density residential uses nearer the center of a village and lower densities at the perimeter.

ALTERNATIVE USES

Alternative uses for school sites will be permitted, if the Lake Elsinore Unified or Perris School District declines to use the specified sites. Alternative uses will be subject to the approval of the Planning Commission and City Council. Any residential dwellings or commercial uses approved for school sites will be in addition to those described by the Specific Plan and may require an amendment to the Specific Plan at the discretion of the City of Lake Elsinore.

ROADWAYS

Minor changes in roadway alignments and street sections are allowed, provided such changes are consistent with the streetscape concept for roads.

PHASING PROGRAM

The phasing program may be modified, provided the objectives of the program continue to be met, and provided that all infrastructure including, but not limited to roads, sewer facilities, water supply, and drainage facilities are available to serve the proposed development. Deviations from the phasing plan will be subject to the approval of the Community Development Director, and if deemed to be a significant change, the Planning Commission. Such changes may not necessarily be considered a specific plan amendment, however.

17.3**FUTURE DEVELOPMENT PROPOSALS**

The following administrative standards apply to the implementation of future development applications (i.e., plot plans, tract maps, or parcel maps) for the North Peak Specific Plan .

- Future tentative or parcel maps and site plan review documents will be consistent with the North Peak Specific Plan.
- Final subdivision or parcel maps will be in substantial conformance with the approved tentative or parcel map as well as the approved site plan documents.
- Building permits for dwelling units will be issued when a final subdivision map has been recorded. Permits may be issued for model units prior to the final map recordation subject to the requirements of the City.

18.1

PHASING CONCEPT

The construction of North Peak will be phased. Although it is not possible to precisely identify every improvement that will be installed in each phase, the following paragraphs describe the likely progression of construction within the community and specify the sequence in which major improvements will be installed. Generally, construction will begin at the southern end of the site, in North Peak Hills and progress north to North Peak Highlands. Total construction of the community will take from ten to twenty years, depending on the demand for housing, interest rates and other factors not directly controlled by the developer. The intent of the phasing plan is to ensure that improvements and services are installed and available as development of the community proceeds.

18.2

PHASE I

Phase I Dwellings: 696 dwelling units

Phase I Commercial Acres: 14 acres

Cumulative Total Dwellings: 696 dwelling units

Cumulative Total Commercial Acres: 14 acres

Phase I will include the area between Highway 74 and the North Peak Hills Village Center. In order to balance cut and fill, however, most of this village will be graded during this phase. El Toro Road will be improved from Highway 74 to the village center, as will the collector road to the east. Several residential areas will be built, open space dedicated, a site for the village school reserved, and the village park completed. Exhibit 18.2a shows diagrammatically the extent of Phase I. The following is a summary of the improvements and other actions to be taken during Phase I. Phase I activities are expected to be completed approximately 2 years following commencement of construction of Phase I.

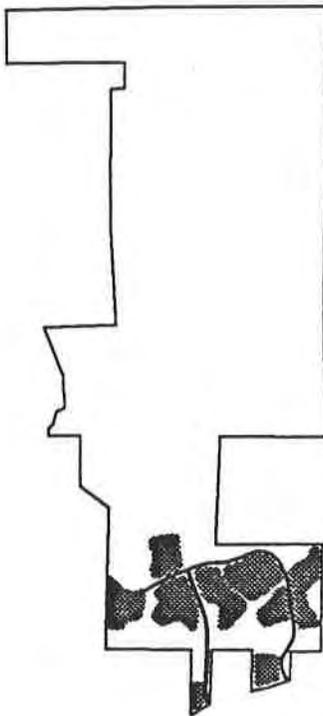


EXHIBIT 18.2A
PHASE I IMPROVEMENTS

Community-wide Improvements Actions

- Create community landscape and lighting district for maintenance of public rights-of-way;
- Form community-wide master home owners association for dedication of open space not owned or maintained by the City;
- Reserve elementary school site in North Peak Hills for Lake Elsinore Unified School District;
- Construct offsite sewer and water lines;
- Build active use park in North Peak Hills (to be constructed by City);

- Build elementary school (to be constructed by school district);
- Widen State Highway 74 to four lanes between Interstate 15 and Conrad Avenue prior to occupancy of any North Peak Development.

Local Improvements Actions

- Provide streets, utilities and drainage facilities within residential and commercial areas;
- Install traffic signal at intersection of Highway 74 and El Toro Road;
- Improve all internal project streets within Phase I area to appropriate roadway standards and install traffic signal when warranted.

18.3

PHASE II

Phase II Dwellings: 575 dwelling units

Phase II Commercial Acres: 5 acres

Cumulative Total Dwellings: 1271 dwelling units

Cumulative Total Commercial Acres: 19 acres

In Phase II, North Peak Hills will become fully developed. El Toro Road will be extended to the intersection with Nichols Road. The village center will be started. Depending on the need for classroom space, the North Peak Hills Elementary School may be built in Phase II, rather than Phase I. Exhibit 18.3a shows the extent of Phase II. The following is a summary of the improvements and other actions to be taken during Phase II. Phase II activities are expected to be completed approximately 4 years following commencement of construction of Phase I.

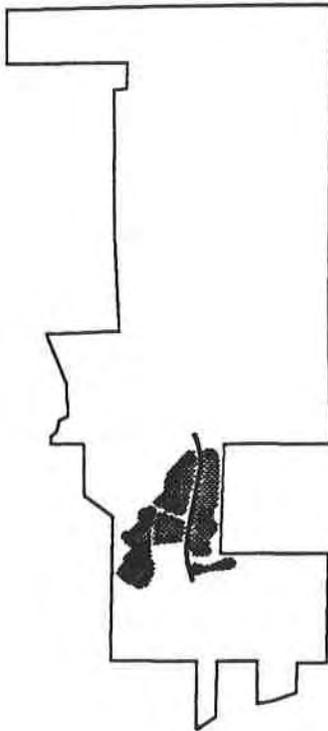


EXHIBIT 18.3A
PHASE II IMPROVEMENTS

Local Improvements/Actions

- Provide streets, utilities and drainage facilities within new subdivisions and commercial areas;
- Improve all internal project streets within Phase II area to appropriate roadway standards and install traffic signal when warranted.

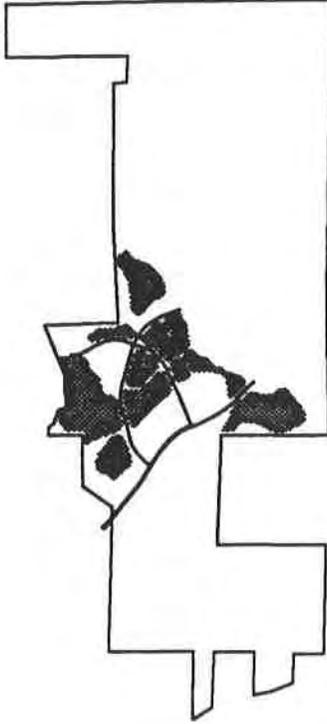


EXHIBIT 18.4A
PHASE III IMPROVEMENTS

18.4

PHASE III

Phase III Dwellings: 1076 dwelling units

Phase III Commercial Acres: 8 acres

Cumulative Total Dwellings: 2347 dwelling units

Cumulative Total Commercial Acres: 27 acres

Development of North Peak Valley will commence in Phase III, including the extension of El Toro Road to the village center, and the construction of the loop road that will eventually serve North Peak Highlands. Additional residential areas will be built, construction of the village center begun and dedication of land for the park and reservation of school site contained in this village will occur. Nichols Road will be built to the edge of the property; the extension of this road will occur prior to the occupancy of 3,000 North Peak dwelling units. Exhibit 18.4a shows the extent of Phase III. The following is a summary of the improvements and other actions to be taken during Phase III. Phase III activities are expected to be completed approximately 4-6 years following commencement of construction of Phase I.

Community-wide Improvements Actions

- Reserve elementary school site in North Peak Valley for Perris School District;
- Build active use park in North Peak Valley (to be constructed by City);
- Build elementary school (to be constructed by school district);
- Widen State Highway 74 to six lanes between Interstate 15 and Dexter Avenue prior to occupancy of 1,700 North Peak residential dwellings.
- Widen State Highway 74 to six lanes between Interstate 15 and Riverside Street prior to occupancy of 2,000 North Peak residential dwellings.

Local Improvements Actions

- Provide streets, utilities and drainage facilities within residential and commercial areas;
- Improve all internal project streets within Phase III area to appropriate roadway standards and install traffic signal when warranted.



EXHIBIT 18.5A
PHASE IV IMPROVEMENTS

18.5

PHASE IV

Phase IV Dwellings: 1268 dwelling units

Phase IV Commercial Acres: 5 acres

Cumulative Total Dwellings: 3614 dwelling units

Cumulative Total Commercial Acres: 32 acres

During Phase IV, development of North Peak Valley will be completed, and North Peak Highlands commenced. Exhibit 18.5a shows the extent of this Phase. The following is a summary of the improvements and other actions to be taken during Phase IV. Phase IV activities are expected to be completed approximately 6-8 years following commencement of construction of Phase I.

Community-wide Improvements/Actions

- Reserve elementary school site in North Peak Valley for Perris School District;
- Build active use park in North Peak Highlands (to be constructed by City);
- Build elementary school (to be constructed by school district);
- Extend Nichols Road between Interstate 15 and the North Peak site prior to occupancy of 3,000 North Peak residential dwellings;
- Construct free westbound turn lane from State Highway 74 to northbound Interstate 15 prior to occupancy of 3,614 dwelling units.

Local Improvements/Actions

- Provide streets, utilities and drainage facilities within residential and commercial areas;
- Improve all internal project streets within Phase IV area to appropriate roadway standards and install traffic signal when warranted.

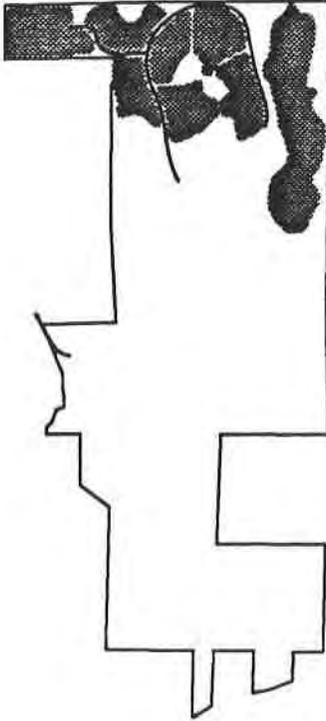


EXHIBIT 18.6A
PHASE V IMPROVEMENTS

18.6

PHASE V

Phase V Dwellings: 1007 dwelling units

Phase V Commercial Acres: 0 acres

Cumulative Total Dwellings: 4621 dwelling units

Cumulative Total Commercial Acres: 32 acres

Phase V will include all development in North Peak Highlands. Exhibit 18.6a shows diagrammatically the extent of Phase V. The following is a summary of the improvements and other actions to be taken during Phase V. Phase V activities are expected to be completed approximately 8-10 years following commencement of construction of Phase I.

Community-wide Improvements/Actions

- Build nature park in North Peak Highlands (to be constructed by City);
- Construct free westbound turn lane from Nichols Road to northbound Interstate 15 prior to occupancy of 4,621 dwelling units.

Local Improvements/Actions

- Provide streets, utilities and drainage facilities within residential and commercial areas;
- Improve all internal project streets within Phase IV area to appropriate roadway standards and install traffic signal when warranted.

18.7

PHASING SUMMARY

The following table is a summary of the phasing at North Peak based on assumptions contained within the Specific Plan.

Anticipated Phasing Schedule

	Commercial Acres		Residential Dwellings							Total Dwellings	Estimated Population
	Village Comm	Hwy Comm	Rural Res	Estate Res	Single Family	Garden Res	Village Res	Multi Family			
Phase I: 0-2 years	0	14	0	25	70	21	0	0	696	2,411	
<i>Cumulative</i>	0	14	0	25	70	21	0	0	696	2,411	
Phase II: 2-4 years	5	0	0	24	27	21	0	7	575	1,724	
<i>Cumulative</i>	5	14	0	49	97	42	0	7	1,271	4,135	
Phase III: 4-6 years	8	0	0	83	48	15	10	7	1,076	3,282	
<i>Cumulative</i>	13	14	0	132	145	57	10	14	2,347	7,417	
Phase IV: 6-8 years	5	0	0	35	100	20	15	7	1,268	3,816	
<i>Cumulative</i>	18	14	0	167	245	77	25	21	3,614	11,233	
Phase V: 8-10 years	0	0	50	143	76	0	0	0	1,007	3,623	
<i>Cumulative</i>	18	14	50	310	321	77	25	21	4,621	14,856	

TABLE 18.7A
ANTICIPATED PHASING

19.1**FINANCING AND MAINTENANCE OF MAJOR SPECIFIC PLAN IMPROVEMENTS**

A facilities financing program is important to implementation of the North Peak Specific Plan. The program should assure the timely financing of public facilities, streets, utilities, and other necessary capital improvements, and should provide for the subsequent maintenance of improvements.

Various techniques are available for the financing of the improvements associated with the development of North Peak. The most appropriate financing mechanism for each particular improvement will involve a multi-step process. First, the City and the developer will agree on the various options from which financing will be chosen. Once the City and developer agree on which options are most viable, these options will be approved as part of the development agreement. At this time, the exact financing method need not be specified, however, the City and the developer must mutually agree to the alternatives.

Some of the most viable funding mechanisms are listed below.

- Impact fees
- Mello-Roos District
- Other forms of assessment districts
- Facilities benefit assessment
- Conventional subdivision financing
- Turnkey construction
- Land reservation, dedication, deeding in fee, or easement
- Hook-up charges
- Reimbursement agreements

Potential operations and maintenance measures include:

- City general fund
- Special assessment or service districts
- Property owners association(s)
- User fees
- Community-contributed service

19.2**COMMUNITY LIGHTING AND OPEN SPACE MAINTENANCE DISTRICT**

The North Peak Lighting and Open Space Maintenance District will be established to maintain public open space areas and provide lighting for streets and public areas. All property owners within the community will be assessed to provide District funding. The District should be created prior to issuance of the first building permit, and will be administered by the City.

As an alternative, North Peak may be annexed into the Lake Elsinore's city-wide Landscaping and Street Lighting District. Under this alternative, North Peak will constitute a separate zone with assessments appropriate to the level of maintenance and amount of improvements to be maintained in the Specific Plan area.

19.3**PROPERTY OWNER ASSOCIATIONS**

A master home owners association will be formed to provide for maintenance of the natural open space, manufactured slopes, and facilities intended for the exclusive use of North Peak residents.

Additional home owners' associations will be established for specific projects when necessary to maintain common areas and facilities intended for use by the residents of that particular project only. This type of association is anticipated for the following projects:

- Multi-family residential projects (such as found in the MF district) with condominium ownership;
- Attached residential projects (such as found in the VR district) with condominium ownership;
- Single family residential projects (such as found in other residential districts) that have common slopes or private recreation facilities.

SECTION VI: REGULATIONS

20.1

SPECIFIC PLAN DISTRICTS

Exhibit 22.1a below shows the Specific Plan districts for North Peak. Each district contains one or more planning unit areas for which the regulations and requirements contained in this section apply.

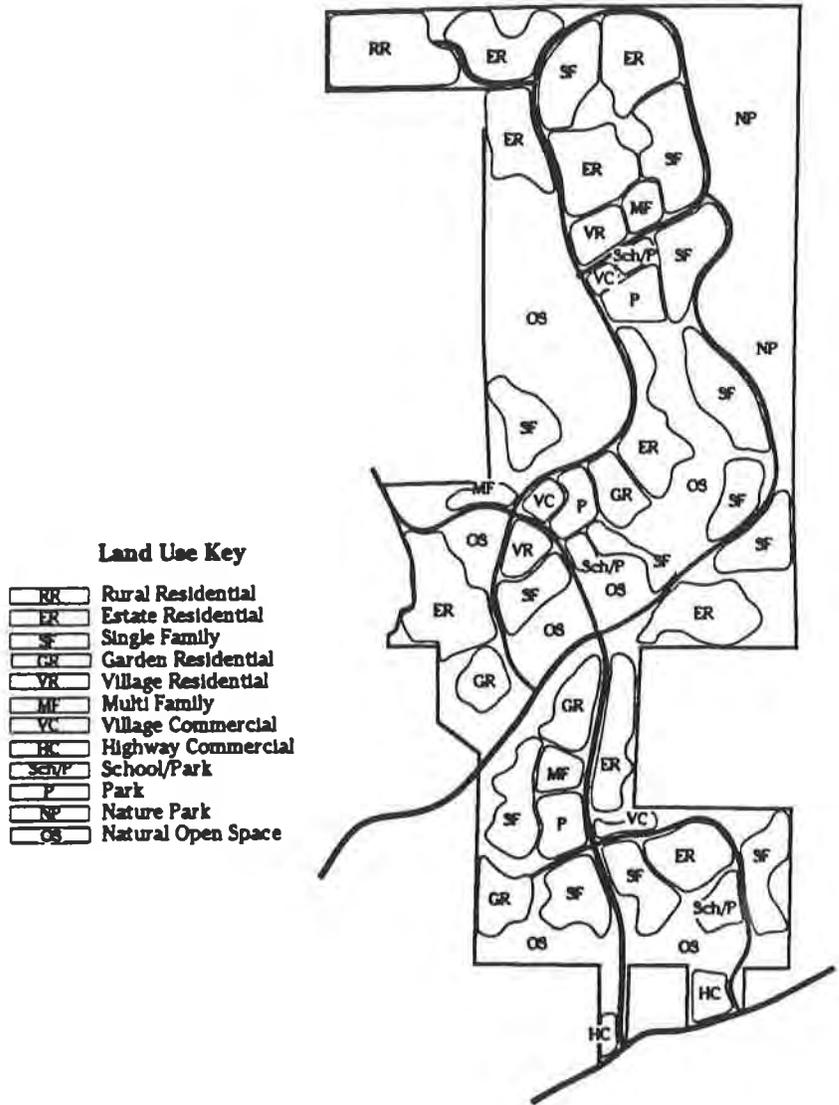


EXHIBIT 21.1A
SPECIFIC PLAN DISTRICTS

21.1**PROCESS AND PROCEDURES**

The order for interpreting questions not spelled out or are not agreed to in the Specific Plan is: Director of Community Development, Planning Commission, then City Council. Any procedural determination may be appealed in the same order.

21.2**CONFLICTING REGULATIONS**

Whenever the regulations contained herein conflict with the City of Lake Elsinore Zoning Ordinance, the regulations contained herein shall take precedence. In the absence of such conflict or in the absence of specific provisions within this code, the requirements of the City's Zoning Ordinance will prevail.

21.3**DWELLING UNIT POOLING**

As the development of the plan occurs, villages may be built out at less than the allocated density, creating a remainder of unbuilt dwelling units. During implementation, the Planning Department shall be responsible for the monitoring and control of these residual dwelling units. The developer may draw upon this accumulated pool for use in other villages, subject to the provision that no village exceed its allocated number of dwellings by more than 15%. It is the express purpose of this provision to permit flexibility in the ultimate distribution of dwelling units while maintaining the total number of dwellings permitted within the Specific Plan at 4,621 dwellings.

21.4**CONTINUED AGRICULTURAL USES**

Legally existing agricultural uses, including structures accessory thereto, shall be permitted to continue within the planning area, subject to the provisions of the Lake Elsinore City Zoning Code.

21.5

MODEL HOMES

Model homes and their garages and private recreation facilities may be used as offices for the first sale of homes within a recorded tract subject to the regulations of the City of Lake Elsinore governing these uses and activities. The garages shall be reconverted prior to sale of the model homes.

22.1

GENERAL TO ALL RESIDENTIAL DISTRICTS

A. SINGLE AND MULTIPLE LOT SUBDIVISIONS

Single and multiple lot subdivisions, also known as conventional and cluster development, respectively, are permitted in all residential districts.

B. BUILDING SETBACKS FROM COMMON AREAS:

Structures which abut a plaza, park, mall, greenbelt or other permanent open space may abut the common property line if they have no openings onto these spaces.

C. FENCES, HEDGES AND WALLS:

(1) Visual barriers shall be limited to a maximum height of six feet. Height of visual barriers within residential front setback areas shall not exceed forty-two inches, except that visual barriers may exceed forty two inches if they are set back from the front property line a distance equal to the dwelling, garage, or carport setback, but not less than ten feet. This shall exclude the planting of trees in parkway, median or front yard areas.

(2) Fences, hedges and walls constructed as acoustical barriers shall have no height limit.

D. TRELLISES:

Open trellis and beam construction shall be permitted to attach the garage or carport to the dwelling and may also extend from the dwelling to the property line in the side, rear yards or front yards, and will not be included in the calculation of the building coverage.

E. TEMPORARY USES PERMITTED:

(1) Model homes, temporary construction offices, temporary real estate offices, and signs.

(2) Continued use of an existing building during construction of a new building on the same building lot.

F. PROJECTIONS INTO YARDS

(1) Roof projections may extend into a required side yard a maximum of fifty percent of the yard's width, not to exceed two feet, and may extend into a required front or rear yard a maximum of four feet.

(2) Greenhouse and bay windows, planting boxes, and fire place structures may project a maximum of two feet into required yard setbacks.

(3) Staircases, balconies, and other similar structural features may not project more than fifty percent into any required front, rear, or side yard setback, but in no case shall distance exceed four feet.

22.2

RURAL RESIDENTIAL (RR)

Land designated as Rural Residential is intended for residential development that has a density of one dwelling unit or less per residential acre. The following regulations apply:

A. USES PERMITTED:

- (1) Single family detached dwellings
- (2) Open space, parks, trails
- (3) Private recreation facilities and buildings

B. ACCESSORY USES PERMITTED:

- (1) Garages and carports, in compliance with site development standards provided herein
- (2) Fences, walls, trellises
- (3) Swimming pools
- (4) Accessory uses and structures necessary or incidental to a principal use

C. USES SUBJECT TO A CONDITIONAL USE PERMIT

- (1) Large or small family day care homes
- (2) Second units
- (3) Keeping of exotic birds or animals, or more than six weaned dogs and/or cats
- (4) Agricultural or horticultural uses for commercial purposes
- (5) Parochial or private schools

D. SITE DEVELOPMENT STANDARDS:

- (1) Building lot area: 15,000 square feet, minimum
- (2) Building lot width: 80 feet, minimum
- (3) Building coverage: 50%, maximum
- (4) Building setbacks: front yard, 30 feet, minimum; side yard,

10 feet, minimum; rear yard, 30 feet, minimum

(5) Building height: 35 feet, maximum

22.3

ESTATE RESIDENTIAL (ER)

Land designated as Estate Residential is intended for residential development that has a density of 2 to 4 dwelling units per net residential acre. The following regulations apply:

A. USES PERMITTED:

- (1) Single family detached dwellings
- (2) Open space, parks, trails
- (3) Private recreation facilities and buildings

B. ACCESSORY USES PERMITTED:

- (1) Garages and carports, in compliance with site development standards provided herein
- (2) Fences, walls, trellises
- (3) Swimming pools
- (4) Accessory uses and structures necessary or customarily incidental to a principal use

C. USES SUBJECT TO A CONDITIONAL USE PERMIT

- (1) Large or small family day care homes
- (2) Second units
- (3) Keeping of exotic birds or animals, or more than six weaned dogs and/or cats
- (4) Agricultural or horticultural uses for commercial purposes
- (5) Parochial or private schools

D. SITE DEVELOPMENT STANDARDS:

- (1) Building lot area: 8,000 square feet, minimum
- (2) Building lot width: 60 feet, minimum
- (3) Building coverage: 50%, maximum
- (4) Building setbacks: front yard, 20 feet, minimum; side yard, 5 feet, minimum; rear yard, 20 feet, minimum
- (5) Building height: 35 feet maximum

22.4**SINGLE FAMILY (SF)**

Land designated as Single Family is intended for residential development that has a density of 4 to 7 dwelling units per net residential acre. The following regulations apply:

A. USES PERMITTED:

- (1) Single family detached dwellings
- (2) Zero lot line dwellings
- (3) Open space, parks, and trails
- (4) Private recreation facilities and Buildings

B. ACCESSORY USES PERMITTED:

- (1) Garages and carports, in compliance with site development standards provided herein
- (2) Fences, walls, trellises
- (3) Swimming pools
- (4) Accessory uses and structures necessary or customarily incidental to a principal use

C. USES SUBJECT TO A CONDITIONAL USE PERMIT

- (1) Large or small family day care homes
- (2) Second units
- (3) Keeping of exotic birds or animals, or more than six weaned dogs and/or cats
- (4) Agricultural or horticultural uses for commercial purposes
- (5) Parochial or private schools

C. SITE DEVELOPMENT STANDARDS:

- (1) Building lot area: 5,000 square feet, minimum; it is intended that residential lots in this district vary from 5,000 to 7,200 square feet in size (see Conditions of Approval #16)
- (2) Building lot width: 50 feet, minimum; it is intended that residential lots in this district vary in width depending on lot size
- (3) Building coverage: 60%, maximum
- (4) Building setbacks: front yard, 5 feet, minimum, if automatic garage door openers are provided; otherwise, 18 feet from the back of sidewalk to a front entry garage or 10 feet to a side entry garage; side yard, zero feet or at least 10 feet, provided that the combined setbacks from both the side property lines on any

building lot shall total not less than ten feet.; rear yard, zero feet, except that vehicular access to a garage or carport shall have a minimum of 24 feet of clear space directly behind the garage or carport to provide adequate back-up room

(5) Building height: 35 feet maximum

22.5

GARDEN RESIDENTIAL (GR)

Land designated as Garden Residential is intended for residential development that has a density of 7 to 12 dwelling units per net residential acre. The following regulations are applicable for these areas:

A. USES PERMITTED:

- (1) Single family detached dwellings
- (2) Single family attached dwellings
- (3) Zero lot line dwellings
- (4) Private recreation facilities and buildings
- (5) Open space, parks, and trails

B. ACCESSORY USES PERMITTED:

- (1) Garages and carports, in compliance with site development standards provided herein
- (2) Fences, walls, trellises
- (3) Swimming pools
- (4) Accessory uses and structures necessary or customarily incidental to a principal use

C. USES SUBJECT TO A CONDITIONAL USE PERMIT

- (1) Large or small family day care homes
- (2) Second units
- (3) Keeping of exotic birds or animals, or more than six weaned dogs and/or cats.
- (4) Horticultural uses for commercial purposes.
- (5) Parochial or private schools
- (6) Convalescent, retirement, rest homes, sanitariums or similar congregate care facilities

D. SITE DEVELOPMENT STANDARDS, DETACHED DWELLINGS:

- (1) Building lot area: 3,000 square feet, minimum
- (2) Building lot width: 35 feet, minimum

- (3) Building coverage: 60%, maximum
- (4) Building setbacks: front yard, 5 feet, minimum, if automatic garage door openers are provided; otherwise, 18 feet from the back of sidewalk to a front entry garage or 10 feet to a side entry garage; side yard, zero feet or at least 10 feet, provided that the combined setbacks from both the side property lines on any building lot shall total not less than ten feet.; rear yard, zero feet, except that vehicular access to a garage or carport shall have a minimum of 24 feet of clear space directly behind the garage or carport to provide adequate back-up room
- (5) Building height: 35 feet maximum
- (6) Landscape: landscape and underground irrigation shall be installed in tracts where the minimum lot size is less than 5,000 square feet.

E. SITE DEVELOPMENT STANDARDS, ATTACHED DWELLINGS:

- (1) Building lot area, Attached Dwellings: Three acres, except in the Village Center Overlay District where there is no minimum.
- (2) Building lot width: no minimum requirement
- (3) Building lot coverage, 70%, maximum
- (4) Building setbacks: as provided in Section 24.8, except that no dwelling or other main building, one, two or three stories in height shall be closer than 10 feet to any other dwelling or main building
- (5) Building height: 35 feet, maximum
- (6) Landscape: Required for common areas and parking lots
- (7) Private Open Space: 100 square feet per dwelling, minimum

22.6

VILLAGE RESIDENTIAL (VR)

Land designated as Village Residential is intended for residential development that has a density of 12 to 16 dwelling units per net residential acre. If these densities are not achieved lower density single family housing may be built. The following regulations are applicable for these areas:

A. USES PERMITTED:

- (1) Single-family dwellings - attached or detached, including, but not limited to townhouses, triplexes, fourplexes and condominiums
- (2) Multiple-family dwellings, including, but not limited to,

apartment projects, condominium projects, and cooperative apartment projects

(3) Recreation facilities and buildings for use by the residents of the project or specified surrounding residents, which may include, but are not limited to, clubhouses, community swimming pools, tennis courts, and other similar recreational uses or operations

(4) Open space, parks, and trails

B. ACCESSORY USES PERMITTED:

(1) Garages and carports, in compliance with site development standards provided herein

(2) Fences, walls, trellises

(3) Swimming pools

(4) Accessory uses and structures necessary or customarily incidental to a principal use

C. USES SUBJECT TO A CONDITIONAL USE PERMIT

(1) Large or small family day care homes

(2) Second units

(3) Keeping of exotic birds or animals, or more than six weaned dogs and/or cats

(4) Horticultural uses for commercial purposes

(5) Parochial or private schools

(6) Convalescent, retirement, rest homes, sanitariums or similar congregate care facilities

(7) Private, non commercial clubs

D. SITE DEVELOPMENT STANDARDS:

(1) Building lot area: Three acres, except in the Village Center Overlay District where there is no minimum

(2) Building lot width: no minimum requirement

(3) Building lot coverage, 70%, maximum

(4) Building setbacks: as provided in Section 24.8, except that no dwelling or other main building, one, two or three stories in height shall be closer than 10 feet to any other dwelling or main building.

(5) Building height: 45 feet, maximum

(6) Landscape: Required for common areas and parking lots

(7) Private Open Space: 50 square feet per dwelling, minimum

22.7

MULTIFAMILY (MF)

Land designated as Multifamily is intended for residential development that has a density of 16 to 24 dwelling units per net residential acre. The following regulations are applicable for these areas:

A. USES PERMITTED:

- (1) Multiple-family dwellings,
- (2) Private recreation facilities and buildings
- (3) Open space, parks, and trails

B. ACCESSORY USES PERMITTED:

- (1) Garages and carports, in compliance with site development standards provided herein
- (2) Fences, walls, trellises
- (3) Swimming pools
- (4) Accessory uses and structures necessary or customarily incidental to a principal use

C. USES SUBJECT TO A CONDITIONAL USE PERMIT

- (1) Large or small family day care homes
- (2) Second units
- (3) Keeping of exotic birds or animals, or more than six weaned dogs and/or cats.
- (4) Horticultural uses for commercial purposes.
- (5) Parochial or private schools
- (6) Convalescent, retirement, rest homes, sanitariums or similar congregate care facilities.
- (7) Private, non commercial clubs

D. SITE DEVELOPMENT STANDARDS:

- (1) Building lot area: Three acres, except in the Village Center Overlay District where there is no minimum.
- (2) Building lot width: no minimum requirement.
- (3) Building lot coverage, 70%, maximum
- (4) Building setbacks: as provided in Section 24.8, except that no dwelling or other main building, one, two or three stories in height shall be closer than 10 feet to any other dwelling or main

building.

(5) Building height: 45 feet, maximum; for towers, cupolas, and other building elements which are uninhabitable and have a floor area of less than 250 square feet, 65 feet.

(6) Landscape: Required for common areas and parking lots

(7) Private Open Space: no minimum.

22.8

SETBACKS FOR ATTACHED AND MULTIFAMILY DWELLINGS

The following table is intended to provide minimum standards for setbacks within North Peak.

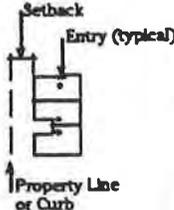
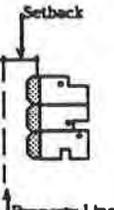
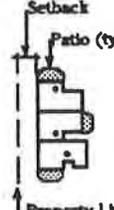
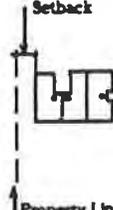
Setback Standards	Frontyard	Rearyard	Rearyard	Sideyard
Garden Residential Village Residential Multifamily Residential				
ADJACENT USE				
Residential or Commercial	10' minimum, 15 feet typical	15' minimum	10' minimum	5' minimum
Open Space	10' minimum	Patio Fence may abut Property Line	5' minimum	5' minimum
Local Public or Private Street	10' minimum	15' minimum	10' minimum	5' minimum
Arterial or Secondary Arterial Street				
with fence or wall at PL	10' minimum, 15 feet typical	25' minimum	25' minimum	20' minimum
without fence or wall at PL	20' minimum	25' minimum	25' minimum	20' minimum

TABLE 22.8A
RESIDENTIAL SETBACKS

23.1**GENERAL PROVISIONS**

The following general provisions are intended to apply to all commercial districts within North Peak. However, these standards shall take precedence only when a specific district regulation does not specify greater or more restrictive criteria.

A. TEMPORARY USES

The following temporary uses may be permitted upon review and conditional approval by the Director of Community Development:

- (1) Temporary on-site construction offices/facilities
- (2) Continued use of an existing building during site preparation or construction of a new building
- (3) On-site leasing and sales offices
- (4) On-site real estate signs and future development signs in conformance with the Section 28.0, "Sign Regulations"

The above uses shall be approved for a specified period of time, not to exceed one year, and a performance bond may be required to remove any structures at the end of the approved period of use.

B. OPERATION IN GENERAL

All commercial uses required to be operated in a completely enclosed building shall be operated in such a way as to produce no objectionable noise or odors outside its own building's walls.

C. LIGHTING

Due to North Peak's proximity to the Mount Palomar Observatory, outdoor lighting should be oriented and shielded to prevent direct illumination above the horizontal plane passing through the luminaire, and to prevent any glare or direct illumination on adjacent properties or streets.

D. LANDSCAPING

New non-residential development shall have all required landscaping installed at the time of occupancy, and said landscaping shall be provided with irrigation systems which comply with standards described in Section 27.0, "Landscape Regulations".

All landscaping and irrigation systems shall be maintained in good condition for as long as the use on the property continues. All developments shall be responsible for landscaping and maintenance of adjacent parkways, unless other provisions have been made.

E. FENCES AND WALLS

Where required, an opaque fence or wall shall be a minimum of six feet as measured from the highest grade elevation on either side of the fence or wall. The following exceptions shall apply:

- (1) In required front yards the maximum height of a fence or wall shall be forty-two inches.
- (2) Along a street identified in the General Plan Circulation Element as a Modified Collector or larger, the Planning Commission, pursuant to Design Review approval, may require a perimeter wall to be eight feet in height or higher.

Nothing within this Section is intended to preclude the Planning Commission, pursuant to Section 29.0, "Design Review," from requiring the design of a specific fence or wall to accommodate a particular situation or condition.

F. SELF-SERVE DEVICES

Self-serve devices, including, but not limited to, newspaper racks, ice machines, and collection containers for recyclable material, may be permitted subject to approval by the Director of Community Development prior to installation. In approving the location of said devices the Director shall ensure that the device:

- (1) Complies with all other City codes and ordinances
- (2) Will not interfere with pedestrian and vehicular circulation patterns
- (3) Will not encroach into a public right-of-way
- (4) Will not encroach into required parking, setback, or landscape areas

G. GASOLINE DISPENSING ESTABLISHMENTS

The provisions of this Section shall apply to all new service

stations and other places where motor vehicle fuels are dispensed to the public.

(1) Site

All new sites for gasoline dispensing establishments shall have a minimum net lot area of 15,000 square feet and a frontage of 125 feet on any street having a driveway.

(2) Pumps

All gasoline pumps and pump islands upon which they are placed shall be set back a minimum of thirty feet from any property line.

(3) Activities

The following activities may be permitted: Dispensing of gasoline, diesel fuels, oil, grease, tires, batteries, and replacement parts and installation of the items enumerated.

Heavy engine or transmission repair to include replacement shall not be permitted in a service station established pursuant to this Chapter. Convenience stores, mini-marts, car washes whether automatic or manual shall be permitted in conjunction with a service station subject to approval of a conditional use permit. All such uses shall be conducted within a completely enclosed building.

(4) Parking

Vehicles shall not be parked on the premises other than in designated parking spaces. No overnight parking shall be permitted except for vehicles under repair when maintained within a fully enclosed building. When a mini-mart or convenience store is operated in conjunction with the gasoline station, a minimum of five parking spaces shall be provided and conveniently located to serve store customers. All car wash installations shall provide adequate stacking distance for a minimum of four vehicles at the entry of the facility. Parking areas for air and water servicing, drying and vacuuming shall be clearly provided.

(5) Design

Except as otherwise provided in this Section, Gasoline Dis-

pending Establishments shall comply with the design concepts and standards contained in this Chapter and within the Individual District Chapters as required for all other commercial businesses. Design review shall be required for Gasoline Dispensing Establishments.

(6) Rest rooms

Men's and women's rest rooms shall be provided and made available to customers. Separate facilities shall be provided for each sex and maintained open to the public during business hours and kept in a sanitary and working condition.

(7) Sale of Alcoholic Beverages

Establishments engaged in the concurrent sale of motor vehicle fuel with alcoholic beverages shall abide by the following requirements as a condition pursuant to the provisions of the Conditional Use Permit Section of the Lake Elsinore Zoning Code:

- (a) No beer or wine shall be displayed within five feet of the cash register or the front door.
- (b) No advertisement of alcoholic beverages shall be displayed at motor fuel islands.
- (c) No sale of alcoholic beverages shall be made from a drive-in window.
- (d) No display or sale of beer or wine shall be made from an ice tub.
- (e) No beer or wine advertising shall be located on motor fuel islands and no self-illuminated advertising for beer or wine shall be located on buildings or windows.
- (f) Employees on duty between the hours of 10:00 P.M. and 2:00 A.M. shall be at least twenty-one years of age to sell beer and wine.

H. AUTOMOBILE STORAGE

Automobiles parked in any required parking space shall be drivable, clearly operational in general, and have a current license and registration.

I. TRASH ENCLOSURES

Trash enclosures shall be constructed pursuant to the City's standard design on file with the Department of Building and Safety. Said enclosures shall be provided based on a minimum ration of one, three-cubic-yard container enclosure for each:

- (1) 5,000 square feet of total floor area for units up to 20,000 square feet
- (2) 10,000 square feet of total floor area for units larger than 20,000 square feet and up to 50,000 square feet
- (3) 15,000 square feet of total floor area for units larger than 50,000 square feet

In determining the number of enclosures to be provided, all resultant numbers shall be rounded up to the next whole integer.

Enclosures shall be placed in a location convenient to the unit(s) it is intended to serve and in no case shall an enclosure be located further than one-hundred feet from the associated unit(s). A decorative trellis work shall be incorporated into the design of any enclosure located within 25 feet of a two-story building having windows overlooking the enclosure.

J. MAILBOXES

When utilized, common-collection mailboxes within a commercial complex shall be installed pursuant to the design standards on file with the Division of Building and Safety. The location of all mailboxes shall be reviewed with the Planning Division prior to installation. Common-collection mailboxes should be located in decorative housings which incorporate similar materials, colors, and textures utilized on the building within the complex. The area for the mailbox should be of sufficient size to enable patrons to pick up or deposit mail without having to encroach upon landscaping, walks, or parking. The housing for the mailbox should offer patrons protection from the elements.

K. CIRCULATION PATTERNS

Circulation patterns within commercial and industrial districts shall be designed to minimize the number of curb cuts required.

Curb cuts should be located as far as possible from intersections and whenever possible be aligned with others on the opposite side of the street.

23.2

VILLAGE COMMERCIAL DISTRICT

The Intent of the Village Commercial District is to provide locations for general retail and office uses which offer the sale of goods and services to the general public and which, through characteristics of their operation, serve primarily the day-to-day shopping needs of local Village residents. The Village Commercial District is not intended for those uses which, because of their size or nature of operation, generate vehicular or truck traffic beyond that normally associated with a neighborhood use.

A. PERMITTED USES

Uses permitted in the Village Commercial District shall include those businesses listed below which operate in compliance with the intent and standards of this district and are conducted entirely within a completely enclosed building. Each business shall be evaluated in terms of its operational characteristics and specific site location.

(1) Service Uses

- (a) Accountants, advertising agencies, appraisers, attorneys, business and management consultants, economists, public relations consultants, and other professional offices
- (b) Administrative or executive offices of any type of business
- (c) Architects, landscape architects, planners, engineers and surveyors, geologists, industrial designers, graphic designers, and interior designers not including retail sales on the premises
- (d) Art and craft galleries or studios
- (e) Barber shops and beauty salons
- (f) Employment agencies, travel agencies, and airline ticket agencies
- (g) Financial institutions, including banks, savings and loan associations, finance companies and credit unions
- (h) Government buildings and service facilities; excluding storage equipment or repair or warehouses
- (i) Insurance brokers and services, investment brokers, real

estate brokers and offices, and title and escrow companies.

- (j) Oculists, opticians, and optometrists
- (k) Office supply and stationery stores
- (l) Photography studio
- (m) Prescription pharmacies
- (n) Quick copy and printing establishments
- (o) Schools and studios for arts, crafts, photography, music and dance
- (p) Other uses consistent with the intent of this district

(2) Retail Uses

- (a) Apparel stores
- (b) Appliance stores
- (c) Bicycle shops
- (d) Food stores; including markets, bakeries, and health food establishments, candy stores
- (e) Florists
- (f) General merchandise stores
- (g) Hardware stores
- (h) Health and exercise clubs
- (i) Hobby supply stores
- (j) Jewelry stores
- (k) Media shops; including bookstores, newsstands, and video tape outlets
- (l) Music stores; including sales of instruments, records, and tapes
- (m) Personal service establishments; including dry cleaning and tailors
- (n) Pet shops; retail sales and grooming only. No boarding of animals
- (o) Schools for dance and music
- (p) Service establishments; such as small appliance repair, watch and jewelry repair, and shoe repair
- (q) Sporting goods stores
- (r) Toy shops
- (s) Vehicle parts sales; (new or rebuilt only) and excluding repair and service
- (t) Other uses consistent with the intent of this district

B. USES SUBJECT TO A CONDITIONAL USE PERMIT

Certain uses, while similar to the permitted uses, may adversely

affect surrounding properties and require additional consideration. Such uses shall require a conditional use permit pursuant to the Lake Elsinore Zoning Ordinance and include the following:

- (1) Automatic car washes: provided they shall be located a minimum of two hundred feet from any residential use
- (2) Bars or cocktail lounges: not in conjunction with a restaurant
- (3) Churches
- (4) Drive-through or drive-in establishments; provided a safe and efficient circulation system can be provided completely on-site
- (5) Game arcades; includes any establishment having five or more mechanical or electronic games of chance, skill or entertainment, whether as the primary use or in conjunction with another business, but excluding vending machines dispensing a product for sale
- (6) Gasoline dispensing establishments, subject to the provisions of Section 22.0, "General Regulations"
- (7) Hotels
- (8) Motels
- (9) Private clubs and lodges
- (10) Restaurants and eating places with outside eating areas
- (11) Small animal veterinary clinics subject to the following addition to all other conditions of the Use Permit: (a) Treatment of animals is restricted to dogs, cats and other small domesticated animals and birds; (b) the operation shall be conducted in a completely enclosed and sound controlled building in such a way as to produce no objectionable noises or odors outside its walls; (c) there shall be no outdoor runs or animal holding areas; (d) there shall be no boarding of animals other than as necessary for recuperation of patients; and (e) the clinic shall have direct access from the parking area

C. SITE DEVELOPMENT STANDARDS

(1) Parcel Size

There shall be no minimum parcel size, providing it can be demonstrated that the property is capable of compliance with the following criteria:

- (a) The site can support safe and efficient on-site circulation and has convenient access to surface streets with adequate capacity.
- (b) There is sufficient parcel size to ensure adequate accommodation of adjacent uses.

(2) Setbacks

Front - 0' minimum.

Side - 0' minimum.

Rear - 0' minimum.

(3) Lot Area

5,000 SF minimum.

(4) Frontage

30' minimum.

(5) Building Height

The maximum building height shall be thirty-five feet, except for architectural features such as towers, cupolas, and other elements that contribute to the overall character of the community, do not contain usable space, and have a footprint area of less than 250 square feet.

(6) Landscape Improvements

The provisions of Section 27.0, "Landscape Regulations" shall be used to determine the landscape requirements for development in the Village Commercial District.

(7) Parking

The provisions of Section 26.0, "Parking Regulations" shall be used to determine the parking for development in the Village Commercial District.

(8) Signs

The provisions of Section 28.0, "Sign Regulations" shall be used to determine the parking for development in the Village Commercial District.

23.3**HIGHWAY COMMERCIAL DISTRICT**

The intent of the Highway Commercial District is to provide for a full range of retail stores, offices, personal and business service establishments offering commodities and services scaled to meet the needs of the residents of the entire city. Because of the intensity of use associated with the Highway Commercial District, properties assigned this designation are located on Highway 74.

A. PERMITTED USES

Uses permitted in the Highway Commercial District shall include those businesses listed below which operate in compliance with the intent and standards of this district and are conducted entirely within a completely enclosed building. Each business shall be evaluated in terms of its operational characteristics and specific site location.

(1) All permitted uses of the Village Commercial District as contained in Section 24.0 "Commercial Development Regulations."

(2) Antique shops and auction galleries

(3) Bowling alleys

(4) Bus depots and transit stations

(5) Department stores

(6) Floor covering shops

(7) Furniture stores

(8) Home improvement centers

(9) Hotels

(10) Motels

(11) Sale of motor vehicle, motorcycle and recreational vehicle parts and accessories and service incidental to the sale of parts

(12) Skating rinks

(13) Second hand and thrift shops

(14) Service businesses similar to retail stores which do not involve warehousing or storage, except accessory storage of commodities sold at retail on the premises

(15) Theaters

(16) Light industrial uses

(17) Business park uses

(18) Other uses consistent with the intent of this district

B. USES SUBJECT TO A CONDITIONAL USE PERMIT

Certain uses, while similar to the permitted uses, may adversely affect surrounding properties and require additional consideration. Such uses shall require a conditional use permit pursuant to the Zoning Ordinance and include the following:

- (1) Uses permitted subject to a conditional use permit in the Village Commercial District.
- (2) Business colleges and professional schools
- (3) Car washes
- (4) Churches
- (5) Dance halls, discotheques, or any establishment providing live entertainment
- (6) Motor vehicle, motorcycle and recreational vehicle sales; and service incidental to the sale of parts but excluding major overhauls, painting, and body work
- (7) Outdoor sales and display incidental and accessory to a permitted use

C. SITE DEVELOPMENT STANDARDS***(1) Lot Area***

The minimum lot area for lots in the Highway Commercial District shall be 25,000 square feet.

(2) Street Frontage Width

The minimum street frontage width on an arterial road in the Highway Commercial District shall be 100 feet.

(3) Setbacks

The following minimum standards shall apply to all new construction in the Highway Commercial District:

- (a) Front: the front yard setback for any building shall average twenty feet but in no case shall be less than fifteen feet.
- (b) Side and rear yard: no setback shall be required from interior lot lines except adjacent to a public right-of-way or a residential use, in which case the minimum setback shall be fifteen feet.

(4) Building Height

The maximum building height shall be forty-five feet, except for architectural features such as towers, cupolas, and other elements that contribute to the overall character of the community, do not contain usable space, and have a footprint area of less than 250 square feet.

(5) Landscape Improvements

The provisions of Section 27.0, "Landscape Regulations" shall be used to determine the landscape requirements for development in the Highway Commercial District.

(6) Parking

The provisions of Section 26.0, "Parking Regulations" shall be used to determine the parking requirements for development in the Highway Commercial District.

(7) Signs

The provisions of Section 28.0, "Sign Regulations" shall be used to determine the sign requirements for development in the Highway Commercial District.

24.1

INTRODUCTION

The Village Center Overlay District has been established with the assumption that planning for compact, integrated areas within North Peak that have a balance of residential and non-residential uses, a range of housing densities and short trips between housing, jobs and shopping opportunities will achieve efficient development patterns and support an attractive quality of life.

24.2

INTENT

It is the intent of this District to provide for those design features, roadway elements and land use relationships not normally permitted within the existing Zoning Code or in other Districts in the North Peak Specific Plan, such as:

- (1) Retail, offices and dwellings located within the same structure;
- (2) Civic and/or institutional uses such as public buildings, day care centers, religious buildings, parks, playgrounds or post offices, etc., to be in close proximity and directly adjacent to retail, offices and/or residential uses;
- (3) On-street parking in both parallel and diagonal configurations as well as shared parking allowances within the Village Center;
- (4) Narrower pavement widths for streets, including smaller curb radii at intersections;
- (5) Angled intersections at special locations for streets and roads;
- (6) Alleyways;
- (7) Vehicular rotaries and "town squares";
- (8) Purposeful mixing of pedestrian and vehicular use;
- (9) Accessory use buildings and garage apartments;
- (10) Variety and mixing of various housing types and lot sizes in close proximity and directly adjacent to each other
- (11) Varied building setbacks which provide flexibility, variety and visual interest and can be used to establish the pedestrian scale of streets through shallow front yards, front fences, porches, outdoor cafes, walks and open-air pavilions for shops.

24.3**LAND USE MIX**

The design and implementation of the village center is expected to address the unique features of each village. The proposed village centers shall conform with the amount of land area shown on the land use plan. The distribution of those land uses may vary, however, from the precise location shown on either the land use plan (Exhibit 5.1a) or the land use district map (Exhibit 21.1a).

24.3**LAND USE WITHIN THE VILLAGE CENTER****A. LOCATION OF USES**

Similar Buildings and Use Types of similar intensities shall generally either abut each other or face one another across streets. Dissimilar use types shall generally abut along rear property lines and at street intersections.

B. PERMITTED USES

All uses permitted within the underlying Districts are permitted within the Village Center Overlay District.

C. USES PERMITTED WITH CONDITIONAL USE PERMIT

- (1) Limited Lodging use, such as "Bed and Breakfast Inns"
- (2) Artisanal use
- (3) Elderly care and social care facilities
- (4) Guest cottages
- (5) Home Offices

D. PROHIBITED USES

The following uses are prohibited in the Village Center District.

- (1) Chemical manufacturing, storage or distribution, as a primary use
- (2) Any Commercial use which is primarily directed to patrons who are encouraged by the site layout, or buildings, to remain in their automobile while receiving goods or services except automobile service stations

- (3) Enameling, plating or painting except artist studios as a primary use
- (4) Outdoor advertising or billboard as a principal use or any outdoor advertising for a use not in the Village Center
- (5) Foundries (except small sculpture studio operations such as a guild or artisanal use), carting, express, moving or hauling terminal or yard, except delivery or pick up of goods or merchandise solely to service businesses in North Peak
- (6) Prisons, detention centers or "half-way houses" associated with prisons or penitentiaries
- (7) The manufacture or disposal of hazardous waste materials
- (8) The manufacture or disposal of radioactive waste
- (9) Agricultural uses, other than home gardens or community gardens
- (10) Scrap yards
- (11) Commercial sand, gravel or other mineral extraction
- (12) Outdoor Storage
- (13) Outdoor Commercial
- (14) Kennels
- (15) Any other use inconsistent with the intent of this District

E. SPECIAL EXCEPTIONS

An application for a special exception within a Village Center District shall be considered by the Director of Community Development under the provisions of Section 29.0, "Design Review." Such application for a special exception within the Village Center shall be approved upon a finding that the following standards have been met:

- (1) The specific site is an appropriate one for the proposed use
- (2) No factual evidence is presented to show that the proposed use will adversely affect the value of adjacent properties; and
- (3) The proposed use shall not create any undue nuisance or hazard, either to the North Peak Community or pedestrian and vehicular flow.

24.4

SITE AMENITIES

Special site amenities may be included within village centers to provide a unique community identification and sense of place. Amenities include, but are not limited to the following.

- (1) Architectural structures, such as clock towers, pavilions, fountains, and amphitheaters
- (2) Natural environmental features, such as natural areas
- (3) Articulated urban open space elements, such as village greens, neighborhood squares, courtyards, and traffic rotaries

24.5

STREET TYPES AND USES

A. GENERAL REQUIREMENTS.

Streets shall be designed to ensure the safety of motorists and pedestrians and shall generally:

- (1) Improve the view of and the view from buildings and other prominent vistas
- (2) Minimize the area devoted to motor vehicle travel and storage
- (3) Promote pedestrian use so that it is generally more convenient and pleasant for most of the community to walk short distances than to drive
- (4) Promote the creation of vista terminations
- (5) Create safe vehicular and pedestrian passage within the village center

Alleys are permitted within the village center.

Utilities should be located in alleys wherever appropriate.

Property lines of each lot shall be coincident with the public right-of-way for that street which serves as primary access to the lot.

B. STREET SECTIONS

The Village Center Site Plan, as described in the Section 29.0 "Design Review," will identify the specific Street Sections

proposed for each street within the Village Center. They are intended to provide the City with sufficient detailed information in both plan and cross section view to enable the City to determine how parked and moving vehicles will interact with each other and with pedestrians so that the safety of the Street Section may be evaluated. The primary review criteria of the Street Sections shall be based upon the Intent of the Village Center Overlay District and the health, safety and welfare of the Community.

C. SIDEWALKS

Sidewalks shall be provided throughout the village center to connect surrounding neighborhoods to the village center core.

- (1) Sidewalks shall be on at least one side of all streets in the village center
- (2) Sidewalks shall be a minimum of 4' in width
- (3) Sidewalks at least 10' in width should be provided adjacent to commercial uses
- (4) In the event of mixed commercial/residential uses within 150' of each other on the same street, the commercial 10' width sidewalk standard should predominate
- (5) Sidewalks shall generally be parallel with the streets they adjoin and may be either within the street right-of-way or within an individual lot. The sidewalk's use and maintenance shall be ensured by permanent easement
- (6) Sidewalks shall be lighted by either street lights or pedestrian lights, or a combination thereof
- (7) Sidewalk materials may be hard surfaced (asphalt, concrete, unit pavers such as brick or interlocking pavers) or soft surfaced (gravel, decomposed granite), subject to Design Review

D. STREET CROSSINGS FOR PEDESTRIAN SAFETY

Pedestrian crossings will be provided at all street intersections. Where intensity of movement or attractiveness of land use across street supports it, mid-block crossings will be provided. Street and pedestrian way design shall be done to minimize pedestrian crossings of arterial or secondary roads.

E. STREET LIGHTING FOR PEDESTRIAN SAFETY

Street lamps shall be installed on both sides of streets at no more than 75 ft. intervals measured diagonally across the street, or parallel to the street, whichever is further.

F. STREET PLANTING FOR ENHANCEMENT

Street trees shall be installed on both street sides on all streets within the village center at the rate of one tree per 30 lineal feet of frontage, unless a comparable alternative is approved as a result of Design Review.

G. STREET LAYOUT AND RELATIONSHIPS.

Street layouts should be generally rectilinear with "deformations" as may be physically proper to adapt streets to topographic or other conditions.

Posted speed limits shall not exceed design speeds. To facilitate the balance of pedestrians and vehicles, 20 m.p.h. design speeds are encouraged throughout the village center with the exception of arterial and secondary roads.

Streets constructed within the village center shall be constructed in accordance with the Lake Elsinore road standards in effect at the time of construction. This provision specifically shall not, however, affect the geometric layout of the village center, only the actual construction standards of a road.

Street intersection spacing shall generally be 200' or more, but may be reduced with the approval of the City Engineer.

Where curbing is not necessary, minimum curb-to-curb dimensions shall be minimum pavement dimensions; and rights-of-way shall include necessary drainage ditches and appurtenances. Alleys shall be designed to accommodate the intended traffic as may be demonstrated during the Design Review process.

25.1**PURPOSE AND INTENT**

The purpose of this section is to provide for sufficient off-street parking and loading spaces for all land uses and to assure the provision and maintenance of safe, adequate and well-designed off-street parking facilities. It is the intent of this section that the number of parking spaces shall be in proportion to the need created by the particular type of use. The standards for parking facilities are intended to reduce street congestion and traffic hazards, promote vehicular and pedestrian safety and efficient land use. Off-street parking and loading areas shall be established in a manner which will promote compatibility between parking facilities and surrounding neighborhoods, protect property values and enhance the environment through good design by providing such amenities as landscaping, walls, fencing and setbacks, improve the appearance of parking lots, yards, uncovered sales areas and buildings, control heat, wind and air pollutants, minimize nuisances, and promote aesthetic values and the general well-being of the residents of North Peak. Off-street vehicle parking shall be provided in accordance with this section at the time the building or structure is constructed or the use is established.

25.2**GENERAL PROVISIONS**

All required parking and loading spaces and driveways shall be maintained in good condition and available for its intended use as long as the use for which it was required continues to operate or exist. Except in residential districts, inside a garage, no storage shall encroach into required parking or loading space or driveway and no vehicles shall be continuously parked in these areas.

A. CALCULATIONS

Where calculations of the number of spaces required results in a fractional number, the next higher whole number shall be used.

In cases of mixed use in a building or on a lot, the total requirements for off-street parking shall be the sum of the requirements for the various uses computed separately, except for shared off-site parking as described in Section 26.7.

B. GARAGE DOORS

When a garage is specifically required, or provided to meet

required parking, entry doors shall be maintained in an operable condition at all times, and no structural alteration or obstruction shall be permitted within the required parking area which would reduce the number of required parking spaces. Use of garages shall be limited to vehicular and general storage purposes only and shall not conflict with any applicable building, housing, or fire codes.

C. USE OF VACANT LOTS

Vacant lots, privately owned lots, and parking lots of commercial and industrial businesses shall not be used for the advertising and sale of motor vehicles, boats or trailers, or similar property unless they are duly licensed by the City to do so, except that occupied property may be used for the sale of personal vehicles of the property owners providing not more than one vehicle may be displayed which is not otherwise prohibited by the Chapter and subject to the provisions of the Section 28.0, "Sign Regulations".

No vehicle, boat, or trailer shall be parked or stored on any vacant or undeveloped property in any district.

25.3

NUMBER OF PARKING SPACES REQUIRED

The following minimum numbers of off-street parking spaces shall be provided in accordance with this Section for all new buildings or uses, and when any building or use is altered, extended, changed, or intensified:

A. COMMERCIAL DISTRICTS

In the Highway Commercial and Village Commercial district, one parking space shall be provided for each 250 square feet of gross floor area.

B. RESIDENTIAL USES

- (1) Single-family detached dwellings: Two enclosed spaces per dwelling.
- (2) Single Family attached dwellings: Two and one-half spaces per dwelling, one of which must be enclosed.
- (3) Multi-family: one covered space per dwelling plus one space for each dwelling with more than one bedroom and one guest

space for each five dwelling units.

(4) Senior Citizen Housing: eight-tenths space per dwelling unit.

C. PLACES OF ASSEMBLY

Auditoriums, churches, theaters and places of assembly: One space per each three seats. Where there are no fixed seats, one space per twenty-one square feet of floor area in places of assembly. Where fixed seats consist of pews or benches, eighteen lineal inches of pew or bench shall be considered one seat.

D. RECREATION FACILITIES

(1) Game courts, such as tennis and racketball: two spaces for each court.

(2) Local Public Parks of less than twenty acres not shared with an adjacent school: two spaces per acre of park with less than ten percent slope.

E. LODGING FACILITIES

Hotels and motels: One space per room or suite, plus one space per every three employees on the largest workshift, plus one space per three persons to the maximum capacity of each public meeting and/or banquet room, plus fifty percent of the spaces otherwise required for accessory uses (e.g. restaurants and bars).

F. RESTAURANTS

One space for each forty-five square feet of customer area, plus one space for each two-hundred square feet of non-customer area.

G. PRIVATE SCHOOLS

(1) Elementary Schools: One space per teacher and staff member, plus one space per two classrooms;

(2) Day Care: One space for each employee plus one space for each ten children the facility is designed to accommodate.

25.4

SIZE OF PARKING SPACES AND REQUIRED AISLE WIDTH.

Parking spaces shall have the following minimum clear dimensions and aisle width as indicated below:

District or Land Use	Space Size (in feet)		Aisle Width (in feet) by Angle of Parking Space			
	Width	Length	30°	45°	60°	90°
Residential: enclosed or covered	10	20	—	—	—	26
Residential: open	9	18	12	15	18	24
Commercial	9	18	12	15	18	24
Compact: residential and commercial	8	15	12	15	18	24
Recreational Vehicle	10	30	12	15	18	24
Parallel Spaces	7	22	—	—	—	—

TABLE 28.4 PARKING SPACE DIMENSIONS

- (1) Open or uncovered spaces with side abutting a wall, building, fence, or other obstruction shall be two feet wider than the standard required width.
- (2) Parking space length may include a two foot allowance for vehicle overhang of a landscaped area.
- (3) The required dimensions of a garage or carport shall be measured from the interior of the garage or carport.
- (4) A maximum of fifty percent of the total number of parking spaces may be compact spaces. Compact spaces shall be grouped together in logical blocks and distributed throughout a parking lot and shall be clearly marked "Compact Cars Only".
- (5) For multi-car garages or carports the minimum width for standard size parking stalls shall be 9-1/2 feet; for compact cars, the minimum width shall be 8-1/2 feet.
- (6) In commercial districts and for commercial uses, all parking spaces shall be striped with double lines two feet apart separating spaces.
- (7) Parking for the handicapped shall be provided in accordance with State requirements.

25.5

LOADING SPACE REQUIREMENTS

Commercial buildings shall be provided with off-street loading spaces per the following schedule:

25.0 PARKING REGULATIONS

<i>Land Use District</i>	<i>Gross Floor Area (Square feet)</i>	<i>Loading Spaces Required</i>
Highway Commercial	12,000 to 20,000 over 20,000	1 Type A 1 Type B
Village Commercial	All Sizes	As specified on approved Village Center Site Plan

TABLE 28.5A LOADING SPACE REQUIREMENTS

Loading spaces shall be provided for each individual building in accordance with the sizes listed.

Loading spaces shall not encroach into any drive aisle or other required spaces.

<i>Loading Space Designation</i>	<i>Length (in feet)</i>	<i>Width (in feet)</i>	<i>Vertical Clearance (in feet)</i>
Type A	20	12	14
Type B	40	12	14

TABLE 28.5B LOADING SPACE BUFFER REQUIREMENTS

Wherever a loading space is located adjacent to parking spaces there shall be a protective landscaped buffer a minimum of five feet wide separating the two.

Loading spaces shall be located and designed such that trucks shall not need to maneuver or back onto a public street or alley, except in the Village Center Overlay District where such maneuvering may be permitted subject to review of the village center site plan.

25.6

DRIVE-THROUGH ESTABLISHMENTS

Notwithstanding any other provisions of this section, additional vehicle storage spaces shall be provided for all establishments having vehicle pick-up windows as follows:

- (1) A drive-through lane with minimum storage for eight vehicles shall be provided at twenty feet per vehicle.

(2) The drive-through lane shall be designed such that it will not interfere with free and orderly circulation of the parking lot.

(3) The drive-through lane shall not encroach upon or block driveways or parking spaces and shall be separated from adjoining driveways, parking spaces, and property lines by a landscaped planter a minimum of five feet in width.

25.7

SHARED AND OFF-SITE PARKING

The shared parking design criteria, methodologies and procedures put forth in the publication, *Shared Parking: A study Conducted under the Direction of the Urban Land Institute*, by Barton Aschman Assoc., Inc. and published 1987 by the Urban Land Institute (International Standard Book #0-87420-9 and Library of Congress Card Catalog Number 83-51648) ("ULI Criteria") shall be considered acceptable within a Village Center. Refer to *North Peak Specific Plan Appendix B* for this information.

The incorporation of shared parking into the village center shall be made at the time of the approval of the tentative map that includes the village center.

In the event of a mix of uses not readily identifiable by ULI criteria, the Planning Commission may require that land areas be set aside as a space allocation for possible parking needs. Any such set aside space shall be eligible for reconsideration for other use one year following the granting of occupancy permits for the mix of uses potentially needing such space.

Parking lots for buildings used principally on holidays (e.g., religious buildings) must be graded, compacted and planted, but may be left unpaved.

25.8

CIRCULATION AND PARKING SPACE LAYOUT.

All parking areas shall be designed as follows:

(1) The location and dimensions of aisle areas adjacent to parking spaces shall be arranged in accordance with the minimum parking standards required in this Section.

(2) For all uses other than one-family and two-family dwellings located in residential districts, parking shall be arranged so as to permit ve-

hicles to move out of the parking area without backing onto a street. This provision may be waived in the Village Center Overlay District.

(3) Tandem spaces shall be allowed in single family detached and duplex dwellings.

(4) No two-way drive aisle shall be less than twenty feet. No one-way drive aisle width shall be less than twelve feet.

(5) All parking shall be designed to provide complete and through circulation wherever possible. Adequate turning radii and turnarounds shall be provided for emergency vehicles and trash and delivery trucks.

(6) Parking spaces should be located within two hundred feet of the use which they are intended to serve. In the Village Center Overlay District, this may be extended to three hundred feet.

(7) For any structure which extends above a drive aisle, the minimum vertical clearance shall be fourteen feet.

25.9

SURFACE OF PARKING AREA

The following standards shall apply to all parking areas required by this Section:

(1) The flooring material for garages and carports in all districts shall be concrete.

(2) All parking and loading spaces and driveways shall be paved and maintained in good and safe condition and shall be so graded and drained as to dispose of all surface water and to prevent water from running off onto adjoining property without the permission of the owner of that property. With the approval of the Planning Commission, paving of driveways for single-family residences may be waived in the Rural Residential District. Drainage courses and swales in parking lots shall be paved with concrete. All asphalt parking lots shall receive a seal coat.

(3) Except for single-family and two-family residences all off-street parking and loading spaces shall be marked by stripes not less than four inches wide painted on the surfaced area, or by similar means as approved by the Community Development Director.

25.10

LIGHTING OF PARKING AREAS

Adequate parking lot lighting for security purposes shall be required and maintained to effectively illuminate the parking area of all developments, except for single-family and duplex dwellings. Lighting shall be located and designed so as to preclude the direct glare of light shining

onto adjacent property, streets, or into the sky above a horizontal plane passing through the luminaire.

25.11

PARKING AND STORAGE OF COMMERCIAL VEHICLES

For the purpose of this section "commercial vehicle" shall mean self-propelled vehicle used or maintained for the transportation of persons for hire, compensation or profit or used and maintained primarily for the transportation of property including, but not limited to, tractors, vans, trailers, panel trucks, dump trucks, but excluding vans and pickups with wheel bases less than one-hundred fifty inches.

(1) Parking or storing of commercial vehicles in residential districts for any length of time is prohibited, except that commercial vehicles may park for the purpose of making pickups and deliveries of materials and merchandise from or to any building or site.

(2) Commercial vehicles shall not be parked or stored on vacant or undeveloped property in any district.

25.12

PARKING AND STORAGE IN RESIDENTIAL ZONES

A. GARAGE DOORS

When a garage is specifically required, entry doors shall remain operable at all times, and no structural alteration or permanent obstruction shall be permitted within the required parking area. Use of garages shall be for vehicular and general storage purposes only, and shall not conflict with any applicable building, housing and fire codes.

B. RECREATION VEHICLES

Recreation vehicles, including, but not limited to, motor homes, travel trailers, boats, and similar equipment shall not be parked within the required front yard setback. They may be parked behind the required front yard setback or in a side or rear yard only if they are screened from adjacent properties and the public right-of-way by a solid fence, wall, or gate a minimum of six feet in height.

No habitation of a trailer, camper, or recreational vehicle shall be allowed.

25.13

PARKING REQUIREMENTS FOR VILLAGE CENTER OVERLAY DISTRICT

A. GENERAL REQUIREMENTS

- (1) On-street parking directly fronting a lot shall count toward fulfilling the parking requirement of that lot.
- (2) Generally, continuous head-in parking should be provided along streets where commercial uses are predominate. Parallel parking is permitted along all other streets.
- (3) Parking lots should generally be located at the rear or at the side of buildings, and shall be screened from the sidewalk.
- (4) Parking lots and parking garages should not abut street intersections or occupy lots which terminate a street vista.
- (5) Parking is permitted in alleys if the alley is constructed with additional width to accommodate such use.
- (6) Adjacent parking lots shall have vehicular connections, via an alley or internally.

B. PARKING REQUIREMENTS

Land area(s) for parking shall be reserved in accordance with the parking requirements defined in Section 26.0. The required parking may be constructed in phases subject to the approval of the Planning Commission.

For other than shared parking, parking spaces may be considered allocated to a particular lot, or use, if those spaces are entirely on that Lot or the use of the spaces has been assured by assignment through easement or other legal guaranty. On-street parking may be allocated to a particular lot when a particular on-street space abuts a particular lot. Where on-street spaces abut more than one lot, a particular parking space may be allocated to a particular Lot only if more than 66% of the space abuts that lot or the space has been assured by assignment.

C. SHARED PARKING

In order to help minimize the land area used for parking requirements within the village center, provision has been made for Shared Parking according to the standards described in Section 28.7 "Parking Standards: Shared and Off-site Parking".

26.1**APPLICABILITY**

The regulations contained in Sections 26.2 to 26.7 are intended to apply to landscape areas to be maintained by private entities.

The regulations contained in Sections 26.8 to 26.12 apply only to areas to be maintained by the North Peak Area of the Lake Elsinore Lighting & Landscape Maintenance Assessment District (NP-LLMAD).

26.2**MINIMUM LANDSCAPE AREA - PRIVATE MAINTENANCE**

Within the Village Commercial and Highway Commercial Districts, there shall be a minimum of ten percent of the net site area (exclusive of public rights of way and easements) landscaped with trees, shrubs and ground covers. Off-site public rights-of-way contiguous with site landscaping can be counted as a part of the overall required landscape area.

A reduction in minimum area may be sought from the Planning Commission and approved through the design review process in recognition of quality design. For purposes of this provision, quality design considerations include: superior visual buffering; visually creative massing of materials; maximum provision of shade; visually pleasing combinations of landscape, hardscape, textures, contouring; noise attenuation value; theme reinforcement; use of courtyards, atriums and other effective treatment of ground floor public space; creative use of water, sculpture or art work; effective use of trees; overall visual interest; ease of maintenance in mature condition and water conservation effectiveness.

26.3**IRRIGATION - PRIVATE MAINTENANCE**

Generally, there should be a permanent automatic irrigation system installed for all landscaped areas with the exceptions of single family development on lots greater than five thousand (5,000) square feet. Temporary irrigation systems may be used at the developer's discretion for areas which will eventually be unirrigated or for areas which will be irrigated by others in the future.

26.4**INSTALLATION AND MAINTENANCE - PRIVATE MAINTENANCE**

Required landscaping shall be maintained in a neat, clean, and healthy condition. This includes pruning, mowing of lawns, weeding, removal of litter, fertilizing, replacement of plant materials where necessary, and

the regular irrigation of all plant material.

Any landscape materials that die shall be replaced within thirty days or upon a written notice from the City of Lake Elsinore to the property owner and/or lessee.

Generally, trees larger than 5 gallon size should be double-staked until they are strong enough to withstand the normal winds (eucalyptus trees are exempt from this requirement on the recommendation of a landscape architect). Annual color plantings shall be replaced upon decline.

26.5

PARKING AREA LANDSCAPING - PRIVATE MAINTENANCE

Wherever any parking area, except for single-family dwellings, is provided, landscaping consisting of trees, shrubs, vines, ground cover, or combinations thereof, and permanent irrigation shall be installed and permanently maintained in accordance with the following standards:

- (1) Where any parking area or driveway abuts a street there shall be a minimum setback of ten feet from the public right-of-way, which shall be fully landscaped and irrigated unless a greater setback is required. Within the Village Center Overlay District, this setback may be zero feet.
- (2) Parking areas shall be screened from view from adjacent arterial roads and secondary roads by either fences, walls, shrubs, berms or a combination thereof. The height of the screening shall be determined by the specific site conditions, but generally should be a minimum of 42".
- (3) Where any parking or driveway areas which are intended to serve a commercial use abuts a residential district a minimum five-foot wide landscaped planter containing evergreen trees spaced not more than thirty feet apart shall be provided. In addition, a masonry wall a minimum of six feet in height shall also be constructed along the property line adjacent to the residential district. Within the Village Center Overlay District, this requirement may be waived.
- (4) Where any parking or driveway abuts a residential or commercial district a landscaped planter a minimum of five feet in width shall separate the parking area or driveway from the property line, unless a greater setback is required. Within the Village Center Overlay District, this setback may be zero feet.
- (5) Internal landscaping in addition to subsection (1), (2), and (3) above, and equal to a minimum of ten percent of the parking and driveway areas is required and should be distributed throughout the parking

area.

(6) All landscape planter beds in interior parking areas shall be not less than four-feet in width and bordered by a concrete curb not less than six-inches nor more than eight inches in height adjacent to the parking surface.

(7) Parking and driveway areas in commercial and residential zones shall be separated from buildings by a landscaped planter.

(8) Where a drive aisle abuts the side of a parking space a landscaped planter shall separate the parking space from the drive aisle.

(9) At least one fifteen gallon tree shall be provided within the parking area for every six parking spaces. Trees may be placed in regular formalized groupings or irregularly, at the discretion of the developer. Trees installed in the public right-of-way adjacent to the project site may be counted to satisfy this requirement.

(10) Any unused space resulting from the design of the parking area shall be used for landscape purposes.

(11) The height of boundary or interior landscaping shall be limited to a height not to exceed three feet six inches, or in the case of trees, no branch shall be below six feet, when within fifteen feet of the point of intersection of: a) a vehicular trafficway or driveway and a street; b) a vehicular trafficway or driveway or sidewalk; c) Two or more vehicular trafficways or driveway or streets.

(12) Required landscaping shall be maintained in a neat, clean, and healthy condition. This includes pruning, mowing of lawns, weeding, removal of litter, fertilizing, replacement of plants where necessary, and the regular watering of all plant material.

26.6

LOADING AND STORAGE SCREENING - PRIVATE MAINTENANCE

All loading, storage, and refuse collection areas shall be screened from streets and adjacent properties with walls, fencing or landscaping, or a combination thereof. Shrub materials utilized exclusively for screening shall be minimum 5 gallon size at time of installation. Trees used for screening purposes shall be minimum 15 gallon size.

26.7

STREET TREES AND FRONT YARD LANDSCAPING - PRIVATE MAINTENANCE

(1) Street trees of a minimum 15 gallon size are required on all public local residential streets at a rate of one tree per 30 lineal feet of frontage. If the required trees are planted in a landscape easement rather than the street right-of-way, they must be planted within 5 feet of the right-of way.

(2) Individual front yard landscaping installed by the developer is required in residential districts in which the minimum lot size is less than 5,000 square feet. The required landscape shall be installed within 30 days of occupancy and shall be equipped with an underground irrigation system.

26.8

LANDSCAPE APPROVAL PROCESS - LLMAD AREAS

A. GENERAL REQUIREMENTS

The regulations contained within the following sections is intended to apply only to areas within the North Peak area of the Lake Elsinore Lighting & Landscaping Maintenance Assessment District (NP-LLMAD).

Plans shall be required according to the following process and requirements for median island and public rights-of-way landscaping.

All plans submitted shall be prepared under the direct supervision of a licensed Landscape Architect, registered in the State of California bearing his/her signature.

B. REVIEW AND APPROVAL PROCESS

1. Preliminary Plan Submittal

The applicant, when prepared for formal project submittal to the City, shall provide a preliminary landscape plan for medians and rights-of-way as part of the total application package for Design Review of a tentative map (See Section 29.0).

2. City Review

Upon receipt of the project, the Community Services Department shall review the plans for compatibility with the Guidelines set forth for median and right-of-way landscape development and the landscape guidelines for the North Peak Specific Plan. After completion of this review, the Community Services Department will return the plans to the Community Development Department with its findings and recommendations. The Community Development Department shall incorporate the recommendations into the project staff report's condition of approval.

3. Approval

Based upon findings and recommendations in the staff report, the preliminary landscape plans are then approved or conditionally approved by the Planning Commission and/or City Council.

4. Final Landscape Plans

Prior to issuance of building permits, the applicant shall submit and receive approval of the landscape construction documents (plans, specifications, etc.) by the Public Works Department/Community Service Department. Three (3) sets shall be submitted in substantial compliance with the conditions of approval. Final landscape plan review by the Public Works Department/Community Service Department will determine:

- a. That the proposed plans are consistent with the spirit and intent of the North Peak Specific Plan.
- b. That proposed plant material will be suitable aesthetically, physiologically, and ecologically for the particular planting situation.
- c. That proposed planting and irrigation system meets the requirements as set forth in the North Peak Specific Plan for median and right-of-way landscape development.
- d. That the overall design and improvements are aesthetically and technically sound to insure low maintenance and operation costs.

Upon approval, a stamped approved "job set" will be available for the applicant along with the permit. The submittal requirements for the final construction documents are provided in Section 30.2.

The applicant shall submit an estimate of maintenance cost for the landscaping in the LLMAD area of responsibility.

5. Revisions to Approved Plans

Changes to the approved plans shall receive Public Works Department/Community Service Department approval prior to implementation in the field. The applicant shall submit the following:

- a. Three (3) sets with revision(s) shown in red and noted on the Title Sheet.
- b. A letter of explanation from the project Landscape Architect justifying the changes.

26.9**PLAN PREPARATION - LLMAD AREAS****A. GENERAL REQUIREMENTS***1. Required Contents*

All plans, including the preliminary landscape plan, planting plan, irrigation plan, and construction documents, shall include the following:

- a. Project Title
- b. Landscape Architect's Name, Address, Phone Number, and License Number
- c. North Arrow
- d. Scale

2. Site plan drawn to scale indicating:

- a. Definition of public right-of-way and privately maintained areas.
- b. Property lines/project limits.
- c. Paving (including streets, sidewalks, driveways, street intersections, medians).
- d. Easements.
- e. Above ground utilities, street lights, fire hydrants, and other appropriate information as it relates to landscape development.
- f. Existing trees to be saved (if applicable).
- g. Slopes (indicate top, toe, and slope gradient).
- h. Stationing shown on landscape plans for median development improvements shall match existing street stationing, available from the Public Works Department.
- i. Match lines shall be clear and labeled for easy plan reference.

B. PRELIMINARY LANDSCAPE PLAN*1. Number of Sets*

Ten (10) sets bound together with the development package.

2. Required Contents

Preliminary landscape plans shall include the following minimum information:

- a. Delineate turf and/or ground cover trees and location of shrubs.
- b. Tree locations. Identify trees by general description (i.e., vertical, canopy, broad dome, evergreen, deciduous, etc.).
- c. Proposed plant palette with botanical name, common name, and size. (This palette shall be from the approved plant list for medians and right-of-way landscape).
- d. Preliminary landscape plans shall be drawn at a minimum scale of 1" = 40'.

C. LANDSCAPE CONSTRUCTION DOCUMENTS*1. Approval Sets*

Three (3) sets of landscape construction documents shall be submitted to the City for approval prior to issuance of building permits.

2. Plan Scale

All plans shall be drafted at a minimum scale of 1" = 30'.

3. Plan Size

Plans shall be 24" x 36".

4. Title Sheet

Cover/Title Sheet (first sheet) shall include:

- a. Project Location Map.
- b. Vicinity Map showing street names, north arrow, project limits.
- c. General Notes.
- d. Title Block.
- e. Signature Block for Approvals. Signature block shall be provided on Title Sheet for the following signatures:

- Director of Public Works/City Engineer.
- Director of Community Services.
- f. Submittal dates block.
- g. Landscape Architect's (or other), Firm Name, Address, Signature and Seal of Registered Landscape Architect.
- h. Owner/Developer's Name, Address, and Phone Number.

5. Grading Plan

- a. Indicate existing and proposed grades with contours and spot elevations.
- b. Note all grades (including finished surface and existing grades), flow lines, etc. within public right-of-way.

6. Construction/Dimension Plan

- a. Dimension all hardscape configuration utilizing existing street station portion.
- b. Flag all construction to appropriate detail.

7. Irrigation Plan

- a. Locate all equipment, heads, valves, backflow preventer(s), etc.
- b. Locate and identify size and type of all non-pressure lateral line, and pressure main line pipe .
- c. Indicate and identify point(s) of connection.
- d. Indicate water meter size and location.
- e. Indicate system design water pressure and existing static water pressure.
- f. All equipment is to be identified by manufacturer's name, model number, and size if applicable.
- g. All heads and/or emitters are to be identified by manufacturer, model number, pattern, radius, and GPM or GPH demand.
- h. All control valves are to indicate manufacturer, model number, size, and estimate GPM demand at each valve.

8. Planting Plan

- a. Indicate botanical name, common name, and location of all plants.

- b. Quantities of maximum spacing of all plants.
- c. Size of plant material to be installed; for example, five (5) gallon, 24" box, flats, etc.

9. Planting and Irrigation Details

Planting and irrigation details are to include the following information:

- a. Tree planting, staking, and/or guying.
- b. Shrub planting.
- c. Irrigation equipment installation including swing joints, control valves, backflow preventers, gate valve, quick coupler, heads, emitters, etc.
- d. Header installation (if applicable).

10. Specifications

Provide written specifications for materials, equipment, and method of installation to include material specifications, installation procedures, and warranties.

26.10

INSPECTION AND ACCEPTANCE - ILMAD AREAS

A. INSPECTION PROGRAM

1. The City will inspect and approve all installations and operations.
2. The Contractor will give the City Landscape Inspector (Public Works Department) 48 hours notice prior to work inspection desired.
3. Inspection will include, but not be limited to:
 - a. Grading and drainage.
 - b. Irrigation trenching, piping, and equipment installation.
 - c. Quality of plant material, location of plant material, inspection of plant pits, and incorporation of specified backfill material.

26.11

DESIGN STANDARDS AND CRITERIA - LLMAD AREAS

A. HARDSCAPE

1. Hardscape paving of areas greater than two feet wide shall not be permitted within median islands.
2. Nose areas created at left-turn pockets, if less than two feet in width, may have hardscape.

B. IRRIGATION

1. All landscaped areas within the NP-LLMAD shall be irrigated by a permanent automatic irrigation system.
2. Backflow preventer to be reduced pressure type.
3. Sprinkler heads (all pop-up shrub or turf) to be Rainbird.
4. RC valves, quick couplers to be Rainbird.
5. Electrical meter panel to be Myers pedestal type.
6. Irrigation controller to be Rainbird MaxiCom Satellite Assembly by Pacific Technical Service. A complete system to be provided for hook up to central control:
 - a. ISC Satellite with radio remote access assembly.
 - b. Cluster Control Unit (CCU) with telephone hook up. Telephone wire from service to CCU unit to be in Schedule 40 conduit.
 - c. Flow sensing unit with flow sensing wires in conduit.
 - d. Master valve with master valve power assembly.
 - e. Two-wire path interconnect from CCU unit to the Satellite units in Schedule 40 PVC conduit.
 - f. All units to be in stainless steel enclosures.
 - g. Rainbird sensing or pulsing decoder assembly as required.
7. City to receive five year service warranty from manufacturer on satellite assembly unit.

C. PLANTING**1. Trees**

a. With the exception of eucalyptus trees planted in the eucalyptus forest area (See Exhibit 14.7b), the minimum tree size shall be 15 gallon minimum.

b. Eucalyptus trees planted in the eucalyptus forest shall be planted in the following proportions:

15-gallon size 20%

5-gallon size 60%

1-gallon size 20%

c. The minimum density of trees within the eucalyptus forest area may be 100 trees per acre.

d. All trees larger than 5 gallon size shall be staked or guyed and planted per City Standards. 15 Gallon eucalyptus trees need not be staked upon the recommendation of the Landscape Architect.

e. All trees in turf areas shall be installed with trunk protection devices. Trunk protectors shall be Arbor Gard or approved equal.

f. Within median islands, trees shall be planted so that the center line of the trunk is a minimum of five feet from the face of the curb; from all other curbs, the minimum setback shall be three feet.

2. Shrubs

a. All shrubs shall be five gallon size or larger with the exception of small accent plants, California native plants unavailable in larger sizes, or plants used as ground cover. These shrubs may be one gallon in size.

b. All shrubs to be planted per City Standards.

3. Ground Cover

a. Shall consist of rooted ground cover with appropriate spacings as approved by City.

b. Plant ground cover in straight rows evenly spaced using triangular spacing.

c. Groundcover may be hydroseeded upon the recommendations of the Landscape Architect.

4. Turf

- a. Turf in areas intended for active human use to be hybrid fescue blend.
- b. Turf may be sodded or hydroseeded.

26.12

PLANT PALLETTE - LLMAD AREAS

The following plant material are approved for use within the NP-LLMAD. These lists are not exclusive and are intended to serve as a general guide; other plant material consistent with the overall intent of the Specific Plan may be used subject to the approval of the City.

A. EUCALYPTUS FOREST

The eucalyptus forest area should be areas designated as eucalyptus forest on the Landscape Master Plan (Exhibit 14.7b) may incorporate the following trees, shrubs and ground covers:

1. Trees

<i>Eucalyptus camaldulensis</i>	River Gum
<i>Eucalyptus maculata</i>	Spotted Gum
<i>Eucalyptus rudis</i>	Swamp Gum
<i>Eucalyptus viminalis</i>	Ribbon Gum
<i>Eucalyptus polyanthemos</i>	Silver Dollar Eucalyptus
<i>Eucalyptus melliodora</i>	Honey Scented Gum
<i>Eucalyptus cladocalyx</i>	Sugar Gum

2. Shrubs and Ground Cover

<i>Atriplex</i> spp.	Salt Bush Species
<i>Acacia redolens</i>	no common name
<i>Arctostaphylos</i> spp.	Manzanita species
<i>Baccharis</i> spp.	Coyote Bush Species
<i>Ceanothus</i> spp.	California Lilac Species
<i>Cotoneaster</i> spp.	Contoeaster Species
<i>Heteromeles arbutifolia</i>	Toyon
<i>Rhus integrifolia</i>	Lemonade Berry
<i>Rhus ovata</i>	Sugar Bush

B. PROCESSIONAL LANDSCAPE

Areas designated as Processional Landscape on the Landscape Master Plan (Exhibit 14.7b) are intended to have a formal character. Trees should have a regular, predictable form and should be planted in formal, regularly spaced rows. Shrubs will also have a formal character and may be clipped into hedges when appropriate. Processional Landscape areas may incorporate the following trees, shrubs and ground covers:

1. Trees

<i>Cedrus deodara</i>	Deodar Cedar
<i>Eucalyptus camaldulensis</i>	River Gum
<i>Eucalyptus maculata</i>	Spotted Gum
<i>Eucalyptus rudis</i>	Swamp Gum
<i>Jacaranda acutifolia</i>	Jacaranda
<i>Magnolia grandiflora</i>	Southern Magnolia
<i>Phoenix dactilifera</i>	Date Palm
<i>Phoenix canariensis</i>	Canary Island Palm
<i>Pistacia chinensis</i>	Chinese Pistache
<i>Schinus molle</i>	California Pepper
<i>Washingtonia robusta</i>	Mexican Fan Palm
<i>Washingtonia filifera</i>	California Fan Palm

2. Shrubs and Ground Cover

<i>Bougainvillea</i> spp.	Bougainvillea Species
<i>Delosperma</i> 'alba'	Ice Plant
<i>Dodonea viscosa</i>	Hopseed Bush
<i>Ligustrum</i> spp.	Privet Species
<i>Lonicera</i> spp.	Honeysuckle
<i>Nerium Oleander</i>	Oleander
<i>Rhapiolepis indica</i>	India Hawthorn
<i>Rosa</i> spp.	Roses

C. ORCHARD LANDSCAPE

Areas designated as Orchard Landscape on the landscape master plan (Exhibit 14.7b) are intended to simulate traditional agricultural plantings of fruit trees. Plantings should be formal clusters of small and medium sized trees with a regular, rounded form. The following plant materials are acceptable within this area:

1. Trees***Citrus* spp.****Citrus*****Olea europaea* 'Fruitless'*****Olea europaea* 'Swan Hill'****European Olive*****Prunus ilicifolia*****Holly Leafed Cherry*****Platanus acerifolia*****London Plane Tree*****Pyrus kawakamii*****Evergreen Pear****2. Ground Cover*****Lolium* spp.****Rye Grass*****Festuca* spp.****Fescue**

27.1**PURPOSE AND INTENT**

All signs at North Peak shall be controlled by these sign regulations. The regulations will contribute to the vitality and value of North Peak and have been carefully planned to respond to the image and environmental quality objectives of North Peak. The regulations incorporate the spirit and intent of the Lake Elsinore sign code. Specific sign programs to be submitted to the City of Lake Elsinore for review.

27.2**SIGN TYPES PERMITTED**

Community Identification Sign
Highway Commercial/Office/Retail Identification Sign
Village Commercial/Office/Retail Identification Sign
Residential Identification Sign
Office/Retail Tenant Identification Sign, Wall Mounted
Office/Retail Tenant Identification Sign, Freestanding
Major Vehicular Directional Sign
Community Marketing Information Sign
Parcel Marketing Information Sign
Future Public Facility or Amenity Information Sign
Vehicular Directional Sign

27.3**SIGN STANDARDS**

All signs, regardless of type or permanency, are affected by nine criteria for design and execution. These Standards describe the variables for each sign type.

These criteria are:

SIGN TYPE: Name reference.

FUNCTION: The purpose of the sign.

PLACEMENT: The position of the sign relative to roadways and other features.

MATERIALS: The range of materials and colors from which the sign can be built.

LANDSCAPING: The treatment of landscaping around the sign.

ILLUMINATION: The method of lighting or mounting.

TYPOGRAPHY: Readability requirements.

SIZE: The area of the sign

QUANTITY: The number of signs allowable.

27.4

COMMUNITY IDENTIFICATION SIGN

A. FUNCTION:

Identify North Peak community entrances to vehicular traffic. Display name, logo and introduce North Peak's overall sign system form, material and color palette. Create a visual image unique to North Peak community.

B. PLACEMENT:

Perpendicular, parallel or flanking entrances at North Peak community external road system.

C. MATERIALS:

Material palette must be compatible with, reflect and unify North Peak area.

D. LANDSCAPING:

Landscaping palette must be consistent with, reflect and unify North Peak community area.

E. ILLUMINATION:

Ground or internal illumination if required.

F. TYPOGRAPHY:

Viewing distance of copy for readability to be 100' minimum.

G. SIZE:

Vertical height of sign background treatment not to exceed 6'0".
Sign copy area not to exceed 40 square feet.

H. QUANTITY:

A total of 4 signs are permitted.

27.5

HIGHWAY COMMERCIAL/OFFICE/RETAIL IDENTIFICATION SIGN

A. FUNCTION:

Identify entrances into the two Highway Commercial sites within the North Peak community. Display area name, logo and reinforce the North Peak community sign system theme.

B. PLACEMENT:

Perpendicular, parallel or flanking entrances at internal road system or freeway ext.

C. MATERIALS:

Material palette must be compatible with, reflect and unify North Peak area.

D. LANDSCAPING:

Landscaping palette must be consistent with, reflect and unify North Peak community area.

E. ILLUMINATION:

Ground or internal illumination if required.

F. TYPOGRAPHY:

Viewing distance of copy for readability to be 75' minimum for internal road system and 300' maximum for freeway road system.

G. SIZE:

For internal road: vertical height of a sign background treatment shall not exceed 5'0" with sign copy area not to exceed 24 square feet.

For freeway road: vertical height of a sign background shall not exceed 40'0" with sign copy area not to exceed 150 square feet.

H. QUANTITY:

Four at Internal roads

Two at Freeways

27.6**VILLAGE COMMERCIAL/OFFICE/RETAIL IDENTIFICATION SIGN****A. FUNCTION:**

Identify entrances into the three village commercial sites within the North Peak community. Display area name, logo and reinforce the North Peak community sign system theme.

B. PLACEMENT:

Perpendicular, parallel or flanking entrance at internal road system.

C. MATERIALS:

Material palette must be compatible with, reflect and unify North Peak area.

D. LANDSCAPING:

Landscaping palette must be consistent with, reflect and unify North Peak community area.

E. ILLUMINATION:

Ground or internal illumination if required.

F. TYPOGRAPHY:

Viewing distance of copy for readability to be 75' minimum.

G. SIZE:

Vertical height of a sign background treatment shall not exceed 5'0" with sign copy area not to exceed 32 square feet.

H. QUANTITY:

Maximum of ten per village.

27.7**RESIDENTIAL IDENTIFICATION SIGN****A. FUNCTION:**

Identify entrances into the thirty residential sites within the North Peak community. Display residential neighborhood name, logo and reinforce the North Peak community sign system theme.

B. PLACEMENT:

Perpendicular, parallel or flanking entrance at internal road system.

C. MATERIALS:

Material palette must be compatible with, reflect and unify North Peak area.

D. LANDSCAPING:

Landscaping palette must be consistent with, reflect and unify North Peak community area.

E. ILLUMINATION:

Ground or internal illumination if required.

F. TYPOGRAPHY:

Viewing distance of copy for readability to be 50' minimum.

G. SIZE:

Vertical height of a sign background treatment shall not exceed 5'0" with sign copy area not to exceed 24 square feet.

H. QUANTITY:

One per site.

27.8**OFFICE/RETAIL TENANT IDENTIFICATION SIGN, MOUNTED****A. FUNCTION:**

Display tenant's name on building fascia to confirm business location to on-site traffic.

B. PLACEMENT:

On wall near tenant's entrance. Developers are encouraged to minimize use of wall signs. Signs should appropriately identify

tenants' scale on the site.

C. MATERIALS:

Material palette must be consistent with and reflect and unify particular architectural design of the building.

D. LANDSCAPING:

N/A

E. ILLUMINATION:

Ground or internal illumination if required.

F. TYPOGRAPHY:

Viewing distance of copy for readability to be 100' minimum.

G. SIZE:

Total sign area per tenant cannot exceed one (1) square foot per lineal foot of tenant frontage. Sign cannot be located above building roof line. Sign copy area cannot exceed 75 square feet. Developers are encouraged to minimize use of wall signs.

H. QUANTITY:

One per business.

27.9

OFFICE/RETAIL TENANT IDENTIFICATION SIGN, FREESTANDING

A. FUNCTION:

Display tenant's name on freestanding sign at tenant's entrance. Used only by tenants who have clear and substantial parcel ownership, i.e. single user parcel.

B. PLACEMENT:

Perpendicular, parallel or flanking entrance at internal road system.

C. MATERIALS:

Material palette must be consistent with and reflect and unify particular architectural design of the building.

D. LANDSCAPING:

Landscaping palette must be consistent with, reflect and unify North Peak community area.

E. ILLUMINATION:

Ground or internal illumination if required.

F. TYPOGRAPHY:

Viewing distance of copy for readability to be 50' minimum.

G. SIZE:

Vertical height of a sign background treatment shall not exceed

5'0" with sign copy area not to exceed 24 square feet.

H. QUANTITY:

One per business

27.10

MAJOR VEHICULAR DIRECTIONAL SIGN

A. FUNCTION:

Orients and provides directional information to vehicular traffic at major decision points within the North Peak community.

B. PLACEMENT:

Perpendicular to internal road system.

C. MATERIALS:

Material palette must be compatible with, reflect and unify North Peak area.

D. LANDSCAPING:

Landscaping palette must be consistent with, reflect and unify North Peak community area.

E. ILLUMINATION:

Reflective copy.

F. TYPOGRAPHY:

Viewing distance of copy for readability to be 100' minimum.

G. SIZE:

Vertical height of a sign background treatment shall not exceed 5'0" with sign copy area not to exceed 10 square feet.

h. QUANTITY:

As approved by the City of Lake Elsinore.

27.11

COMMUNITY MARKETING INFORMATION SIGN

A. FUNCTION:

Identify North Peak community boundary. Display North Peak community name and community marketing information, i.e., *North Peak community 2,000 acre master planned environment*.

b. PLACEMENT:

Perpendicular or parallel to North Peak community external road system.

C. MATERIALS:

Material palette must be compatible with, reflect and unify North Peak area.

D. LANDSCAPING:

Landscaping palette must be consistent with, reflect and unify North Peak community area.

E. ILLUMINATION:

Ground or internal illumination if required.

F. TYPOGRAPHY:

Viewing distance for readability to be 100' minimum.

G. SIZE:

Vertical height of sign background treatment not to exceed 25'0". Sign copy area not to exceed 100 square feet.

H. QUANTITY:

Total of six.

27.12**PARCEL MARKETING INFORMATION SIGN****A. FUNCTION:**

Identify parcel boundary. Display parcel name and marketing information, e.g., *Peak Estates Single Family Homes from \$150,000 up.*

B. PLACEMENT:

Perpendicular or parallel to North Peak community internal road system.

C. MATERIALS:

Material palette must be compatible with, reflect and unify North Peak area.

D. LANDSCAPING:

Landscaping palette must be consistent with, reflect and unify North Peak community area.

E. ILLUMINATION:

Ground or internal illumination if required.

F. TYPOGRAPHY:

Viewing distance for readability to be 30' minimum.

G. SIZE:

Vertical height of sign background treatment not to exceed 10'0". Sign copy area not to exceed 32 square feet.

H. QUANTITY:

One per parcel.

27.13**FUTURE PUBLIC FACILITY OR AMENITY INFORMATION SIGN****A. FUNCTION:**

Identify public facility or amenity boundary. Display parcel name and use, i.e., *Future Park Site North Peak Park*.

B. PLACEMENT:

Perpendicular or parallel to North Peak community internal road system.

C. MATERIALS:

Material palette must be compatible with, reflect and unify North Peak area.

D. LANDSCAPING:

Landscaping palette must be consistent with, reflect and unify North Peak community area.

E. ILLUMINATION:

Ground or internal illumination if required.

F. TYPOGRAPHY:

Viewing distance for readability to be 30' minimum.

G. SIZE:

Vertical height of sign background treatment not to exceed 10'0". Sign copy area not to exceed 32 square feet.

H. QUANTITY:

1 per parcel.

27.14**MARKETING DIRECTIONAL SIGN****A. FUNCTION:**

List on-site sales area with directional arrows to aid the safe flow of traffic.

B. PLACEMENT:

Perpendicular or parallel to North Peak community internal road system.

C. MATERIALS:

Material palette must be compatible with, reflect and unify North Peak area.

D. LANDSCAPING:

Landscaping palette must be consistent with, reflect and unify North Peak community area.

E. ILLUMINATION:

Ground or internal illumination if required.

F. TYPOGRAPHY:

Viewing distance for readability to be 30' minimum.

G. SIZE:

Vertical height of sign background treatment not to exceed 10'0". Sign copy area not to exceed 32 square feet.

H. QUANTITY:

As approved by the City of Lake Elsinore.

28.1**PURPOSE AND INTENT**

The purpose of this section is to establish a design review process for development proposals in order to ensure that development within North Peak occurs in a manner which enhances the character and quality of the community and that the scale, spacial relationships and architectural treatment of structures including materials, colors, and design, visually contribute to the overall character of North Peak. Design review also applies to the ancillary elements of projects such as signs and landscaping in order to ensure that the overall development maintains the same integrity of design as approved for the primary structure(s).

28.2**DESIGN REVIEW REQUIRED**

No building permit shall be issued for, and no person shall commence to use, any structure, including signs, until that structure and its accompanying development has received design review approval pursuant to the provisions of this Section.

28.3**DESIGN REVIEW APPLICATION**

Application for a design review shall be filed with the Planning Department on a form prescribed by the Director of Community Development and shall include, but not be limited to, the following data and maps:

- (1) Name and address of the applicant.
- (2) A statement that the applicant is the owner or the authorized agent of the owner of the property subject to the design review request. This provision shall not apply to proposed public utility right-of-way.
- (3) The address and legal description of the property (assessor's parcel number).
- (4) A list of all owners of property located within three-hundred feet of the exterior boundaries of the subject property; the list shall be keyed to a map showing the location of these properties.
- (5) Site plans, scaled and fully dimensioned, indicating the location of all property lines, type and location of all buildings and structures, entrances, parking, signs, walls, and preliminary grading information.
- (6) Landscape plans that show the location of landscape areas and proposed planting treatments; a list of proposed plant materials shall be provided with the proposed sizes at the time of installation.

- (7) Location of existing improvements on adjacent properties and public right-of-way within one hundred feet of the site boundaries.
- (8) Elevations and floor plans for all buildings, except in the Village Center Overlay District where a multi-phased development is anticipated and precise elevations and floor plans have not yet been established.
- (9) A statistical inventory of the project including size of site, number of parking spaces, and coverage information.
- (10) A statement from the applicant describing the project and its objectives.
- (11) The Director may require additional information or plans, if necessary, to determine whether a design review request should be granted or denied. The Director may also authorize omission of any plans and drawings required by this action if he finds they are not necessary.

28.3

DESIGN REVIEW HEARING

With the exception of a minor design review as provided herein, the Planning Commission shall hold at least one hearing on each application for a design review. The hearing shall be set and notice given as prescribed in the zoning ordinance. At this hearing, the Planning Commission shall review the application and drawings submitted and receive comments from the public concerning the proposed development and the manner in which it will effect the subject property and surrounding properties.

28.4

SCOPE OF DESIGN REVIEW

In order to achieve the purpose of this Chapter the following design concepts shall be paramount in the consideration of any design review approval:

- (1) The scale and spatial relationship of all structures should be appropriate to the site and to surrounding developments. Structures should be located on their lots so as to create interest and varying vistas as a person moves along the street.
- (2) The design concept should complement the quality of existing development and create a visually pleasing relationship between the proposed and existing projects.

(3) Exterior materials should demonstrate a concern for quality and originality. The use of a particular material should, as a rule, exemplify the special characteristics of the product or be demonstrative of its unique application.

(4) Electrical, mechanical and other equipment should be screened; such screening should be an integral aspect of the project design.

(5) Accessory structures, fences, and walls should be harmonious with the design of the principal structures and should employ compatible building materials.

(6) Project should respect neighboring property's privacy, quiet, function, or views. Service areas including, but not limited to, openings, docks, and equipment, should be located in such a way as to not create a nuisance for adjoining properties.

(7) Circulation patterns and parking areas should be efficient and should not adversely affect traffic on adjoining right-of-ways. Adequate parking should be provided and maneuvering areas for trucks and for drive-through facilities should generally be contained on-site; within the village center overlay district, on-street parking and maneuvering may be permitted consistent with the intent of that district.

(8) Landscaping should be spread over the entire site and be visually interesting all year. Landscaping should screen parking and storage areas and shade parking lots.

(9) The project should consider solar orientation and provide for efficient energy use.

(10) The project should be harmonious with the topography of the site in order to minimize grading.

(11) Signs should be harmonious with the overall design concept of the project; materials and colors should be consistent with those used for the principal structures.

28.5

ACTIONS OF THE PLANNING COMMISSION

The Planning Commission may recommend design review approval as

the project was submitted or may recommend changes in the design or the application may be denied. A design review approval may be recommended subject to such conditions as the Planning Commission may prescribe. Conditions may relate to, but are not limited to, site design, including relationship to surrounding uses, functions of buildings or portions of site, setback, coverage, amount of open space, infrastructure improvements; shape, height, and bulk of structures; distances between buildings; architectural design of structure including exterior materials, colors, and textures; location of ancillary equipment; locations of points of ingress and egress; location, amount, and design of parking areas; location and efficiency of truck maneuvering and loading areas; landscaping, including location and general nature; signs including location, size, design, and height; lighting; walls and fences, including location, height, and materials; project grading; and project phasing.

28.6

FINDINGS OF THE PLANNING COMMISSION

The Planning Commission shall make the following findings before recommending a design review approval:

- (1) The project, as approved, will comply with the goals and objectives of the Lake Elsinore General Plan and the North Peak Specific Plan.
- (2) The project complies with the design directives contained in the North Peak Specific Plan Section 28.0 "Design Review."
- (3) Conditions and safeguards pursuant to Section 28.5, "Action of the Planning Commission," including guarantees and evidence of compliance with conditions, have been incorporated into the approval of the subject project to insure development of the property in accordance with the objectives of this North Peak Specific Plan and the district in which the site is located.

28.7

ACTION BY THE CITY COUNCIL

A report of the findings and recommendations of the Planning Commission shall be transmitted to the City Council after the Planning Commission hearing, except that denials by the Planning Commission shall be final unless appealed. If approved, the City Council shall make its own determination as to whether the proposed project complies with the provisions of Section 28.6, "Findings of the Planning Commission," and may approve, modify, or disapprove the recommendations and actions of the Planning Commission.

The decision of the Planning Commission shall be final ten days from the date of the decision unless an appeal has been filed with the City Council pursuant to the provisions of the Zoning Ordinance.

28.8

MINOR DESIGN REVIEW

Minor Design Review is intended for projects which are significantly smaller in scale than those for which a normal design review procedure is appropriate. The Planning Commission may approve, deny, or conditionally approve the following projects subject to the provisions of this section:

- (1) Additions to commercial and industrial structures or permitted accessory structure in excess of 2,000 square feet.
- (2) Duplexes not involving more than a total of four units.

The Community Development Director or their designee may approve, deny, or conditionally approve the following projects subject to the provisions of this section:

- (1) Single-family detached dwellings, including in-fill and development in approved subdivisions.
- (2) Additions to commercial and industrial structures or the construction of accessory structures of 500 square feet or less in area.
- (3) Additions to residential dwellings.
- (4) Alterations to existing structures which substantially change the appearance of the structure even though the floor area may not be affected (i.e. changes in the shape or exterior materials).
- (5) The following residential accessory structures: Unenclosed structures such as carports, gazebos, and patio covers (both trellis-type and solid) as well as enclosed structures containing less than 600 square feet of floor area such as sheds, cabañas, children's play-houses, and work shops.
- (6) Reconstruction of commercial structures which have been destroyed or substantially damaged, provided said structures were legally in existence and said reconstruction returns the structure to its original size and appearance.
- (7) Signs pursuant to Section 27.0, "Sign Regulations".
- (8) Modifications of site plans for building movement within a range of

0-15 feet.

(9) Modification of setbacks within a range of 0-5 feet.

(10) Substitutions of materials which are in conformance with the original design approval.

Application for minor design review shall be as prescribed by Section 29.3, "Design Review Application".

The Planning Commission or the Community Development Director shall be empowered to approve a minor design review as the project was submitted or may require changes in the design or may deny the project and instruct the Building Division to issue no building permits for the project.

The Planning Commission or the Community Development Director shall only approve a minor design review when the findings required by Section 29.6, "Findings of the Planning Commission", have been made.

The person submitting the project for minor design review may appeal the Community Development Director decision and/or conditions to the Planning Commission; provided that said appeal be submitted to the Community Development Director within 10 days. The Planning Commission shall hear the appeal in the same manner prescribed for a Design Review pursuant to the provisions of this Chapter. Planning Commission decisions may be appealed to City Council. No construction related to the minor design review shall commence prior to Planning Commission or Community Development Director action/approval.

The Community Development Director may also elect to submit any minor design review to the Planning Commission when, in the opinion of the Community Development Director, any of the following may be applicable:

- (1) The project may significantly affect properties other than the applicant's and additional public notification and input is warranted.
- (2) The project requires an environmental impact report or a negative declaration.
- (3) The project may be generally controversial within the community.
- (4) The project will need a variance.

Unless specific provisions are specified for minor design review, for the general administrative purposes of this Chapter, the terms design review and minor design review shall be interchangeable.

28.9

LAPSE OF DESIGN REVIEW APPROVAL

A design review approval shall lapse and shall become void one year following the date on which the design review became effective, unless prior to the expiration of one year, a building permit related to the design review is issued and construction commenced and diligently pursued toward completion. Notwithstanding conditions to the contrary, a design review granted pursuant to this Section shall run with the land for this one year period and shall continue to be valid upon a change of ownership of the site which was the subject of the design review application.

28.10

MODIFICATION OF DESIGN REVIEW APPROVAL

Any alteration or expansion of a project for which there has been a design review approval as well as all applications for modification or other change in the conditions of approval of a design review shall be reviewed according to the provisions of this Chapter in a similar manner as a new application.

28.11

RE-APPLICATION

Following the denial of a design review application or the revocation of a design review approval, no application for a design review for the same or substantially the same design concept on the same or substantially the same site, shall be filed within one year from the date of denial or revocation of the design review.

28.12

OCCUPANCY

No structure which has received a design review or minor design review approval shall be occupied or used in any manner or receive a Certificate of Occupancy until the Director of Community Development has determined that all conditions of approval have been complied with.

Abut. Contiguous to; two adjoining lots with a common property line are considered to be abutting.

Access or access way. The place, means or way by which pedestrians and vehicles shall have safe, adequate and usable ingress and egress to a property or use.

Accessory Structure. A structure, including patio cover located on the same lot with a principal building serving an incidental and secondary use to the main building or the use of the land. It shall not apply to second units.

Accessory Use. A use that is incidental and secondary to the principal use of the main building or the use of the land and devoted exclusively to the main use of the lot or building.

Adjacent Means the same as abutting, however, public rights-of-way and major utility easement shall not be construed as separating adjacent uses.

Agriculture. The tilling of soil, the raising of crops, horticulture, small livestock farming, dairying or animal husbandry and related uses.

Alley. A public or private way at the side or rear of property providing secondary access to abutting properties. Design speed shall not exceed 15 m.p.h. The alleyway can be either one-way or both directions and can have parallel parking on one side.

Amenity. A natural or man-made feature which enhances a particular property but may or may not be a normal requirement of the zoning ordinance.

Animal Hospital. A place where animals or pets are given medical or surgical treatment, and where the boarding of the same animals is limited to short-term periods incidental to the hospital care.

Apartment Complex. An apartment house or two or more apartment houses constructed and operated as one multiple residential entity.

Apartment House. A structure containing four or more apartment units.

Apartment Unit. One or more rooms with private bath and kitchen facilities comprising an independent self-contained dwelling unit not owned in fee simple.

Applicant. Property owner or legal authorized agent representing the property owner.

Architectural Feature. A part, portion, or projection that contributes to the beauty or elegance of a building or structure, exclusive of signs, that is not necessary for the structural integrity of the building or structure or to make said building or structure habitable.

Area, Gross. The total land area within a lot or parcel of land or within a designated portion of a lot or parcel of land.

Area, Net. That portion of a lot or parcel of land exclusive of public rights-of-way, public easements, major utility easements, or other encumbrances which preclude the property owner from having full use of the entire surface of the land.

Arterial Road. A road containing 4 lanes of travel or more separated by a median and designated on the Lake Elsinore General Plan as a Major Road. Within North Peak, Nichols Road and portions of El Toro Road are designated as Arterial Roads

Automobile Parking. Parking of operational and street legal motor vehicles on a temporary basis within an off-street parking area, or on a public or private street.

Automobile Service Station. A use providing motor fuel, oil, tires, batteries, small parts and accessories, and services incidental thereto for automobiles, light trucks, and similar motor vehicles.

Automotive Services. A use engaged in sale, rental, service, or major repair of new or used automobile, trucks, trailers, boats, motorcycles, mopeds, recreational vehicles, or other similar vehicles, including tire recapping, painting, body and fender repair, and engine, transmission, air-conditioning, and glass repair and replacement and similar services

Bar. An establishment used primarily for the serving of liquor, by the drink, to the general public.

Berm. Man-made mound(s) of earth, eighteen inches in height or higher used for decorative, screening or buffering purposes.

Bikeway. A paved pathway, usually separated from streets and sidewalks, designed to be used by bicyclists.

Building Coverage. The gross area of a lot or parcel of land occupied by all of the ground floor of a building or structure which is under roof. As a percentage, it is the relationship between the ground floor area of the building under roof and the net area of the site.

Building Height. The vertical distance from the average finished grade of the pad to the highest point of the structure.

Building. Any structure built or maintained for the support, shelter or enclosure of persons, animals, chattels or property of any kind. The work "building" as used in this title includes the work "structure."

Building, Accessory. Same as "accessory structure."

Business Face. The square footage of the front of the building or store unit, computed by multiplying the lineal frontage by the height extend-

ing from finished grade to the ceiling line of the most upper story. For the purposes of the North Peak Specific Plan, each side of the building or store unit upon which a sign may be located shall also be considered a business face.

Business Frontage. The lineal footage of any side of a business building facing an adjacent street or the unit's designated automobile parking area and upon which a sign may be located.

Business. The purchase, sale or other transaction or place thereof involving the handling or disposition of any article, substance or commodity for livelihood or profit, including in addition, operation or provision of any service or service establishment, office building, outdoor advertising sign and/or structure, recreational and/or amusement enterprise conducted for livelihood or profit.

Carport. A roofed structure, open on two or more sides, designed for automobile parking.

Circulation Element. The adopted Circulation Element of the General Plan of the City of Lake Elsinore which designates routes for all streets and arterial highways within the highways within the City.

City Council. The City Council of the City of Lake Elsinore
City. The City of Lake Elsinore.

Clinic. An establishment where patients are admitted for examination and treatment by one or more physicians, dentists, psychologists or social workers and where patients are not lodged overnight.

Cluster Development. A development design technique that concentrates buildings in specific areas on the site to allow the remaining land to be used for recreation, common open space and/or preservation of environmentally sensitive features.

Collector Road. See Street, Collector.

Commercial Recreation. A use providing recreation, amusement, or entertainment services, including theaters, bowling lanes, billiard parlors, skating arenas and similar services, operated on a private or for-profit basis, but excluding uses defined as outdoor recreation services.

Commercial Use. A business, normally involving office, retail sales, or service uses.

Common Area. Land in a development held in common and/or single ownership and not reserved for the exclusive use or benefit of an individual tenant or owner.

Conditional Use. A use that requires special consideration because of its unusual characteristics, so that it may be properly located with

respect to the effects on surrounding properties.

Conditional Use Permit. An approval required for a conditional use to be permitted in a district or zone.

Condominium, Condominium Development. A building, or group of buildings, in which units are owned individually, and the structure, common areas and facilities are owned by all the owners on a proportional basis, as generally described in Section 783 of the California Civil Code.

Congregate Care. A residential complex intended for the sole occupancy by residents sixty-two years of age and older and having a common dining facility and no kitchen facilities in the individual units.

Convalescent Home. See "rest home."

Conventional Development. A development other than a condominium apartment or cluster development, with each dwelling unit situated on a single residential lot of record and not having a common wall.

County. The County of Riverside.

Courtyard. An open, unoccupied space, other than a required yard, on the same lot with a building or buildings and which is bounded on two or more sides by such building(s).

Cal-De-Sac. A dead-end road or street.

Curb Radius. The curved edge of the street at an intersection measured at the inner edge of the parking lane.

Day Care, Large Family. The use of a residential dwelling unit and the lot upon which it is located for the daytime care of seven to twelve children, including those who reside at the home, for periods of less than 24 hours per day, while the parents or guardians are away.

Day Care, Small Family. The use of a residential dwelling unit and the lot upon which it is located for the daytime care of six or fewer children, including those who reside at the home for periods of less than 24 hours per day, while the parents or guardians are away.

Density, Gross. The number of dwelling units within the gross area of a project divided by the total number of gross acres.

Density, Net. The number of dwelling units within a project divided by the number of net acres.

Design Review. The process of City review and approval of development proposals pursuant to the provisions of Section 28.0, Design Review.

Director. The Community Development Director for the City of Lake Elsinore, or his designee.

Display Frontage. The lineal footage of the front of an area used for display and sale of merchandise located outdoors; typically the portion of a display area facing a street or automobile parking area.

District. A specifically delineated area in North Peak for which regulations and requirements uniformly govern the use, placement, spacing, and size of lots and buildings.

Drive-in. An establishment which provides parking facilities and service to those facilities in order that patrons may utilize on-site goods and/or services without leaving their vehicles. Said drive-in service may be in conjunction with, or exclusive of, any other form of service, including drive-through or conventional seating.

Drive-through. An establishment which offers service via a convenience automobile drive aisle and associated facilities in order that patrons may utilize goods and/or services without leaving their vehicles. Said drive-through service may be in conjunction with, or exclusive of, any other form of service, including drive-in or conventional seating.

Driveway. An unobstructed paved area providing access to a vehicle parking, loading, or maneuvering facility.

Duplex. A structure on a single lot containing two dwelling units, each of which is totally separated from the other by an unpierced wall extending from ground to roof or an unpierced ceiling and floor extending from exterior wall to exterior wall, except for a common stairwell exterior to both dwelling units.

Dwelling Unit. A building designed exclusively for the occupancy of one family for living and sleeping purposes and having a kitchen facility for only one family.

Dwelling, Multiple-family. See apartment.

Dwelling, Single-family Attached. Two or more dwelling units, each owned in fee and located on individual lots but joined along a single lot line, each of which is totally separated from the other by an unpierced wall extending from ground to roof.

Dwelling, Single-family Detached. A dwelling unit owned in fee and located on an individual lot which is not attached to any other dwelling unit by any means.

Easement. A recorded right or interest in the land or another, which entitles the holder thereof to some use, privilege or benefit out of or over said land.

Eating Place. An establishment which is used for the serving of food

and beverages to patrons for compensations, exclusive of those establishments offering only convenience seating (eight or fewer seats) or offering just take-out service.

Educational Institution. See school

Elementary School. A school for grades typically from kindergarten through eighth.

Enclosed Building or Structure. A building enclosed by a permanent roof and on all sides by solid exterior walls pierced by windows and customary entrance and exit doors.

Enclosed Space. An area enclosed on all sides by a solid physical barrier, such as a solid wood fence or masonry wall.

Equestrian Trail. A natural surfaced path for equestrian use.

Erect. To build, construct, attach, hang, place, suspend, or fix with regards to a structure.

Existing use. The present conduct of an activity, or the performance of a function or operation, on a site or in a building or facility.

Exterior Boundary. The perimeter of any lot or parcels of land or group of lots or parcels to be developed as an integrated project.

Facade. The exterior wall of a building.

Family. One or more persons immediately related by blood, marriage or adoption living together as a single housekeeping unit in a dwelling unit together with any domestic employees. A group of not more than six unrelated persons living together as a single housekeeping unit with their domestic employees shall also be considered a family.

Farm. A parcel of land used for agricultural activities .

Fast Food Place. An establishment which is used for the serving of food and beverages to patrons for compensation and which provides only convenience seating (eight or fewer seats) and/or offers just take-out service.

Floor Area Ratio. The numerical value obtained by dividing the gross floor area of a building or buildings located upon a lot or parcel of land by the total net area of such lot or parcel of land.

Floor Area, Gross. The total area of a building under roof, in square feet, including to the outside of the exterior walls of all floors.

Frontage. With regards to a lot, that side of a lot abutting on a street, typically, the front lot line. With regards to a building, see "business frontage."

Fence. Any device forming a physical barrier between two areas and constructed of chain-link, louver, stake, masonry, or lumber in accordance with adopted City standards.

Garage Apartment. A rentable, leasable or saleable dwelling unit typically above a detached rear garage, accessory use building or outbuilding which is accessed from an alley or local street and is normally between 350 and 650 square feet.

Garage, Private. An enclosed building, or a portion of a building, used primarily for automobile parking. Garages shall not be used for habitation.

Garage, Public. An enclosed building, other than a private garage, used for the maintenance or temporary storage of motor vehicles.

Gasoline Dispensing Establishments. A business including service stations and other places where motor fuels are dispensed.

General Plan. The adopted General Plan of the City of Lake Elsinore.

Grade. The degree of rise or descent of a surface.

Grade, Finished. The final elevation of the ground surface after development.

Greenhouse. A building or structure including lath-houses which is devoted solely to the propagation, protection or cultivation of flowers or other plants.

Gross Floor Area. See Floor Area, Gross.

Home Improvement Center. A retail service engaged in providing retail sale, rental, service, or related repair and installation of home improvement products, including building materials, paint and wallpaper, carpeting and floor covering, decorating, heating, air conditioning, electrical, plumbing, and mechanical equipment, roofing supplies, yard and garden supplies, home appliances, and similar home improvement products.

Home Occupation. A use customarily conducted entirely within a dwelling and carried on by the inhabitants thereof, which use is clearly incidental and secondary to the use of the structure for dwelling purposes and which does not change the character thereof; it shall not be construed to mean any use which by its nature or scope adversely affects activities or causes substantial diminution in the enjoyment, use or property values of property in the neighborhood.

Home Office Use. Premises used for the transaction of business or the supply of professional services, employing no more than 2 persons - one of whom must be the home owner or renter.

Homeowners Association. A community association which is organized within a development in which individual owners share common

Interests and responsibilities for open space, landscaping or facilities.

Hospital. A facility licensed by the State Department of Public Health for the accommodation and medical care of sick, injured or infirmed persons and includes sanitariums, alcoholic sanitariums and institutions for the cure of drug addicts and mental patients.

Hotel. Any building or portion thereof with access provided through a common entrance, lobby or hallway to six or more guest rooms, and which rooms are designed, intended to be used or are used, rented or hired out as temporary, overnight, or weekly accommodations for guests.

Industry. The manufacture, fabrication, processing or production of any article, substance or commodity or any other treatment thereof in such a manner as to change the form, character or appearance thereof. In addition, it shall include trucking facilities, warehousing, storage facilities, businesses serving primarily industry, and similar enterprises.

Infrastructure. Any and all of the public facilities and services needed for development of a lot or parcel of land.

Institutional Use. A non-profit or quasi-public use or institution, such as a church, library, public or private school, hospital, or municipally owned or operated building, structure or land, used for public purpose.

Island, Traffic. A raised barrier, sometimes landscaped, located in a vehicle travel path for the Purpose of directing circulation patterns.

Kennel. Any property where four or more dogs or cats, over the age of four months, are kept or maintained for the purpose of boarding, breeding, raising, or training.

Kiosk. A freestanding structure upon which temporary information and/or posters, notices and announcements are posted.

Kitchen. Any area of a building intended or designed to be used or maintained for the cooking and/or preparation of food.

Land Use District. See "district."

Landscaping. The planting and maintenance of a combination of trees, shrubs, vines, ground cover, flowers or lawns, in addition, the combination or design which may include natural features such as rock and stone, and structural features, including but not limited to water elements, art works, decorative walks, decorative walls, and benches.

Limited Lodging Use. The provision of a small number of rooms (generally no more than 10 or 12) for short-term letting with food

service before noon only. Commonly referred to as "Bed and Breakfast".

Loading Area. The portion of a site developed to accommodate loading spaces including the related aisles, access drives, and buffers.

Loading Space. An off-street space or berth on the same lot and contiguous with the building it is intended to serve, for the temporary parking of commercial vehicles while loading or unloading. Loading spaces shall not make use of public rights-of-way for the maneuvering of vehicles utilizing the space nor shall they encroach in parking areas or drive aisles.

Lot Area. The total area within the property lines of a lot.

Lot Area, Net. The total area within the property lines of a lot exclusive of public streets or other public dedications of any easements which preclude the property owner from having full use of the property for the purpose(s) for which it is designated.

Lot Coverage. See "building coverage."

Lot Depth. The horizontal distance between the midpoint of the front lot line and the midpoint of the rear lot line.

Lot Width. The horizontal distance between side lot lines, measured at the front setback line, or the average of the front and rear lot lines, whichever is greater.

Lot, Corner. A lot located at the intersection of two streets, or of two segments of a curved street, forming an angle of not more than one hundred thirty five degrees.

Lot, Flag (Panhandle). A lot having access to a street by means of a private driveway, access easement, or parcel of land not meeting the requirements of this Specific Plan for lot width, but having a dimension of at least 20 feet at its narrowest point.

Lot, Interior. A lot other than a corner lot or reversed corner lot.

Lot, Key. A lot, the side line of which adjoins the rear line of one or more adjoining lots.

Lot, Reversed Corner. A corner lot, the street side of which is substantially a continuation of the front lot line of the first lot to its rear.

Lot, Through. A lot other than a corner lot abutting more than one street.

Lot Line. A line bounding a lot as herein defined.

Lot Line, Front. On an interior lot, the lot line abutting the street. On a corner lot, the shorter lot line abutting a street or the line designated as the front lot line by a subdivision or parcel map. On a through lot, the lot line abutting the street providing the primary access to the lot. On a

flag (panhandle) lot, the interior lot line designated as a front lot line by a subdivision or parcel map.

Lot Line, Interior. A lot line not abutting a street.

Lot Line, Rear. A lot line, not intersecting a front lot line, which is most distant from and most closely parallel to the front lot line. In the case of an irregularly shaped lot or lot bounded by only three lot lines, a line within the lot having a length of 10 feet, parallel to and most distant from the front lot line shall be interpreted as the rear lot line for the purpose of determining required yards, setbacks and other provisions of this Specific Plan.

Lot Line, Side. A lot line that is not a front or rear lot line.

Lot Line, Street. A lot line abutting a street.

Lot. Any numbered or lettered parcel shown on a recorded tract map, a record of survey recorded pursuant to an approved division of land, or a parcel map.

Main Use. Any use of a building, structure, or land which is not clearly and entirely incidental, secondary, or accessory to some other use on the same parcel or unit of development.

Motel. A building or group of buildings containing guest rooms or dwelling units designed, intended or used primarily for the accommodation of transient automobile travelers; including, but not limited to, buildings, or building group designated as auto cabins, motor courts, or motor hotels.

Mound. See Berm.

Multi-Phase Development. A development project that is constructed in increments, each increment being capable of existing independently of the others.

Multiple Family Use. The use of a site for two or more dwelling units, which may be in the same building or in separate buildings on the same site.

Multiple lot subdivisions. See conventional development.

Net Density. See Density, Net.

Net Lot Area. See Lot Area, Net.

Net Residential Acre. See Residential Acre, Net.

Nursing Home. See "rest home."

Off-Street Parking Space. A temporary storage area for a motor vehicle that is not located on a dedicated or private street right-of-way.

Office Uses. A building or group of buildings designed for use as a place of business for persons engaged in recognized professions, administrative functions, and/or business activities, and for those support activities related to such uses.

On-Street Parking Space. A temporary storage area for a motor vehicle which is located on a dedicated street right-of-way.

Open Space. Any parcel or area of land or water essentially unimproved and set aside, dedicated, designated or reserved for public or private use for enjoyment, or for the use and enjoyment of owners and occupants of land adjoining or neighboring such open space.

Open Space, Common. Any parcel or area of land or water set aside, dedicated, designated or reserved for use and enjoyment of all owners and occupants of a project. Usable common open space shall constitute area(s) readily accessible, practical, and generally acceptable for active and/or passive recreation uses. In no case shall common open space include required setback areas or contain structures other than those intended for landscape or recreational purposes.

Open Space, Private. A fenced or otherwise screened area designated for a specific tenant or resident and which is devoid of structures and improvements other than patio covers or those structures or improvements intended for landscape or recreational purposes.

Open Space, Public. Open space maintained for the use and enjoyment of the general public.

Outdoor Recreation Service. A privately owned or operated use providing facilities for outdoor recreation activities, including golf, tennis, swimming, riding, or other outdoor sport or recreation, operated predominantly in the open, except for accessory or incidental enclosed services or facilities.

Parcel. An area, parcel, site, piece of land, or property which is the subject of a development action.

Parking Aisle. Driveways which have parking space taking access from either or both sides and which are intended primarily to provide access to parking spaces.

Parking Area or Lot. A portion of a site devoted to the temporary parking of motor vehicles, including the actual parking spaces, aisles, access drives, and related landscaped area.

Parking Space. An area on a lot or within a building, used or intended for use for parking of a motor vehicle, having permanent means of access to and from a public street or alley independently of any other

parking space.

Parkway. A portion of a street or highway right-of-way between the back of the curb and the right-of-way line and which is not intended to be used as part of the roadway.

Patio House. A single-family dwelling on a separate lot with open space setbacks on three sides and with a court. Patio homes may be attached to similar houses on adjacent lots and still meet this definition.

Permitted Use. Any use allowed within district regulations and subject to the restrictions applicable to that land use district.

Pet, Household. An animal clearly considered customary to a residential use, e.g., dogs, cats, birds, and fish.

Planning Commission. The Planning Commission of the City of Lake Elsinore.

Premises. A lot or a building site, or a specified portion of a lot or building site, that contains the structures and the open space needed for the location, maintenance and operation of the use of the property.

Private. Belonging to or restricted for the use or enjoyment of particular persons.

Professional Offices. A use providing professional or consulting services in the fields of law, architecture, design, engineering, accounting, and similar professions.

Public Education Facility. A public school, operated by a public school district and offering instruction in the several branches of learning and study required to be taught in the public schools by the Education Code of the State of California.

Public Utility Installation. Buildings and other structures and equipment owned and operated by a public utility or private utility company subject to regulation by the State Public Utilities Commission.

Public. Belonging or open to, enjoyed and used by and/or maintained for the public generally, but not limited to a facility the control of which is wholly or partially exercised by some governmental agency including the City of Lake Elsinore.

Quasi-public. A use which involves as its primary purpose, the administration of a required government program or a government regulatory program.

Recreation Facilities and Buildings. Facilities intended for use by the residents of a project or specified surrounding residents, which may include, but are not limited to, clubhouses, community swimming pools,

tennis courts, and other similar recreational uses or operations.

Recreational Vehicle. Any device which may be used for camping or recreational purposes and which, with the exception of vehicle and/or utility trailers, is not currently registered for operation on public streets including, but not limited to, camper units, shells, travel trailers, vehicle trailers, utility trailers, boats, airplanes, gliders, off-highway vehicles and other devices used for recreational purposes.

Residential Acre, Net. The area of land devoted to a specific land use, including the building area, parking area, driveways, private roads and accessory buildings or uses, but not including public right-of-way or other land uses.

Rest Home. Premises operated as a boarding home, and in which nursing, dietary and other personal services are furnished to convalescents, invalids, and non-ambulatory aged persons. It does not include premises in which persons suffering from a mental sickness, disease, disorder or ailment or from a contagious or communicable disease are kept, or in which surgical or other primary treatments are performed, such as are customarily provided in sanitariums or hospitals or in which persons are kept or served who normally would be admissible to a mental hospital.

Restaurant. See Eating Place.

Retail. The selling of goods, wares or merchandise directly to the ultimate consumer or persons without a resale license.

Right-of-Way. An area or strip of land, either public or private, on which an irrevocable right of passage has been recorded for the use of vehicles or pedestrians, or both.

Room Addition. An added room that takes access from interior of a principal residential unit.

Salvage Operation or Yard. Any area, lot land, parcel, building or structure or part thereof used for the storage, collection, processing, purchase, sale or abandonment of wastepaper, rags, scrap metal or other scrap or discarded goods, materials, machinery or two or more unregistered, inoperable motor vehicles or other type of junk.

Sanitary Sewers. Pipes that carry only domestic or commercial sewage and into which storm, surface, and ground waters are not intentionally admitted.

Satellite Dish Antenna. A parabolic and/or disc-shaped antenna of either solid or mesh construction, intended for the purpose of receiving communications from an orbiting satellite.

Scenic Easement. An easement, the purpose of which is to limit development in order to preserve a view or scenic area.

Scenic Highway. Any highway designated as a Scenic Highway by an agency of the city, county, state or federal government.

School. Public, parochial and other nonprofit institutions conducting regular academic instruction at kindergarten, elementary, high school, and collegiate levels. Such institution must either; 1) offer general academic instruction equivalent to the standards prescribed by the State Board of Education, or 2) confer degrees as a college or university of undergraduate or graduate standing. This definition does not include schools, academies or institutions, incorporated or otherwise, which operate for a profit, nor does it include commercial or trade schools.

Screening. A method of visually shielding or obscuring one abutting or nearby structure or use from another by fencing, walls, berms, or densely planted vegetation.

Second Unit. A subordinate dwelling unit with complete and independent living facilities attached to or contained within a single-family detached dwelling.

Senior Citizen Housing. Individual living apartment complex intended for the sole occupancy by residents fifty five years of age and older.

Service Business. Direct sale to the public of personal and professional service.

Service Station. A gasoline dispensing establishment offering the sale of gasoline, oil, minor accessories, and minor repair services done indoors for the operation of motor vehicles but not including painting, body work, restoration, auto wrecking, salvaging, radiator rodding or rebuilding, and other than occasional major motor overhauls.

Service. An act, or any results of useful labor, which does not in itself produce a tangible commodity.

Setback Area. The area between the building line and the property line, or when abutting a street, the area between the building line and the ultimate right-of-way line.

Setback Distance. The distance between the building line and the property line, or when abutting a street, between the building line and the ultimate right-of-way line.

Setback Line. A line within a lot parallel to and measured from a corresponding lot line, forming the boundary of a required yard and governing the placement of structures and uses on the lot.

Setback. The area between the building line and the nearest property line.

Shared Parking. Any parking spaces intended to be utilized by more than one use, where persons occupying the spaces are unlikely to need the spaces at the same time

Sidewalk. A paved surface or leveled area separated from the street and used as a pedestrian walkway.

Sign Area. Means and is computed as the entire area within a geometric pattern enclosing the limits of writing, representation, emblem, or any figure together with any material or color forming an integral part of the display or used to differentiate such sign from the background against which it is placed, provided that in the case of a sign designed with more than one exterior surface, the area shall be computed as including only the maximum single display surface visible from any ground position. The supports, uprights, or structures on which any sign is supported shall not be included in determining the sign area unless such supports, uprights, or structures are designed in such a manner as to form an integral background of the display.

Sign, Animated or moving. Any sign or part of a sign which changes physical position by any movement or rotation of which gives the visual impression of such movement or rotation.

Sign, Portable. Any movable external sign that is not permanently secured or attached to an approved structure, support, or anchor.

Sign. Any device or part thereof capable of visual communication or attraction including any announcement, declaration, demonstration, display, illustration, insignia or symbol used to advertise or promote the interest of any person, partnership, association, corporation, institution, organization, product, service, event, location or other business entity by any means, including words, letters, figures, design, symbols, fixtures, colors, illumination or projected images. "Sign" shall not include any official notice, directional, warning, or information signs or structures issued by any federal, state, county or municipal authority.

Sign, Free Standing. Any non-movable sign not affixed to a building.

Sign, Monument. A free standing sign no taller than six feet in height and which is incorporated into a project's landscape design.

Sign, Pole. A free standing sign, the supports or uprights of which have received no substantial architectural enhancement.

Sign, Roof. A sign erected wholly upon or above the roof or highest horizontal plane of a building or structure. Signs placed on portions of a building, the structure of which serves primarily to circumvent the intent of this Section, shall be considered "Roof Signs."

Sign, Temporary. A sign intended to be displayed for a limited period of time not to exceed 31 days. Such signs shall be constructed of cloth, canvas, fabric, wood, or plastic.

Sign, Wall. A sign of solid face construction or individual letters which is mounted to and parallel with an exterior wall of a building. Letter and figures incorporated into such signs shall protrude from the sign face. However, in no case shall any part of a wall sign extend more than one foot out from the surface of the wall upon which is mounted.

Single lot subdivisions. See cluster development.

Small Family Day Care Home. See day care, small family.

Storage Area. An area used or intended for the storage of materials, refuse, or vehicles and equipment not in service. Storage areas shall not incorporate any other areas of project development such as parking areas, landscaping, and yard areas unless specifically authorized by the applicable land use regulations.

Story. That portion of building between the upper surface of any floor and the upper surface of the floor next above, except that if there is no floor above, then the space between such floor and the ceiling or roof above.

Street. A public or private vehicular thoroughfare or right-of-way, other than an alley, which serves as primary access to a property or residential unit and which is wider than twenty-six feet in width, curb to curb.

Street, Local. A low speed, low volume highway primarily for access to residential, business and other abutting property. A local street has parking and a significant amount of pedestrian traffic.

Street, Collector. A medium speed highway abutting similar land uses. The primary function is to collect and distribute trips within a hierarchy of roads and, secondarily, to carry short trips between adjacent neighborhoods. A community collector has emergency parking only and has a significant amount of pedestrian traffic.

Street Furniture. Man-made, above-ground items that are usually found in a street right-of-way such as hydrants, manhole covers, benches, traffic lights and signs, utility poles and lines, parking meters, and the like.

Street Tree. A spreading canopy tree which will supply shade and is naturally adapted to the locale to provide a long, low maintenance, life span. Street trees can be planted in grass strips adjacent to sidewalks or within tree grates placed in the sidewalks.

Structure. Anything that is built or constructed and requires a fixed

location on the ground, including a building, wall, cover, or other edifice of any kind.

Subdivision. The division of a lot, tract, or parcel of land into two or more lots, tracts, parcels or other divisions of land for sale, development or lease.

Swimming Pool. Any tank or pool created by artificial means and designed for the purpose of containing a body of water offering the possibility of use for swimming, bathing or total body immersion and any portion of which exceeds twenty-four inches in depth and which pool or tank is not readily portable in design or construction.

Tavern. See Bar.

Temporary Structure. A structure which is permitted within a land use district without any foundation of footing and which is removed when the designated time period, activity, or use for which the temporary structure was erected has ceased.

Temporary Use. A use permitted within a land use district and established for a fixed period of time with the intent to discontinue such use upon the expiration of the time period.

Terracing. An erosion control method that uses small hills and contours on the land surface to control flooding and runoff.

Tot Lot. An improved and equipped play area for small children.

Tower. Any fabricated structure or device, including guy wires, used to support one or more antennas as defined herein and to maintain said antennas at the proper elevation.

Townhouse. A single-family attached dwelling in a row of such units in which each unit has its own front and rear access to the outside and no unit is located over another.

Tract House. A dwelling in a residential development containing houses similar in size and appearance.

Tract. An area, parcel, site, piece of land, or property which is the subject of a residential development action involving five (5) lots or more.

Trail. A trail or way designed for and used by equestrians, pedestrians, or cyclists using non-motorized bicycles.

Trailer. A structure standing on wheels, towed or hauled by another vehicle, and used for short-term human occupation, carrying materials, goods or objects, or as a temporary office.

Triplex. A dwelling containing three dwelling units, each of which has

direct access to the outside or to a common wall.

Truck Terminal. Premises used for the parking, servicing, repairing, or storage including the storage for rental or leasing purposes, of any truck except where such use is incidental to a permitted use, servicing only said permitted use, and wholly owned by the owners of the permitted use.

Undeveloped Land. Land in its natural state before development.

Unique Natural Feature. That part of natural environment which adds character to a location and which, if altered or damaged, cannot be artificially replaced.

Use. The purpose for which land or a building is occupied, arranged, designed or intended, or for which either land or building is, or may be, occupied or maintained. A use may be passive. For example, parking and/or storage is a use of property.

Utility services. Establishments engaged in the generation, transmission and/or distribution of electricity, gas or steam, including water and irrigation systems and sanitary systems used for the collection and disposal of garbage, sewage and other waste by means of destroying or processing materials.

Variance. Permission to depart from the literal requirements of the City of Lake Elsinore Zoning Ordinance.

Vehicular accessway. A private, non-exclusive vehicular easement affording access to abutting properties.

Wing wall. An architectural feature in excess of six feet in height, which is a continuation of a building wall projecting beyond the exterior walls of a building.

Yard. An open space that lies between the principal building and the nearest lot line. Such yard is unoccupied and unobstructed from the ground upward, except as may be specifically provided for in the zoning ordinance. Unless otherwise specified, a yard is fully landscaped.

Zero Lot Line Development. The location of a building on a lot in such a manner that one or more of the building's sides rests directly on a lot line and complies with all fire code requirements for construction on a lot line.

Zone or Zoning District. See "district."

Zoning Ordinance. The comprehensive zoning ordinance of the City of Lake Elsinore.

APPENDIX A

**RELATIONSHIP OF THE NORTH PEAK SPECIFIC PLAN
TO THE LAKE ELSINORE GENERAL PLAN**

1.1**INTRODUCTION: PRIMARY AND BASIC GOALS**

State law requires that a specific plan be consistent with the adopted general plan of the city or county. It also requires that specific plans discuss the ways in which the proposed plan implements the goals, policies and objectives of the general plan. The following sections contain lists of the approved goals, policies and objectives of the Lake Elsinore General Plan and a brief discussion of the ways in which the North Peak Specific Plan implements those goals, policies and objectives.

PRIMARY GOAL

The General Plan's Primary Goal is to enhance the quality of life for the residents of the City of Lake Elsinore. This implies that the City will seek to ensure the maintenance of an environment that is responsive to the individual's psychological, aesthetic, and physical needs.

Specific Plan Implementation: The Specific Plan will govern future land use decisions to ensure that development under the Specific Plan will enhance the quality of life for residents. The quality of life will be enhanced through the development of three residential villages with support commercial and civic center uses, the use of extensive landscaping and the preservation of open space, and the development of attractive high-quality residential neighborhoods.

BASIC GOALS*Basic Goal 1 :*

Encourage the kind of economic growth and development which supplies jobs and help provide for the needs of existing and future residents, and maintains the City's ability to finance public improvements, and provide for human services.

Specific Plan Implementation:

The provision of limited commercial uses within village centers will help to provide for the needs of surrounding village areas. The implementation of the Specific Plan and related approvals will ensure that the necessary public improvements will be developed both for the Specific Plan area and for off-site areas affected by development under the Specific Plan. The Specific Plan provides for school sites.

Basic Goal 2 :

Ensure that growth and development occurs in an orderly fashion in accordance with adopted policies and procedures governing the use of land, residential development, and distribution of new housing units throughout the Planning Area.

Specific Plan Implementation:

The Specific Plan establishes policies, procedures and standards which govern the use of land, residential development, provision of services and distribution of new housing units.

Basic Goal 3 :

Preserve the natural environment of Lake Elsinore by adopting city-wide and area-specific policies and programs for open space preservation and management of the environment.

Specific Plan Implementation:

The Specific Plan includes an open space plan which preserves more than half of the land within the Specific Plan area as open space. Open space to be preserved will include natural open space, public rights-of-way and other areas visible from major public roads, public parks and private recreation facilities.

Basic Goal 4 :

Encourage the development of cultural, educational and recreational facilities and activities, ensuring their availability to all segments of the population.

Specific Plan Implementation:

The Specific Plan provides for three village centers that will serve as the social, commercial, and recreational center of each village. Each village center will be centrally located and easily accessible to everyone in the village. The Specific Plan also provides for school sites within each village.

1.2**LAND USE ELEMENT GOALS****GOAL 1.1:**

Provide the citizens of Lake Elsinore with a balanced community of residential, commercial, industrial, recreational, and institutional uses necessary to satisfy the social and economic segments of the population.

•Policy 1.1:

It is the policy of the City to establish and maintain a balance of land uses throughout the community.

•Specific Plan Implementation:

The Specific Plan provides for the establishment of three village centers, which will provide social, commercial and recreational uses to residents. A variety of housing types also will be provided.

•Objective (1.1a):

Encourage the development of both existing and new neighborhoods in an orderly fashion, wherever growth does not exceed the capacity of the community to provide necessary services and facilities.

•Specific Plan Implementation:

All infrastructure improvements will be assured prior to or concurrent with development of new neighborhoods in North Peak.

School and park sites will be dedicated in accordance with school district requirements and adopted City ordinances.

•Objective (1.1b):

Encourage the development of commercial centers at strategic points in the Planning Area.

•Specific Plan Implementation:

Neighborhood commercial sites are provided in close proximity to residential areas.

Community and sub-regionally oriented commercial sites are located within the Specific Plan area on Highway 74.

•Objective (1.1c):

Enhance and encourage the revitalization of the Central Business District to serve the needs of permanent residents and tourists.

•Specific Plan Implementation:

No tourist-oriented commercial sites are designated within the project area to compete with such uses in the CBD.

GOAL 1.2:

Enhance the quality of life for Lake Elsinore residents while accommodating gradual development which harmonizes with

the natural environment.

•*Policy 1.2:*

It is the policy of the City of Lake Elsinore to recognize the importance of land uses in determining the quality of life and its effect on the environment.

•*Specific Plan Implementation:*

The Specific Plan provides for the preservation of significant open space and the provision of traditional Southern California landscaping, thereby ensuring that residents will benefit from development which harmonizes with the natural environment.

•*Objective (1.2a):*

Create an environment which is satisfying to the residents of the community, and which will appeal to the many people in Southern California who seek locations for recreation purposes or permanent residence in Lake Elsinore.

•*Specific Plan Implementation:*

A satisfying environment will be created for permanent residents through the establishment of a variety of high-quality residential neighborhoods. Both active and passive recreational areas and expansive open spaces have been integrated into the design of the project.

GOAL 1.3:

Provide an adequate level of public services and facilities for the present and future residents of the City.

•*Policy 1.3:*

It is the policy of the City to ensure that adequate public services and facilities are provided in a timely and adequate manner.

•*Specific Plan Implementation:*

The Specific Plan provides for the timely development of adequate public services and facilities through the phasing and financing sections of the plan.

•*Objective (1.3a):*

Correct existing sewer and water deficiencies prior to, or

concurrent with, the extension of services and facilities to undeveloped areas.

•*Specific Plan Implementation:*

Both on-site and off-site improvements to water and sewer facilities are programmed as part of this project.

•*Objective (1.3b):*

Provide adequate solid waste facilities which are suitably located to serve the Planning Area.

•*Specific Plan Implementation:*

Solid waste disposal services will be arranged prior to the occupancy of residential and non-residential buildings within the project.

•*Objective (1.3c):*

Provide adequate school facilities and services to all new development in the City.

•*Specific Plan Implementation:*

School sites have been incorporated into the land use plan in accordance with the Lake Elsinore Unified School District, Perris High School District and Perris Elementary School District criteria.

20.3

CIRCULATION ELEMENT GOALS

GOAL 2.1:

Provide a network of transportation systems to serve the needs of residents and visitors to the Elsinore Valley.

•*Policy 2.1 (2.1a):*

It is the policy of the City of Lake Elsinore to provide for safe, fast and efficient movement of people and goods within Lake Elsinore and between Lake Elsinore and other parts of the region by an integrated system of streets, freeways, public transit and other transportation facilities.

•*Specific Plan Implementation:*

The project will be connected to other parts of Lake Elsinore

and the region through existing freeways and surface streets, and additional transportation improvements will be provided as necessary to ensure an efficient transportation system.

•*Objective (2.1a):*

Plan for and implement a network of arterial, major, and collector roads that connect the various parts of the City together.

•*Specific Plan Implementation*

Roadways within North Peak have been designed and will be improved to provide adequate access both to areas within North Peak and to other areas of the City.

•*Objective (2.1b):*

Ensure that adequate on-site parking facilities are provided for all land uses.

•*Specific Plan Implementation:*

In conjunction with some on-street parking in the village center deemed desirable to create an active village atmosphere, adequate on-site parking facilities will be provided.

•*Objective (2.1c):*

Maintain the current level of bus services and expand such services as required when demand levels increase.

•*Specific Plan Implementation:*

Transit facilities such as benches, shelters, and pedestrian access will be provided along expanded transit routes as they are developed by the City or other agencies.

•*Objective (2.1d):*

Provide for the safe and convenient use of bicycles throughout the City for recreation and as a viable alternative to the automobile as a form of local transportation.

•*Specific Plan Implementation:*

Bike lanes linking designated City and County routes have been delineated throughout the project and will be improved in conjunction with road construction.

1.4

ENVIRONMENTAL RESOURCES ELEMENT GOALS

GOAL 3.1:

Conserve biotic and physical resources of scientific and ecological value for the benefit of future generations.

•Policy 3.1:

It is the policy of the City of Lake Elsinore to ensure the long-term viability of the community's natural biological environment.

•Specific Plan Implementation:

Sensitive habitats, including riparian habitat and endangered species habitat, will be avoided wherever feasible. Where it is not possible to avoid such habitats, mitigation will ensure provision of additional habitat, either on- or off-site.

Objective (3.1a):

Protect and maintain significant examples of plant and animal life by reducing negative impacts of human activities.

Specific Plan Implementation:

Large areas of undisturbed open space are retained in the Plan; sensitive habitats will be avoided or, where avoidance is not possible, mitigation will ensure provision of replacement habitat.

GOAL 3.2:

Preserve the unique open space character of the Lake Elsinore Planning Area.

•Policy 3.2:

It is the policy of the City of Lake Elsinore to preserve the sense of open space and important scenic and visual resources.

•Specific Plan Implementation:

The Specific Plan preserves more than half of the site as open space. In highly visible areas, natural slopes will be retained. For example, several significant landforms that are visible from Highway 74 will visually shield the community from the highway.

•*Objective (3.2a):*

Protect the physical resources which create the unique scenic and visual character of the Elsinore Valley.

•*Specific Plan Implementation:*

Extensive open space has been retained by utilizing clustered development concepts adjacent to natural areas. Natural slopes will be maintained in highly visible areas, and natural slope conditions will be imitated where feasible.

GOAL 3.3:

Promote the use of mineral, groundwater and air resources with economic or public significance in a manner which will insure their productivity and utility to present and future generations.

•*Policy 3.3:*

It is the policy of the City of Lake Elsinore to enhance the economic potential of the area's natural resources.

•*Specific Plan Implementation:*

No minerals or other resources of economic significance, or major groundwater sources of drinking water, are present on the project site. Impacts on air quality should not exceed state air quality standards.

•*Objective (3.3a):*

Maintain and improve the Planning Area's air quality.

•*Specific Plan Implementation:*

The establishment of Village Centers, with commercial uses to serve Village residents, will reduce vehicle trips. Concentrations of vehicle emissions from traffic generated from the project will be consistent with the planning forecast contained in the Air Quality Management Plan.

GOAL 3.4:

Protect and preserve areas with prime agricultural lands from premature conversion to urbanized uses.

•*Objective:*

Protect and maintain agricultural lands which permit production of food and fiber as well as provide open space views which contribute to the rural character of the Lake Elsinore Valley.

•*Specific Plan Implementation:*

No land classified as Prime Agricultural Land will be converted to urban uses as part of this project.

GOAL 3.5:

Protect and expand areas containing recreational resources in a manner that will preserve the integrity of the resource.

•*Policy 3.5:*

It is the policy of the City of Lake Elsinore to protect and enhance natural resources having recreational value to the planning area.

•*Specific Plan Implementation:*

The Specific Plan preserves more than half of the land within the project area as open space. Two nature parks will be established, and active use parks will be located in each village. Hiking trails will also be provided.

•*Objective (3.5a):*

Utilize the open space system to provide outdoor recreation opportunities for residents and visitors.

•*Specific Plan Implementation:*

Recreational opportunities are available in open space areas adjoining development areas. Hiking trails to North Peak's natural areas will provide access to these open space areas.

GOAL 3.6:

Enhance the physical, mental and spiritual well being of City residents by providing opportunities for relaxation, rest, activity, education, and relationships with their neighbors.

•*Policy 3.6:*

It is the policy of the City of Lake Elsinore to provide a system of public parks, riding and hiking trails, and outdoor recreation facilities which not only preserve significant areas of natural beauty for citizen enjoyment, but which also serves the needs of the citizens in their immediate environments. The system is to be augmented by private outdoor recreation facilities that are compatible with the goals and objectives of the public system.

•*Specific Plan Implementation:*

The Specific Plan preserves more than half of the land within the project area as open space. Two nature parks will be established, and active use parks will be located in each village. Hiking trails will also be provided. A private recreation facility intended for North Peak residents will be located in the village center in North Peak Valley. The facilities will provide space for specialized recreation activities, such as swimming and tennis.

•*Objective (3.6a):*

Provide recreational facilities which are easily accessible to all residents.

•*Specific Plan Implementation:*

Three neighborhood parks have been incorporated into the land use plan in close proximity to residential areas.

The Village Centers will provide a focus for community activities.

GOAL 3.7:

Ensure the preservation and enhancement of Lake Elsinore's historic and cultural resources.

•*Policy 3.7:*

It is the policy of the City of Lake Elsinore to provide incentives for the maintenance and restoration of cultural and historic resources in the City and Planning Area.

•*Specific Plan Implementation:*

No sites of cultural or historic significance are located within the Specific Plan area.

•*Objective (3.7a):*

Identify and preserve historical and cultural resources within the Planning Area.

•*Specific Plan Implementation:*

No sites of cultural or historic significance are located within the Specific Plan area.

•*Objective (3.7.b):*

Identify and preserve significant archaeological sites within the Planning Area.

•*Specific Plan Implementation:*

Significant archaeological sites have been identified and reviewed in detail in the Environmental Impact Report (EIR) for the Specific Plan. Such sites will be preserved, or any impacts on the site will be mitigated as provided in the EIR.

GOAL 3.8:

Provide a living environment free from potential hazards associated with geologic or seismic activity.

•*Policy 3.8:*

It is the policy of the City of Lake Elsinore to prohibit construction of intense urbanized uses in areas of geologic or seismic hazard.

•*Specific Plan Implementation:*

Although the entire City is located in an area of potential seismic activity, the potential impacts of such activity will be mitigated by enforcement of City building code which addresses geologic and seismic safety considerations.

•*Objective (3.8a):*

Reduce the loss of life, property and the economic and social dislocation resulting from geologic and seismic activity.

•*Specific Plan Implementation:*

Although the entire City is located in an area of potential seismic activity, the potential impacts of such activity will be mitigated by enforcement of City building code which addresses geologic and seismic safety considerations.

GOAL 3.9:

Provide a living environment free from potential hazards associated with slope failure or mudslide.

•*Policy 3.9:*

It is the policy of the City of Lake Elsinore to prohibit construction of intense urbanized uses in areas of moderate to steep slopes.

•*Specific Plan Implementation:*

None of the village centers will be constructed in areas of moderate to steep slopes. Within residential areas, slope

failures and mudslides will be prevented by constructing stable slope grading, revegetating slopes, and constructing properly engineered drainage systems. Proposed grades for the site will be designed to increase stability in any unstable areas.

•*Objective (3.9a):*

Reduce the loss of life, damage to property, and social and economic dislocations resulting from slope failures and mudslides.

•*Specific Plan Implementation:*

Drainage will be controlled by constructing more stable slope gradients, revegetating slopes, and constructing properly engineered drainage systems.

During the tentative map preparation process, proposed grades for the site will be designed to increase stability in any unstable areas.

GOAL 3.10:

Provide a living environment free from potential hazards associated with inadequate drainage or flooding.

•*Policy 3.10:*

It is the policy of the City of Lake Elsinore to prohibit construction of intense urbanized uses in areas of potential hydrologic hazards.

•*Specific Plan Implementation:*

The Specific Plan includes a drainage plan which describes facilities necessary to collect storm water and safely convey it to off-site facilities. The drainage plan provides facilities that will protect the specific plan area in the 100-year storm.

•*Objective (3.10.a):*

Minimize loss of life, damage to property, and social and economic dislocation resulting from flood or dam failure hazards.

•*Specific Plan Implementations:*

The project is not located in an area subject to flood or dam failure hazards.

GOAL 3.11:

Provide a living environment free from potential hazards associated with extensive wildland and structural fires.

•Policy 3.11:

It is the policy of the City of Lake Elsinore to restrict construction of structures in areas susceptible to wildland fires, while assuring the availability of adequate fire protection in existing and newly urbanized portions of the Planning Area.

•Specific Plan Implementation:

The threat of wildland fire will be reduced through the installation and maintenance of fuel modification zones between development areas and natural open space. Fire protection will be provided by the Riverside County Fire Department.

•Objective (3.11a):

Reduce the loss of life, damage to property, and the economic and social dislocations resulting from wildland and structural fires.

•Specific Plan Implementation:

The threat of wildland fire will be reduced through the installation and maintenance of fuel modification zones between development areas and natural open spaces; conditions relating to fire safety will be imposed during project review and the building permit process.

GOAL 3.12:

Provide an adequate level of public security.

•Policy 3.12:

It is the policy of the City of Lake Elsinore to assure the availability of adequate police protection.

•Specific Plan Implementation:

Police protection will be provided from the existing Lake Elsinore police station at 117 South Langstaff Street in Lake Elsinore. Response time is 6 minutes for life-threatening emergencies and 26 minutes for non-emergencies.

•Objective (3.12a):

Reduce the loss of life, damage to property, and the economic and social dislocations resulting from breaches in security and violations of law.

•*Specific Plan Implementation:*

Subdivisions and development projects will be designed in keeping with defensible space concepts. Building security requirements will be enforced at building permit stage.

1.5

NOISE ELEMENT GOALS

GOAL 5.1:

Protect and maintain those areas having acceptable noise environments, and provide for the reduction of noise where the noise environment is unacceptable.

•*Policy 5.1:*

It is the policy of the City of Lake Elsinore to establish and support a coordinated program to protect and improve the noise environment in the City.

•*Specific Plan Implementation:*

The project will comply with all City noise standards and requirements.

•*Objective (5.1a):*

Protect and enhance the City's noise environment by simultaneously controlling noise at its source, along its transmission paths, and at the site of the ultimate receiver. First priority shall be given to residential areas to assure an environment free from excessive or damaging noise. Control of noise at its source shall be given priority over changes to residential structures or neighborhoods.

•*Specific Plan Implementation:*

Anticipated noise levels generated by traffic through the project will not exceed City standards; adequate landscape and building setbacks will ensure noise attenuation.

1.6

COMMUNITY DESIGN ELEMENT GOALS

GOAL 6.1:

Improve Lake Elsinore's physical, visual, and historic environments.

•*Policy 6.1:*

It is the policy of the City of Lake Elsinore to create the highest order of visual continuity and functional compatibility among

the various physical and historic components of the Lake Elsinore community.

•*Specific Plan Implementation:*

The Specific Plan will ensure the development of attractive residential neighborhoods and traditional village centers. Extensive landscaping and the preservation of open space will also contribute to an attractive environment.

•*Objective (6.1a):*

Recognize and protect major views in the City with particular attention given to scenic hillsides and the Lake.

•*Specific Plan Implementation:*

Predominant landforms within the project area will be maintained; grading concepts will utilize existing contours to the extent feasible.

Views of the site from important adjacent public rights-of-way have been considered in the development of the plan for North Peak.

•*Objective (6.1b):*

Enhance the general quality of design and emphasize the unique character of each residential neighborhood, and commercial and industrial areas.

•*Specific Plan Implementation:*

Extensive design guidelines and development standards have been incorporated into this Specific Plan; to be applied during Site Plan and Subdivision Map Review.

•*Objective (6.1c):*

Encourage the preservation of buildings which have historic and/or architectural merit.

•*Specific Plan Implementation:*

No buildings worthy of preservation exist on the site.

•*Objective (6.1d):*

Maintain the present scale of buildings within the City.

•*Specific Plan Implementation:*

Development standards contained within this Specific Plan

recognize and are consistent with the present scale of the City.

GOAL 6.2:

Improve the visual quality as well as the physical efficiency of the existing and future circulation systems.

•Policy 6.2:

It is the policy of the City of Lake Elsinore to protect the scenic characteristics of local roads, especially scenic routes.

•Specific Plan Implementation:

The Specific Plan provides that views of the project from Highway 74 will be screened by existing land forms. In keeping with the overall community goal of evoking traditional California community development, all roads and streets within the Specific Plan area will be built to their minimum paved travel width. The vehicular system will be designed to provide convenient automobile access throughout the community.

•Objective (6.2a):

Provide safe, attractive, scenic routes which will serve the motoring public, bicyclists and pedestrians.

•Specific Plan Implementation:

Extensive landscaped corridors are provided along the project circulation system which incorporate ample pedestrian facilities and bike lanes.

•Objective (6.2b):

Increase the clarity of routes for travelers.

•Specific Plan Implementation:

The proposed streetscape program defines the hierarchy of highways and streets serving the project area, providing clearer direction for those utilizing the circulation system.

1.7

HOUSING ELEMENT GOALS

GOAL 8.1:

Provide a variety of housing types proportionally priced and sized to meet resident and community needs.

•Specific Plan Implementation:

The Specific Plan provides for housing which ranges from apartments to large single family homes, thereby providing a

range and variety of housing types priced and sized to meet resident and community needs.

•*Policy 8.1.a:*

The City of Lake Elsinore shall recognize the existing housing needs of current residents.

•*Specific Plan Implementation:*

The Specific Plan will provide a wide variety and range of housing needs. Its implementation will not interfere with the City's efforts to rehabilitate and protect existing housing stock.

•*Policy 8.1.b:*

The City of Lake Elsinore shall recognize the housing needs of the future population.

•*Specific Plan Implementation:*

The Specific Plan provides for the development of more than 4,600 dwelling units, which will help to meet the housing needs of the City's future residents.

•*Policy 8.1.c:*

The City of Lake Elsinore shall recognize the housing needs for residents with specialized needs.

•*Specific Plan Implementation:*

All state requirements regarding handicapped accessibility will be met.

•*Policy 8.1.d:*

The City of Lake Elsinore shall maximize the utilization of local groups such as the Community Development Corporation, Redevelopment Agency, Local Development Corporation, Elsinore Aid and Senior Citizen Information Center to implement housing development and referral programs.

•*Specific Plan Implementation:*

The developer of the project will cooperate with all City agencies and referral programs.

•*Policy 8.1.e:*

Streamlined and clarified administrative procedures for granting approval and issuing permits. Establish time limits for such approvals allowing developers to ascertain development costs.

•*Specific Plan Implementation:*

This policy is a matter for City implementation, not for implementation through the Specific Plan.

•*Policy 8.1.f:*

Actively encourage efforts to private lenders to provide alternative financing methods to make home ownership available to a greater number of households.

•*Specific Plan Implementation:*

This policy is a matter for City implementation, not for implementation through the Specific Plan.

•*Policy 8.1.g:*

Actively assist private developers in identifying and preparing land suitable for housing development for groups with specialized needs.

•*Specific Plan Implementation:*

This policy is a matter for City implementation, not for implementation through the Specific Plan.

GOAL 8.2:

Encourage development in areas of existing public facilities and services.

•*Specific Plan Implementation:*

The Specific Plan area will utilize some existing public services, including fire and police services. As specified in the EIR the project will ensure that adequate and timely public facilities and services are provided.

•*Policy 8.2.a:*

Actively assist private developers in identifying and preparing land suitable for housing development

•*Specific Plan Implementation:*

The Specific Plan area is a large assembled property, which allows for master planning of a desirable residential development.

•*Policy 8.2.b:*

Protect neighborhoods from adverse environmental factors.

•Specific Plan Implementation:

The Specific Plan includes specific standards and guidelines which will ensure that residential neighborhoods will be protected from adverse environmental factors.

•Policy 8.2.c:

Improve the physical character of existing neighborhoods.

•Specific Plan Implementation:

There are no existing neighborhoods in the Specific Plan area.

•Policy 8.2.d:

Provide a comprehensive coordinated effort to improve City infrastructure.

•Specific Plan Implementation:

The Specific Plan provides for the timely development of sufficient infrastructure to serve the development.

GOAL 8.3:

Maintain the existing housing stock through rehabilitation.

•Specific Plan Implementation:

There is no existing housing stock in the Specific Plan area. Therefore, this goal, and the policies which relate to the goal, are not applicable to the Specific Plan area.

GOAL 8.4:

Provide environmentally sensitive and energy efficient housing in the City.

•Policy 8.4.a:

Encourage energy conserving and environmentally sensitive site planning, construction, and rehabilitation techniques.

•Specific Plan Implementation:

The Specific Plan includes a grading plan which will minimize cut and fill, imitate natural conditions wherever feasible and retain natural slopes in highly visible areas. Development has been clustered near natural areas to ensure the maximum preservation of open space. Energy conservation will be taken into account in site planning and design.

GOAL 8.5:

Facilitate communication between public and private sectors in the housing market.

•*Specific Plan Implementation:*

The developer shall cooperate fully with the City in addressing the housing market.

•*Policy 8.5.a:*

Encourage public and private efforts to eliminate all forms of discrimination in housing.

•*Specific Plan Implementation:*

Housing discrimination within the Specific Plan area will not be allowed.

•*Policy 8.5.b:*

Promote representative citizen participation in the formulation, implementation, and review of housing programs.

•*Specific Plan Implementation:*

The policy is intended for implementation by the City, not by the Specific Plan.

Objective 1:

Concentrate on providing public facilities and services in areas of existing development and discourage development which requires extending public facilities and services.

•*Specific Plan Implementation:*

North Peak is directly adjacent to existing and developing residential areas and represents a logical extension of the prevailing urban pattern;

The extension of public facilities and services will be funded by those areas which directly benefit from those facilities and services.

•*Objective 2:*

To provide additional incentives to develop infill housing.

•*Specific Plan Implementation:*

Development of the Specific Plan will not impair the City's ability to provide incentives to develop infill housing.

•**Objective 3:**

To provide for safe and sanitary housing through rehabilitation.

•**Specific Plan Implementation:**

Development of the Specific Plan will not impair the City's rehabilitation efforts.

•**Objective 4:**

To provide for the rehabilitation of existing residential units to preserve them as part of the City's housing stock in affordable housing for existing residents.

•**Specific Plan Implementation:**

Development of the Specific Plan will not impair the City's rehabilitation efforts.

APPENDIX B

**EXCERPT FROM: *SHARED PARKING: A STUDY CONDUCTED
UNDER THE DIRECTION OF THE URBAN LAND INSTITUTE***

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The survey results demonstrated that a reduction in the number of parked vehicles occurs as a result of shared parking. The data were sufficiently consistent to indicate that a quantitative basis for estimating the demand for shared parking does exist. Based upon the findings of the survey, a methodology was developed to determine parking demand for the conditions typically found in a mixed-use development. This methodology is universal in its application and flexible enough to incorporate adjustment factors as necessary to suit specific policies, programs, and market conditions.

THE METHODOLOGY

The methodology involves four basic steps that may be applied, with appropriate background information to an existing or proposed project. Exhibit 25 illustrates the organization and flow of work. The basic flow of work begins with a review of the development plan and proceeds through the four steps (and sub-tasks) to an estimate of demand for shared peak parking. In support of these activities input from other analyses may be added. They could include an additional data base to refine or modify unit parking factors or other characteristics and market analyses.

The methodology is designed to be sequential, but it can be used in an iterative fashion to test the impact of alternative development plans, assumptions, or policies.

Step 1: Initial Project Review

An analysis of shared parking deals with more detailed issues and relationships than traditional analyses of parking demand. Knowledge of the site and intended land use therefore becomes more important. In addition to square footage or other measurements of land use, it is necessary to describe both the physical and anticipated functional relationships between the land uses. While the physical relationships concern the basic physical layout and organization of facilities-for example, vertical or horizontal projects, distances between land uses, surrounding uses, proximity to transportation and other parking facilities- functional relationships concern the intended character and type of land uses and how the project will work. For example, in a project that includes retail, hotel, and office space, retail facilities may be clearly oriented to hotel guests, office workers, or other "captive persons," or to

external shoppers. Early in the planning process for a development, the information describing relationships between land uses may not be available. If not, a set of assumptions and/or alternative development scenarios should be identified for the analysis. A checklist of questions dealing with these assumptions is as follows:

- What is the square footage by use (or number of hotel rooms and theater seats)?
- If a hotel is included, will banquet rooms and convention facilities be available?
- If meeting rooms and convention facilities are provided, what are the intended concept for programs and the intended audience?
- If a cinema is included, how many theaters will it have? What type of programs will be scheduled? What are the assumptions regarding show times?
- If residential space is included, will any parking constraints be observed (reserved parking, for example)?

Step 2: Adjustment for Peak Parking Factor

This step produces an appropriate set of peak parking demand factors. They represent the number of parking spaces needed per unit of land use or other parameter. To determine the factors, the following subtasks are necessary.

Verification of Land Use and Selection of Parking Parameters. The land uses described for the project in step 1 define the specific set of peak parking factors needed for the analysis of parking demand. The parameter for each factor should be verified. Generally, square feet of floor space or rooms or dwelling units would be used; however, other variables might be more appropriate for certain unique activities.

Specifically, the following information must be verified:

- Verify that occupied GLA is to be used, including or excluding common areas.
- Convert convention facilities to equivalent square feet if capacity per person is used in the building program (15 square feet per person may be used if another density factor is not available).

Selection of Parking Factors. A preliminary value should be selected or determined for the set of peak parking factors.

Information could be drawn from three sources: (1) parking factors suggested by the study (see exhibit 26), (2) validated experience of the developer or other local authorities, or (3) new parking field surveys. It is essential to know what season or time of year and mode of travel are represented in the specific source for factors. This information should be described in terms of month of year (by land use) and approximate percent of nonauto use (that is, percent of person-trips made by modes other than auto).

Adjustment for Season. For demand analyses, all parking factors need to reflect the same "design condition." Typically, the 30th highest hour has been used for highway project. Similarly, for development analyses, the appropriate design period must be selected; that is, the peak season for each land use must be determined, based on developer's data, another source, or study results (see exhibit 27).

However, because the design month frequently is different for each land use in a multiuse development, trial and error may be required to determine which month produces the maximum aggregate parking demand. The intent of the exercise is to recognize the "aggregate effects" of seasonality. This concept is the same as that used to determine the impact of daily peaks.

Using the quantity for each land use, test calculations (parking demand factor multiplied by floor space) are made to identify the controlling land use. On this basis, a design month can be selected. Each parking factor is then adjusted to the same month. For example, if December is selected as the design month for a mixed-use project, the retail factor would be the normal peak, but the hotel factor would be factored to a value less than its seasonal peak.

Adjustment for Mode Transportation Used. Just as the parking demand factors must be adjusted to the same season, they must also be adjusted to reflect the mode of transportation used. The recommended approach is a twofold change. First, available peak parking demand factors are adjusted upward to reflect 100 percent auto use. Second, these parking factor for 100 percent auto use are adjusted downward to reflect the expected conditions at the development project being analyzed. for the typical suburban project where transit is not available, the second modification is not needed. However, for downtown project in urban areas where transit may be used

for 10 to 560 percent of the trips , this correction is significant.

The source for data about transportation modes may be specific transportation survey or transportation data available from planning studies for the urban area. The latter choice requires an assessment of the information's applicability to a specific site.

Adjustment for Captive Market. This adjustment is optional because the effect of a captive market are difficult to identify. Without this adjustment, the demand estimate for shared parking would probably be too conservative.

The existence of the captive patron relationship is identified by surveys of employees, visitors, and patrons as well as by parking surveys. Captive markets could be large enough to significantly lower parking demand. The data might indicate a widely ranging relationship that may not be predictable, however. They might be analyzed in a "what if" sense to test the possible impacts. Assuming a representative value of captive market support could reduce parking factors for retail or entertainment uses. an alternative would be to undertake a specific market analysis. This analysis would include a site-specific assessment of the potential for captive market support.

Step 3: Analysis of Hourly Accumulation

This step produces an estimate of hourly parking accumulations for each land use during a typical weekday or weekend day (Saturday). The results of this step identify the shape of hourly accumulation curves for five basic land uses. The curves were reasonably consistent for a wide range of surveyed sites involving office, regional retail, and residential facilities (see exhibit 28). Nonroom-related hotel activities and entertainment uses varied significantly, however. If site-specific data are not available for these two land uses, survey results could be used.

Accumulation curves are then estimated for each land use, based on the selected hourly values described in terms of the percent of maximum design-day parking demand expected at every hour during the day. The parking demand factor (step 2) multiplied by each hourly percentage produces an estimate of parking demand for every land use component by hour of day.

Step 4: Estimate of Shared Parking

The hourly parking demand for each land use is merged to estimate overall shared parking demand for a proposed project. This step is simply the hour-by-hour addition of parking demand for each use to estimate the aggregate accumulation. As noted previously, the method described above should be used for weekday and Saturday conditions to test for the controlling value.

SAMPLE USE OF THE METHODOLOGY

The following sample situation has been devised to demonstrate the use of the recommended methodology.

1. *Objective:* To estimate the peak parking requirements for a proposed mixed-use development.
2. *Plan:* The proposed development has the following components:
 - Office = 400,000 square feet GLA
 - Retail = 300,000 square feet GLA
 - Hotel = 500 rooms plus 5,000 square feet of restaurant and conference facilities with 200 seat capacity.
3. *Location:* The project will be located in the downtown of a medium-size urban community whose regional population is approximately 1.5 million.
4. *Mode Split:* Based on surveys conducted at existing developments in the downtown, it is estimated that 75 percent of employees and patrons and 50 percent of hotel guests will use autos. The number of persons per auto is assumed to be typical (1.2 for employees, 1.8 for patrons, 1.4 for hotel guests).
5. *Captive market:* Based upon regional market surveys, it is estimated that 15 percent of all retail patrons will be office employees within the development. It is also estimated that 50 percent of the hotel restaurant patronage will be generated *outside* the development.

The unadjusted peak parking demand ratios (see Appendix C) for the component land uses are as follows:

•WEEKDAY

Office: 3.0 parking spaces per 1,000 square feet GLA

Retail: 3.8 spaces per 1,000 square feet GLA

Hotel Rooms: 1.25 spaces per room

Hotel restaurant: 10.0 spaces per 1,000 square feet GLA

Hotel conference room: 0.5 space per seat

•SATURDAY

Office: 0.5 parking space per 1,000 square feet GLA

Retail: 4.0 spaces per 1,000 square feet GLA

Hotel rooms: 1.25 spaces per room

Hotel restaurant: 10.0 spaces per 1,000 square feet GLA

Hotel conference room: 0.5 space per seat

Factoring each ratio by the estimated percentage of auto use yields the following adjusted ratios:

•WEEKDAY

Office: $3.0 \times 0.75 = 2.25$ parking spaces per 1,000 square feet GLA

Retail: $3.8 \times 0.75 = 2.85$ spaces per 1,000 square feet GLA

Hotel Rooms: $1.25 \times 0.50 = 0.63$ space per room

Hotel restaurant: $10.0 \times 0.75 = 7.5$ spaces per 1,000 square feet GLA

Hotel conference rooms: $0.5 \times 0.75 = 0.38$ space per seat

SATURDAY

Office: $0.5 \times 0.75 = 0.38$ parking space per 1,000 square feet GLA

Retail: $4.0 \times 0.75 = 3.0$ spaces per 1,000 square feet GLA

Hotel rooms: $1.25 \times 0.50 = 0.63$ space per room

Hotel restaurant: $10.0 \times 0.75 = 7.5$ spaces per 1,000 square feet GLA

Hotel conference rooms: $0.5 \times 0.75 = 0.38$ space per seat

The ratio for retail parking demand also should be factored for market synergy for a weekday, when office employees are present:

Retail (weekday): $2.85 \times (1-0.15) = 2.42$ spaces per 1,000 square feet GLA

The survey data on the captive market in this instance do not estimate the possible synergistic effect resulting from hotel guests' patronage of the retail facilities. To be conservative, therefore, this effect is assumed to be negligible. However, the unadjusted demand ratio for the hotel restaurant (10 spaces per 1,000 square feet GLA) already is based on a typical 50 percent patronage by nonguests. Another very conservative assumption is that the hotel conference facilities are fully used by nonguests.

Next, the ratios for each component land use need to be factored according to the month of the year during which the overall peak parking accumulation would be greatest. In some instances, the peak month for a weekday may not be the same as the peak month for a Saturday. In that case, only by trial and error can the condition (that is, combination of day and month) for peak parking demand be determined. In this instance, however, a tedious trial-and-error analysis can be avoided by an inspection of the relative size of each component land use and the relative differences in peak daily and monthly demands.

Based on the monthly values in Appendix C, the contribution of the hotel components to overall parking demand remains the same on a weekday and a Saturday of a given month. Thus, for a given month, the condition for overall peak parking demand depends only upon the relative size of the retail and office components. Since the office component is large relative to the retail component, it is most likely that the peak condition will occur on a weekday rather than on a Saturday.

The monthly office demand will remain constant, the monthly retail demand will peak during December, and the monthly hotel components will peak during the summer. Based on an inspection, however, the relative contribution of retail parking demand to total project parking demand during December (compared with that of hotel parking demand during the summer) is much larger.

The peak parking demand at the entire development will therefore most likely occur on a weekday in December. The peak parking demand may then be estimated by conducting an hourly parking accumulation analysis using the following weekday ratios, adjusted to the month of December:

Office: $2.25 \times 1.00 = 2.25$ spaces per 1,000 square feet GLA

Retail: $2.42 \times 1.00 = 2.42$ spaces per 1,000 square feet GLA

Hotel rooms: $0.63 \times 0.85 = 0.54$ space per room

Hotel restaurant: $7.5 \times 0.93 = 6.98$ spaces per 1,000 square feet GLA

Hotel conference rooms: $0.38 \times 1.00 = 0.38$ space per seat

An hourly parking accumulation analysis, using the above ratios and the hourly values from Appendix C, reveals that the peak accumulation for the combined land uses would be 1,809 cars, occurring at 2:00 p.m. This result is revealed only by calculating the accumulation for each hour of the day. The calculation for 2:00 p.m. would be as follows:

Adjusted Peak Ratio x Floor Area x 2:00 p.m.
Value (Appendix C)/Peak Value (Appendix C)

For each land use, the calculations are as follows:

Office: $2.25 \times 400 \times (2.9 \text{ divided by } 3.0) = 870$ spaces

Retail: $2.42 \times 300 \times (3.7 \text{ divided by } 3.8) = 707$ spaces

Hotel rooms: $0.54 \times 500 \times (0.5 \text{ divided by } 1.0) = 135$ spaces

Hotel restaurant: $6.98 \times 5 \times (7.2 \text{ divided by } 12.0) = 21$ spaces

Hotel conference rooms: $0.38 \times 200 \times (0.5 \text{ divided by } 0.5) = 76$ spaces

$870 + 707 + 135 + 21 + 76 = 1,809$ total spaces

Because the proposed development will be in a downtown area, this weekday parking demand of 1,809 cars must be assessed relative to the existing surpluses and deficiencies in the supply of parking spaces within walking distance of the development.

As an additional demonstration of the use of this method, four of the test cases included in exhibit 24 have been selected for refined analysis. Exhibits 29, 30, 31, and 32 indicate the results for projects 10, 14, 16, and 17, respectively. The findings indicate refined estimates of peak parking demand, including any assumptions used concerning the adjustments for season, mode of transportation, or captive market.

Project 10. By adjusting the restaurant to the October seasonal factor, and by using a 50 percent captive portion for the hotel restaurant and 50 percent hotel occupancy for the day (indicated by survey data), the shared parking estimate is 638

spaces. This number compares closely to actual parking. Further, this analysis assumes that the conference facilities were not being significantly used on the day of the analysis.

Project 14. By adjusting the restaurant use to an October condition, using the captive market relationship of 10 percent for the restaurant (based on the surveys), and selecting an office factor of 2.3 spaces per 1,000 square feet, the estimated demand would be 1,776 spaces. This number is reasonably comparable to the actual count, but the analysis suggests that further surveys of the project are needed. The use of a lower peak factor needs further verification. It is possible that some of the demand may use off-site parking.

Project 16. By reflecting a seasonal factor for the retail use (75 percent for July) and using a 50 percent captive market factor for the restaurant, the estimate of shared parking is 600 spaces, which agrees with observed counts. The captive factor seems reasonable, given the isolated nature of the project.

Project 17. By reflecting a small but significant use by transportation other than auto (11 to 12 percent) for the three uses (as indicated by the survey) and a seasonal adjustment for the cinema (to December), and by expecting 1.50 persons per car for retail space, the shared parking estimate is 3,054 spaces, which compares closely to the actual count.

These comparisons indicate that the method can produce parking demand estimates that replicate existing conditions. Clearly, detailed data are needed. However, rationalization based on sound assumptions can be used to develop the estimates as well. The simplicity of the methodology allows parametric analysis to test wide variations in input data.

APPLICATIONS TO PLANNING AND DESIGN

Because the methodology estimates potential parking requirements for specific mixed-use developments, it can be used as evidence for a zoning procedure and as a development design tool. Use in zoning procedures is significant because of the parking standards currently used in most urban areas. In many cases, the shared parking analysis will indicate lower parking requirements.

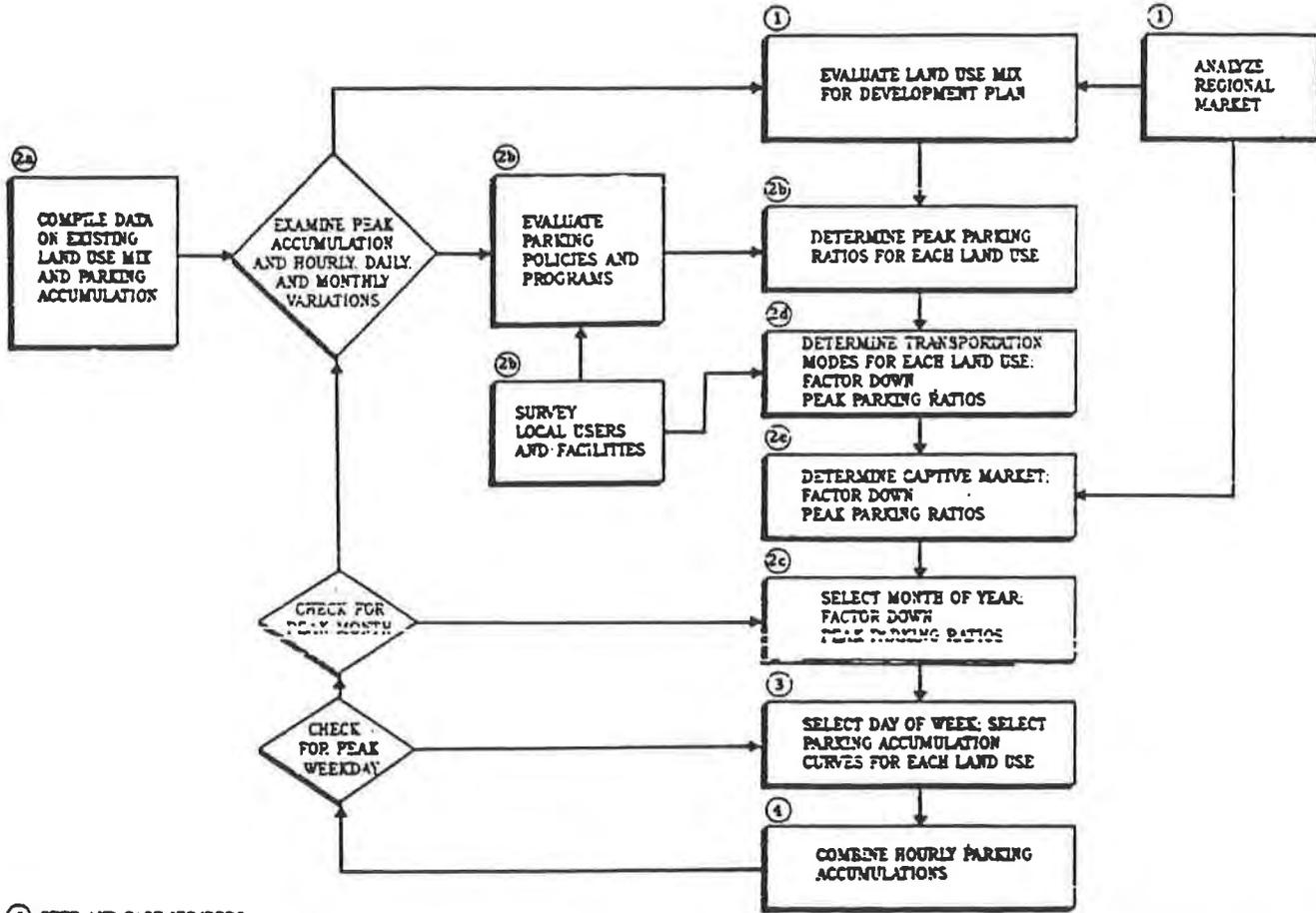
For programming development, the process has several applications. First, the decision to develop a combined land use project is largely determined on the basis of market research and certain physical design constraints like site conditions and building height. The proposed floor areas of each land use of the development plan translate into a peak parking accumulation, using the shared parking methodology. The time of day (or in some instances, the day of the week or season of year) at which the peak parking accumulation occurs is not always obvious without using the proposed methodology. As the nature of the demand distribution is directly related to the relative sizes of the component land uses, not only will the methodology define a combined land use peak parking requirement, which is less than the sum of the individual land use peaks but it will also identify periods of time during which parking facilities are significantly underused. This knowledge may present an opportunity to revise the development plan to increase the marginal size of a component land use without a corresponding marginal increase in the size of parking infrastructure.

This feature can best be described by referring to exhibit 21 (p. 37), which illustrates an office/hotel mixed-use development. The peak shared parking demand for this case would be 1,445 spaces and would occur on a weekday morning; in the evening, the demand would be about 500 spaces. Because 1,445 parking spaces would be provided to serve the midday peak, more than 900 of these spaces would be vacant after 7:00 p.m. and on Saturdays. If the relative quantities of office and hotel space were adjusted, it is possible that the use of parking space could be more equalized. Although these criteria alone may or may not define an optimal development program, the opportunity exists to identify when use of parking space would be at maximum efficiency. This consideration could be an important one for planning the development.

Second, the methodology could be used to evaluate certain policies and programs aimed at reducing parking demand. Some policies and programs are feasible and attractive only in mixed-use projects. For example, mixed-use projects sometimes make transit use more viable because the need to use an automobile, once on the site, is reduced. Conversely, mixed-use projects generate more trips, encouraging better transit service. Some policies and programs that could be evaluated

more effectively with this shared parking methodology are staggered work hours, variable retail and entertainment activity hours, and carpooling programs.

EXHIBIT 25
SHARED PARKING METHODOLOGY



① STEP AND TASK NUMBERS

**EXHIBIT 26
REPRESENTATIVE PEAK PARKING DEMAND FACTORS**

Land Use	Unit	Weekday	Saturday
Office	Parking spaces per 1,000 sq. ft. GLA	3.00	0.50
Retail (400,000 sq. ft.)	Parking spaces per 1,000 sq. ft. GLA	3.80	4.00
Retail (600,000 sq. ft.)	Parking spaces per 1,000 sq. ft. GLA	3.80	5.00
Restaurant	Parking spaces per 1,000 sq. ft. GLA	20.00	20.00
Cinema	Parking spaces per seat	0.25	0.30
Residential	Parking spaces per dwelling unit ^a	1.00	1.00
Hotel			
Guest room	Parking spaces per room	1.25 ^b	1.25 ^b
Restaurant/lounge	Parking spaces per 1,000 sq. ft. GLA	10.00	10.00
Conference rooms	Parking spaces per seat ^c	0.50	0.50
Convention area	Parking spaces per 1,000 sq. ft. GLA ^c	30.00	30.00

^aPer one auto owned per dwelling unit.

^bFactored up to 100 percent auto use from the 80 percent auto use indicated in exhibit 13.

^cUsed by nonguests: the given rates thus are upper bounds, which are very rarely achieved.

**EXHIBIT 27
REPRESENTATIVE MONTHLY VARIATIONS AS
PERCENTAGE OF PEAK MONTH**

Month	Office	Retail	Restaurant	Cinema	Residential	Hotel	Hotel	Hotel	Hotel
						Rooms	Rooms		
January	100%	65%	80%	90%	100%	90%	65%	100%	20%
February	100	65	75	70	100	90	70	100	40
March	100	70	90	50	100	95	80	100	80
April	100	70	90	70	100	95	85	100	80
May	100	70	95	70	100	95	85	100	100
June	100	75	100	100	100	100	90	100	100
July	100	75	100	100	100	100	100	100	50
August	100	75	85	70	100	100	100	100	50
September	100	75	80	80	100	95	90	100	70
October	100	75	80	70	100	95	90	100	70
November	100	80	80	50	100	85	80	100	40
December	100	100	90	50	100	85	65	100	20

EXHIBIT 28
 REPRESENTATIVE HOURLY ACCUMULATION BY
 PERCENTAGE OF PEAK HOUR

Hour of Day	Office		Retail		Restaurant		Cinema	Residential non-CBD		Residen- tial CBD	Hotel				Conference Room	Conven- tion Area
	Daily		Daily		Daily			Daily	Daily		Daily		Daily	Daily		
	Weekday	Saturday	Weekday	Saturday	Weekday	Saturday	Weekday		Saturday	Weekday	Saturday	Weekday			Saturday	Weekday
6:00 a.m.	3%	—	—	—	—	—	—	100%	100%	100%	100%	90%	20%	20%	—	—
7:00 a.m.	20	20%	8%	3%	2%	2%	—	87	95	95	85	70	20	20	—	—
8:00 a.m.	65	60	18	10	5	3	—	79	96	90	65	60	20	20	50%	50%
9:00 a.m.	93	90	42	30	10	6	—	73	81	57	55	50	20	20	100	100
10:00 a.m.	100	80	68	45	20	8	—	66	74	55	45	40	20	20	100	100
11:00 a.m.	100	100	87	73	30	10	—	59	71	55	35	35	30	30	100	100
12:00 Noon	90	100	97	85	50	30	30%	60	71	55	30	30	50	30	100	100
1:00 p.m.	90	80	100	95	70	45	70	59	70	55	30	30	70	45	100	100
2:00 p.m.	97	60	97	100	60	45	70	60	71	65	35	35	90	45	100	100
3:00 p.m.	95	40	95	100	60	45	70	61	73	65	35	40	55	45	100	100
4:00 p.m.	77	40	87	90	50	45	70	66	75	57	45	50	50	45	100	100
5:00 p.m.	47	20	79	75	70	60	70	77	81	90	60	60	70	60	100	100
6:00 p.m.	23	20	82	65	90	90	90	85	85	92	70	70	90	90	100	100
7:00 p.m.	7	20	89	60	100	95	90	94	87	94	75	80	100	95	100	100
8:00 p.m.	7	20	87	55	100	100	100	96	92	90	90	90	100	100	100	100
9:00 p.m.	3	—	61	40	100	100	100	98	95	98	95	95	100	100	100	100
10:00 p.m.	3	—	32	36	90	95	100	99	96	96	100	100	90	95	50	50
11:00 p.m.	—	—	13	13	70	85	80	100	98	100	100	100	70	85	—	—
12:00 Mid- night	—	—	—	—	50	70	70	100	100	100	100	100	50	70	—	—

EXHIBIT 29
APPLICATION OF RECOMMENDED METHODOLOGY
TO TEST CASE NUMBER 10
(Office/hotel)

WEEKDAY

Hour	Office	Retail	Restan- rant	Cin- ema	CBD Resi- dential	Non-CBD Resi- dential	Hotel Room	Hotel Con- ference	Hotel Con- vention	Totals ^a	Observed
6:00 a.m.	16	0	0	0	0	0	136	0	0	152	—
7:00 a.m.	107	0	1	0	0	0	151	0	0	259	—
8:00 a.m.	338	0	3	0	0	0	136	0	0	477	—
9:00 a.m.	499	0	5	0	0	0	121	0	0	625	—
10:00 a.m.	537	0	10	0	0	0	91	0	0	638	594
11:00 a.m.	537	0	15	0	0	0	76	0	0	628	576
12:00 Noon	483	0	25	0	0	0	76	0	0	584	511
1:00 p.m.	483	0	35	0	0	0	76	0	0	594	552
2:00 p.m.	521	0	30	0	0	0	76	0	0	627	594
3:00 p.m.	499	0	30	0	0	0	76	0	0	605	570
4:00 p.m.	414	0	25	0	0	0	91	0	0	529	—
5:00 p.m.	252	0	35	0	0	0	121	0	0	408	—
6:00 p.m.	124	0	45	0	0	0	136	0	0	305	—
7:00 p.m.	38	0	50	0	0	0	136	0	0	224	—
8:00 p.m.	38	0	50	0	0	0	151	0	0	239	—
9:00 p.m.	16	0	50	0	0	0	151	0	0	217	—
10:00 p.m.	16	0	45	0	0	0	151	0	0	212	—
11:00 p.m.	0	0	35	0	0	0	136	0	0	171	—
12:00 Midnight	0	0	25	0	0	0	136	0	0	161	—
SATURDAY											
6:00 a.m.	0	0	0	0	0	0	136	0	0	136	—
7:00 a.m.	18	0	1	0	0	0	151	0	0	170	—
8:00 a.m.	54	0	2	0	0	0	136	40	0	231	—
9:00 a.m.	72	0	3	0	0	0	121	100	0	295	—
10:00 a.m.	72	0	4	0	0	0	91	100	0	266	259
11:00 a.m.	90	0	5	0	0	0	76	100	0	270	306
12:00 Noon	90	0	15	0	0	0	76	100	0	280	281
1:00 p.m.	72	0	23	0	0	0	76	100	0	270	312
2:00 p.m.	54	0	23	0	0	0	76	100	0	252	259
3:00 p.m.	36	0	23	0	0	0	76	100	0	234	290
4:00 p.m.	36	0	23	0	0	0	91	100	0	249	—
5:00 p.m.	18	0	30	0	0	0	121	100	0	269	—
6:00 p.m.	18	0	45	0	0	0	136	100	0	299	—
7:00 p.m.	18	0	48	0	0	0	136	100	0	301	—
8:00 p.m.	18	0	50	0	0	0	151	100	0	319	—
9:00 p.m.	0	0	50	0	0	0	151	100	0	301	—
10:00 p.m.	0	0	48	0	0	0	151	40	0	239	—
11:00 p.m.	0	0	43	0	0	0	136	0	0	178	—
12:00 Midnight	0	0	35	0	0	0	136	0	0	171	—

^aNumbers may not add exactly because of rounding.

EXHIBIT 30
APPLICATION OF RECOMMENDED METHODOLOGY
TO TEST CASE NUMBER 14
(Office/hotel/entertainment)

WEEKDAY

Hour	Office	Retail	Restau- rant	Cin- ema	CBD Resi- dential	Non-CBD Resi- dential	Hotel Room	Hotel Con- ference	Hotel Con- vention	Totals*	Observed
6:00 a.m.	41	0	0	0	0	0	160	0	0	202	—
7:00 a.m.	276	0	8	0	0	0	178	0	0	462	—
8:00 a.m.	869	0	21	0	0	0	160	40	0	1,090	—
9:00 a.m.	1,283	0	41	0	0	0	142	100	0	1,567	—
10:00 a.m.	1,380	0	83	0	0	0	107	100	0	1,670	1,498
11:00 a.m.	1,380	0	124	0	0	0	89	100	0	1,693	1,438
12:00 Noon	1,242	0	207	0	0	0	89	100	0	1,638	1,138
1:00 p.m.	1,242	0	290	0	0	0	89	100	0	1,721	1,243
2:00 p.m.	1,339	0	248	0	0	0	89	100	0	1,776	1,333
3:00 p.m.	1,283	0	248	0	0	0	89	100	0	1,721	1,318
4:00 p.m.	1,063	0	207	0	0	0	107	100	0	1,476	—
5:00 p.m.	649	0	290	0	0	0	142	100	0	1,181	—
6:00 p.m.	317	0	373	0	0	0	160	100	0	950	—
7:00 p.m.	97	0	414	0	0	0	160	100	0	771	—
8:00 p.m.	97	0	414	0	0	0	178	100	0	789	—
9:00 p.m.	41	0	414	0	0	0	178	100	0	733	—
10:00 p.m.	41	0	373	0	0	0	178	40	0	632	—
11:00 p.m.	0	0	290	0	0	0	160	0	0	450	—
12:00 Midnight	0	0	207	0	0	0	160	0	0	367	—

SATURDAY

6:00 a.m.	0	0	0	0	0	0	160	0	0	160	—
7:00 a.m.	60	0	8	0	0	0	178	0	0	246	—
8:00 a.m.	180	0	12	0	0	0	160	40	0	393	—
9:00 a.m.	240	0	25	0	0	0	142	100	0	507	—
10:00 a.m.	240	0	33	0	0	0	107	100	0	480	190
11:00 a.m.	300	0	41	0	0	0	89	100	0	530	190
12:00 Noon	300	0	124	0	0	0	89	100	0	613	171
1:00 p.m.	240	0	186	0	0	0	89	100	0	615	163
2:00 p.m.	180	0	186	0	0	0	89	100	0	555	158
3:00 p.m.	120	0	186	0	0	0	89	100	0	495	122
4:00 p.m.	120	0	186	0	0	0	107	100	0	513	—
5:00 p.m.	60	0	248	0	0	0	142	100	0	551	—
6:00 p.m.	60	0	373	0	0	0	160	100	0	693	—
7:00 p.m.	60	0	393	0	0	0	160	100	0	714	—
8:00 p.m.	60	0	414	0	0	0	178	100	0	752	—
9:00 p.m.	0	0	414	0	0	0	178	100	0	692	—
10:00 p.m.	0	0	393	0	0	0	178	40	0	611	—
11:00 p.m.	0	0	352	0	0	0	160	0	0	512	—
12:00 Midnight	0	0	290	0	0	0	160	0	0	450	—

*Numbers may not add exactly because of rounding.

EXHIBIT 31
APPLICATION OF RECOMMENDED METHODOLOGY
TO TEST CASE NUMBER 16
(Office/retail/entertainment)

WEEKDAY

Hour	Office	Retail	Restan- rant	Cin- ema	CBD Resi- dential	Non-CBD Resi- dential	Hotel Room	Hotel Con- ference	Hotel Con- vention	Totals ^a	Observed
6:00 a.m.	12	0	0	0	0	0	0	0	0	12	—
7:00 a.m.	81	6	5	0	0	0	0	0	0	91	78
8:00 a.m.	255	13	11	0	0	0	0	0	0	280	312
9:00 a.m.	377	31	23	0	0	0	0	0	0	430	420
10:00 a.m.	405	50	45	0	0	0	0	0	0	500	480
11:00 a.m.	405	64	68	0	0	0	0	0	0	537	492
12:00 Noon	365	72	113	0	0	0	0	0	0	549	516
1:00 p.m.	365	74	158	0	0	0	0	0	0	596	600
2:00 p.m.	393	72	135	0	0	0	0	0	0	600	528
3:00 p.m.	377	70	135	0	0	0	0	0	0	582	492
4:00 p.m.	312	64	113	0	0	0	0	0	0	489	516
5:00 p.m.	190	59	158	0	0	0	0	0	0	406	378
6:00 p.m.	93	61	203	0	0	0	0	0	0	356	336
7:00 p.m.	28	66	225	0	0	0	0	0	0	319	—
8:00 p.m.	28	64	225	0	0	0	0	0	0	318	—
9:00 p.m.	12	45	225	0	0	0	0	0	0	282	—
10:00 p.m.	12	24	203	0	0	0	0	0	0	238	—
11:00 p.m.	0	10	158	0	0	0	0	0	0	167	—
12:00 Midnight	0	0	113	0	0	0	0	0	0	113	—

SATURDAY

6:00 a.m.	0	0	0	0	0	0	0	0	0	0	—
7:00 a.m.	14	4	5	0	0	0	0	0	0	22	—
8:00 a.m.	41	8	7	0	0	0	0	0	0	55	—
9:00 a.m.	54	23	14	0	0	0	0	0	0	91	—
10:00 a.m.	54	35	18	0	0	0	0	0	0	107	—
11:00 a.m.	68	59	23	0	0	0	0	0	0	149	—
12:00 Noon	68	66	68	0	0	0	0	0	0	201	—
1:00 p.m.	54	74	101	0	0	0	0	0	0	229	—
2:00 p.m.	41	78	101	0	0	0	0	0	0	220	—
3:00 p.m.	27	78	101	0	0	0	0	0	0	206	—
4:00 p.m.	27	70	101	0	0	0	0	0	0	198	—
5:00 p.m.	14	59	135	0	0	0	0	0	0	207	—
6:00 p.m.	14	51	203	0	0	0	0	0	0	267	—
7:00 p.m.	14	47	214	0	0	0	0	0	0	274	—
8:00 p.m.	14	43	225	0	0	0	0	0	0	281	—
9:00 p.m.	0	31	225	0	0	0	0	0	0	256	—
10:00 p.m.	0	31	214	0	0	0	0	0	0	245	—
11:00 p.m.	0	8	191	0	0	0	0	0	0	199	—
12:00 Midnight	0	0	158	0	0	0	0	0	0	158	—

^aNumbers may not add exactly because of rounding.

EXHIBIT 32
APPLICATION OF RECOMMENDED METHODOLOGY
TO TEST CASE NUMBER 17
(Office/retail/entertainment)

WEEKDAY

Hour	Office	Retail	Restan- rant	Cin- ema	CBD Resi- dential	Non-CBD Resi- dential	Hotel Room	Hotel Con- ference	Hotel Con- vention	Totals*	Observed
6:00 a.m.	5	0	0	0	0	0	0	0	0	5	—
7:00 a.m.	31	170	0	0	0	0	0	0	0	201	—
8:00 a.m.	98	383	0	0	0	0	0	0	0	481	480
9:00 a.m.	144	894	0	0	0	0	0	0	0	1,038	859
10:00 a.m.	155	1,448	0	0	0	0	0	0	0	1,603	1,499
11:00 a.m.	155	1,853	0	0	0	0	0	0	0	2,007	1,818
12:00 Noon	139	2,066	0	117	0	0	0	0	0	2,322	1,858
1:00 p.m.	139	2,129	0	237	0	0	0	0	0	2,506	1,998
2:00 p.m.	150	2,066	0	237	0	0	0	0	0	2,453	1,938
3:00 p.m.	144	2,023	0	237	0	0	0	0	0	2,404	1,758
4:00 p.m.	119	1,853	0	237	0	0	0	0	0	2,209	1,698
5:00 p.m.	73	1,682	0	237	0	0	0	0	0	1,992	1,499
6:00 p.m.	36	1,746	0	293	0	0	0	0	0	2,075	1,439
7:00 p.m.	11	1,895	0	293	0	0	0	0	0	2,199	1,718
8:00 p.m.	11	1,853	0	353	0	0	0	0	0	2,217	1,558
9:00 p.m.	5	1,299	0	353	0	0	0	0	0	1,657	519
10:00 p.m.	5	681	0	353	0	0	0	0	0	1,040	320
11:00 p.m.	0	277	0	293	0	0	0	0	0	570	—
12:00 Midnight	0	0	0	237	0	0	0	0	0	237	—
SATURDAY											
6:00 a.m.	0	0	0	0	0	0	0	0	0	0	—
7:00 a.m.	5	140	0	0	0	0	0	0	0	145	—
8:00 a.m.	15	280	0	0	0	0	0	0	0	296	—
9:00 a.m.	21	841	0	0	0	0	0	0	0	861	—
10:00 a.m.	21	1,261	0	0	0	0	0	0	0	1,282	—
11:00 a.m.	26	2,101	0	0	0	0	0	0	0	2,127	2,008
12:00 Noon	26	2,382	0	117	0	0	0	0	0	2,524	2,381
1:00 p.m.	21	2,662	0	237	0	0	0	0	0	2,919	2,611
2:00 p.m.	15	2,802	0	237	0	0	0	0	0	3,054	2,812
3:00 p.m.	10	2,802	0	237	0	0	0	0	0	3,049	2,869
4:00 p.m.	10	2,522	0	237	0	0	0	0	0	2,769	2,410
5:00 p.m.	5	2,101	0	237	0	0	0	0	0	2,343	—
6:00 p.m.	5	1,821	0	293	0	0	0	0	0	2,120	—
7:00 p.m.	5	1,681	0	293	0	0	0	0	0	1,980	—
8:00 p.m.	5	1,541	0	353	0	0	0	0	0	1,900	—
9:00 p.m.	0	1,121	0	353	0	0	0	0	0	1,474	—
10:00 p.m.	0	1,121	0	353	0	0	0	0	0	1,474	—
11:00 p.m.	0	280	0	293	0	0	0	0	0	574	—
12:00 Midnight	0	0	0	237	0	0	0	0	0	237	—

*Numbers may not add exactly because of rounding.

APPENDIX C

CONDITIONS OF APPROVAL

CONDITIONS OF APPROVAL FOR SPECIFIC PLAN 90-2

Planning Department

1. The text of the August 1990 version of the North Peak Specific Plan and Environmental Impact Report shall be revised to incorporate corrections and revisions and any typographical errors subsequent to approval of the Specific Plan and Environmental Impact Report. A legible set of Specific Plan and Environmental Impact Report exhibits shall be made a part of the final documents. A final Specific Plan and Environmental Impact Report shall be submitted for review and approval by the Community Development Director within 30-days of approval of the Specific Plan and Environmental Impact Report. No permit shall be issued until final administrative approval of the Specific Plan and Environmental Impact Report by the Community Development Director.
2. Developer shall comply with all EIR mitigation measures as identified in the North Peak Mitigation Monitoring Program.
3. Double fronting lots on primary or secondary streets shall have a minimum lot depth of 120 feet as measured from back of curb or a combination of the increased parkway width and lot depth in order to create sufficient separation from these more heavily traveled streets.
4. Front yard setbacks for garages as measured from back of curb shall be seventeen-feet (17'), with minor variations for grading and aesthetic purposes permitted to a minimum of ten-feet (10'), subject to the approval of the Planning Commission.
5. Side yard setbacks shall be a minimum of five-feet (5') with three-feet (3') of level ground excluding slopes. Corner lots shall have a setback of a minimum of ten-feet (10'), of which five-feet (5') shall be level ground.
6. The side and rear elevations of homes shall be well designed and given enhanced architectural treatment, especially when they will be visible from public right-of-way.
7. An open space plan shall delineate areas to be within homeowners' association easements and other maintenance responsibilities and shall be approved by the Community Development Director and the Community Services Director.
8. A neighborhood entry monument and median program shall be designed for use throughout the Specific Plan Area. Placement of entry monumentation shall be in compliance with the City's sight-distance standards.
9. Slopes on individual lots that are in excess of three-feet (3') in height shall be landscaped and irrigation shall be installed by the developer.
10. A wall and fencing plan shall be developed which designated where the various fencing types are to be located, i.e., decorative masonry, or a combination wrought iron and decorative masonry or wood. Said wall and fencing plan shall prohibit wood fencing unless provisions are made for a homeowners' association to ensure maintenance. The wall and fencing plan shall be reviewed and approved by the Community Development Director prior to tentative map approval.
11. In order to break up small lot residential subdivisions with various types of open space, residential projects of 5,000 square foot lots or less shall be located adjacent to passive and/or active open space.

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2.12.91 CC

CONDITIONS OF APPROVAL FOR SPECIFIC PLAN 90-2 CONTINUED

12. For single-family detached housing, on-street parking shall be provided at a minimum of one (1) space per unit/lot. On-street parking can be aggregated within a neighborhood.
13. The grade for local streets shall generally not exceed nine percent (9%) with the exception that grades up to a maximum of fifteen percent (15%) will be considered because of design constraints with the approval of the City Engineer.
14. One side of each single-loaded street shall be posted for "no parking". Painted curbs shall not be allowed.
15. All open space and slopes except for public parks, schools, Bureau of Land Management owned open space, and Flood Control District facilities outside the public right-of-way shall be owned and maintained by either a master owners' association or private owners. All open space areas owned by the homeowners' association shall be offered for irrevocable dedication to the City. Maintenance responsibilities shall be further defined per the provisions of Condition #7 above.
16. Single-family designated planning areas which have a minimum lot size of 5,000 square feet shall have an average lot size of 6,500 square feet, and the number of 5,000 square foot lots shall not exceed twenty percent (20%) of the total units (400 units) averaged over all single-family parcels within each Village.
17. Conditions, Covenants, and Restrictions (CC & R's) shall be submitted to the Community Development Director for review and approval concurrent with the submittal of the first tentative tract map.

Building and Safety

18. The maximum grade for any manufactured slope shall be 2:1 or 50% or as may be approved by the Chief Building Official based upon recommendations by a soils engineer on slope stability.
19. Contour grading shall be practiced throughout the project by using natural curves and variable slopes, including variations to the standard 2:1 slope.
20. Drainage structures required on large slopes shall be designed to avoid an angular layout or rigid geometric patterns.
21. Garage door openers shall be installed in all residential units. In cases where garage doors are set back less than 20-feet from the back of the sidewalk, roll-up doors shall be used and shall be subject to design review.

Community Services

22. Any tentative map approved pursuant to this Specific Plan shall provide for the offer of dedication of park land in accordance with the following schedule: (i) dedication of the Phase I Park (14 cres) prior to or concurrent with the opening of the first model complex; (ii) offer for dedication of the Phase III Park (17 acres) prior to issuance of the 1,900th residential building permit in the Specific Plan area, and dedication to a public entity other than the City the 34-acre parcel of the Nature Park in Village II prior to issuance of the 1,900th residential building permit in the Specific Plan area; and (iii) offer for dedication of the Phase IV Park (18 acres) prior to issuance of the 3,000th residential building permit in the Specific Plan area, and dedication to a public entity other than the City the remaining 196 acres of the Nature Park prior to issuance of the 3,000th residential building permit in the Specific Plan area.

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CONDITIONS OF APPROVAL FOR SPECIFIC PLAN 90-2 CONTINUED

23. Developer shall participate in the City of Lake Elsinore City-wide Landscaping and Street Lighting District pursuant to Resolution 88-27.
24. Prior to approval of the first tentative map within the jurisdiction of each school district, applicant shall have entered into a school impact mitigation agreement with the appropriate school district or, City shall have considered the adequacy of school facilities or available means of financing school facilities to meet the needs and demand of new development proposed in such tentative map to be approved by the City.
25. The landscaped parkway width for Urban Arterials, Major Highways, and Collector Streets shall be a minimum of eighteen-feet (18') measured from back of curb. An average thirty-six-foot-wide (36') landscaped parkway measured along the entire length of each street is desired and can be modified at the tentative map approval subject to the Community Development Director's approval. It is the intent of this standard to allow for reductions of the 36' average for streets with walls on only one side. The parkway gradient perpendicular to the curb shall be a maximum of 5:1 within the first eighteen-feet (18') of parkway measured from back of curb. The remaining landscaped parkway may be graded to a maximum 2:1 slope.
26. The requirement of the City's multi-purpose trail plan shall be incorporated as part of any tentative tract map submittal.
27. The Nature Park shall include at least twenty-five (25) acres of easily accessible public park area pursuant to City standards; available for uses such as picnic areas, trails, tot lots or other turfed areas. Said park shall be credited towards the projects compliance with the City's park dedication provisions.

APPROVED
12.12.91 CC