

**Cottonwood
Hills**
Specific Plan

Cottonwood Hills Specific Plan

Specific Plan 88-1
City Of Lake Elsinore, California
March 14, 1989

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10

11

12

<u>TABLE OF CONTENTS</u>	<u>Page</u>
List of Tables	viii
List of Figures	ix
EXECUTIVE SUMMARY	ES-1
Chapter 1 - INTRODUCTION	1-1
1.1 Community Description	1-1
1.2 Significance of Specific Plan	1-6
1.3 Site Analysis	1-8
1.3a Existing Conditions	1-8
1.3b Development Factors	1-12
Chapter 2 - OVERVIEW OF SPECIFIC PLAN	2-1
2.1 Specific Plan Goals	2-1
2.2 Overall Specific Plan	2-2
2.2a Planning Concepts	2-2
2.2b Land Use Summary	2-3
2.3 Specific Plan Designation	2-7
2.4 General Plan Conformance	2-8
2.5 Conformance with Specific Plan District Ordinance	2-9
Chapter 3 - RESIDENTIAL ELEMENT	3-1
3.1 Residential Land Use	3-1
3.1a Housing Mix	3-1
3.1b Housing Location	3-2
3.1c Balanced Community	3-6
3.2 Residential Design and Development	3-7
3.2a Residential Development Standards	3-7
3.2b Residential Design Guidelines	3-7
3.3 Residential Implementation	3-21
3.3a Unit Transfer	3-21
3.3b Residential Review	3-22
Chapter 4 - COMMERCIAL/INSTITUTIONAL ELEMENT	4-1
4.1 Commercial Land Use	4-1
4.1a Commercial Sites	4-1
4.1b Commercial Development Standards	4-1
4.1c Commercial Design Guidelines	4-5

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Chapter 4 - COMMERCIAL/INSTITUTIONAL ELEMENT (continued)	<u>Page</u>
4.2 Institutional Land Use	4-9
4.2a Institutional Sites	4-9
4.2b Institutional Development Standards	4-10
4.2c Institutional Design Guidelines	4-10
4.3 Commercial/Institutional Implementation	4-12
 Chapter 5 - PUBLIC FACILITIES AND SERVICES ELEMENT	 5-1
5.1 Public Schools	5-1
5.1a School Districts	5-1
5.1b School Facilities	5-1
5.1c School Design Guidelines	5-4
5.2 Public Parks	5-7
5.2a Parks Requirements	5-7
5.2b Neighborhood Parks	5-8
5.2c Community Park	5-8
5.2d Creek Park	5-9
5.2e Project Recreational Facilities	5-9
5.3 Public Services	5-14
5.3a Fire Protection	5-14
5.3b Police Protection	5-14
5.3c Library Services	5-14
5.3d Solid Waste Service	5-15
5.3e Postal Service	5-15
5.4 Utilities	5-16
5.4a Water Service	5-16
5.4b Sewer Service	5-16
5.4c Gas & Electric Services	5-18
5.4d Communications Services	5-18
5.4e Utility Access Roads	5-18
5.5 Public Facilities & Services Implementation	5-19
 Chapter 6 - RESOURCES MANAGEMENT ELEMENT	 6-1
6.1 Open Space System	6-1
6.2 Open Space Design Guidelines	6-12
6.2a Community Landscape Concept	6-12
6.2b Riparian Habitat Areas	6-13
6.2c Natural Open Space Interfaces	6-14
6.2d Fire Management	6-15
6.2e Extraction Operations	6-15
6.3 Open Space Preservation and Maintenance	6-21
6.4 Grading	6-25
6.4a Grading Objectives	6-25
6.4b Community Grading Concept	6-25
6.4c Grading Guidelines	6-27
6.4d Geology and Soils	6-33

Chapter 6 - RESOURCES MANAGEMENT ELEMENT (continued)	<u>Page</u>
6.5 Drainage and Hydrology	6-34
6.5a Drainage Objectives	6-34
6.5b Drainage Concepts	6-34
6.6 Energy Conservation	6-37
6.6a Energy Conservation Measures	6-37
6.6b Subdivision Design	6-37
6.7 Cultural Resources	6-39
6.8 Biological Resources	6-40
6.8a Vegetation	6-40
6.8b Wildlife	6-40
6.9 Water Conservation	6-42
Chapter 7 - CIRCULATION ELEMENT	7-1
7.1 Street System	7-1
7.1a Railroad Canyon Road	7-1
7.1b Major Road System	7-4
7.1c Minor Roads	7-6
7.2 Streetscape Design	7-19
7.2a Streetscape Design Objectives	7-19
7.2b Streetscape Concept	7-19
7.2c Streetscape Design Guidelines	7-20
7.2d Traffic Noise Mitigation	7-21
7.3 Auto Parking	7-27
7.4 Public Transportation	7-28
7.5 Bicycle Circulation	7-30
7.6 Pedestrian/Equestrian Circulation	7-31
7.6a Pedestrian System	7-31
7.6b Hiking/Riding Trails	7-32
7.7 Circulation Implementation	7-35
Chapter 8 - ZONING ELEMENT	8-1
8.1 SF1: Single-Family Detached Residential 1 District	8-3
8.2 SF2: Single-Family Detached Residential 2 District	8-8
8.3 SF3: Single-Family Attached Residential 3 District	8-14
8.4 MF1: Multi-Family Attached Residential 1 District	8-21
8.5 MF2: Multi-Family Attached Residential 2 District	8-29
8.6 C1: Neighborhood Commercial District	8-36
8.7 C2: General Commercial District	8-42
8.8 OS: Open Space District	8-46
8.9 Community Signage	8-48

	<u>Page</u>
Chapter 9 - COMMUNITY WIDE IMPLEMENTATION ELEMENT	9-1
9.1 Implementation Process	9-1
9.1a Development Processing	9-1
9.2b Specific Plan Changes	9-2
9.2 Site Plan Review	9-4
9.2a Site Plan Review Application	9-4
9.2b Administration	9-5
9.3 Phasing Program	9-6
9.3a Development Phasing	9-6
9.3b Transportation Phasing	9-7
9.3c Public Facilities and Services Phasing	9-7
9.3d Phasing of Grading	9-7
9.3e Residential Monitoring Program	9-8
9.4 Financing Program	9-20
9.4a Road Benefit District	9-20
9.4b City Development Agreement	9-20
9.4c School Agreements	9-21
9.4d Community Lighting and Open Space Maintenance District	9-21
9.4e Property Owner Associations	9-21
 Chapter 10 - PLANNING UNITS	 10-1
10.1 Planning Unit 1	10-3
10.2 Planning Unit 2	10-4
10.3 Planning Unit 3	10-5
10.4 Planning Unit 4	10-6
10.5 Planning Unit 5	10-7
10.6 Planning Unit 6	10-8
10.7 Planning Unit 7	10-9
10.8 Planning Unit 8	10-10
10.9 Planning Unit 9	10-11
10.10 Planning Unit 10	10-12
10.11 Planning Unit 11	10-13
10.12 Planning Unit 12	10-14
10.13 Planning Unit 13	10-15
10.14 Planning Unit 14	10-16
10.15 Planning Unit 15	10-17
10.16 Planning Unit 16	10-18
10.17 Planning Unit 17	10-19
10.18 Planning Unit 18	10-20
10.19 Planning Unit 19	10-21
10.20 Planning Unit 20	10-22
10.21 Planning Unit 21	10-23
10.22 Planning Unit 22	10-24
10.23 Planning Unit 23	10-25

Chapter 10 - PLANNING UNITS (continued)

	<u>Page</u>
10.24 Planning Unit 24	10-26
10.25 Planning Unit 25	10-27
10.26 Planning Unit 26	10-28
10.27 Planning Unit 27	10-29
10.28 Planning Unit 28	10-30
10.29 Planning Unit 29	10-31
10.30 Planning Unit 30	10-32
10.31 Planning Unit 31	10-33
10.32 Planning Unit 32	10-34
10.33 Planning Unit 33	10-35

APPENDICES

Appendix A	Legal Description	A-1
Appendix B	Summary of Environmental Impacts and Mitigation	B-1
Appendix C	General Plan Conformance	C-1
Appendix D	Approval Documents	D-1

LIST OF TABLES	<u>Page</u>
1.1 Surrounding Land Uses	1-2
2.1 Land Use Tabulation	2-5
3.1 Housing Tabulation	3-3
3.2 Detached Residential Design Guidelines	3-9
3.3 Attached Residential Design Guidelines	3-14
4.1 Commercial Land Use	4-2
4.2 Commercial Design Guidelines	4-6
4.3 Institutional Design Guidelines	4-11
5.1 Student Generation	5-3
5.2 Elementary School Design Guidelines	5-5
5.3 Neighborhood Park Design Guidelines	5-10
5.4 Community Park Design Guidelines	5-12
5.5a Public Facility Implementation Options/ On-Site Facilities	5-20
5.5b Public Facility Implementation Options/Services	5-22
5.5c Public Facility Implementation Options/Utilities	5-23
6.1 Community Open Space	6-4
6.2 Fire Management Program	6-19
6.3 Community Open Space Preservation and Maintenance	6-22
6.4 Drainage Concepts	6-36
7.1a Streetscape Design Guidelines/Overall Community	7-22
7.1b Streetscape Design Guidelines/Adjacent Natural Open Space	7-24
7.1c Streetscape Design Guidelines/Adjacent Development Projects	7-25
7.2 Circulation Implementation	7-36
8.1 Temporary Sign Permit Period Schedule	8-53
9.1 Residential Phasing Program	9-10
9.2 Transportation Phasing Program	9-12
9.3 Public Facilities Phasing Program	9-16

LIST OF FIGURES

	<u>Page</u>
1.1 Location and Surrounding Uses	1-4
1.2 City Sphere of Influence	1-5
1.3 Slope Analysis	1-9
1.4 Existing Site Conditions	1-10
1.5 Development Factors	1-11
2.1 Land Use Plan	2-4
3.1 Residential Land Use	3-5
3.2 Typical Local Streetscape	3-13
3.3 Off-Site Interface	3-14
3.4 Off-Site Interface	3-15
3.5 River Interface	3-18
3.6 Creek Interface	3-19
4.1 Commercial/Institutional Land Use	4-4
5.1 Public Facilities Plan	5-2
5.2 Utilities Concept Plan	5-17
6.1 Community Open Space	6-3
6.2 Open Space Interfaces	6-16
6.3 Open Space Interfaces	6-17
6.4 Grading Concept	6-26
6.5 Grading Approaches	6-28
6.6 Cut Slope Variations	6-29
6.7 Residential Grading	6-32
6.8 Drainage Concept	6-35
7.1 Area-Wide Street Network	7-2
7.2 Community Street System	7-3
7.3 Street Section A-1 Railroad Canyon Road/Northeast	7-8
7.4 Street Section A-2 Railroad Canyon Road/Southwest	7-9
7.5 Street Section B-1 Cottonwood Hills Road/West	7-10
7.6 Street Section B-2 Cottonwood Hills Road/East	7-11
7.7 Street Section C Holland Road	7-12
7.8 Street Section D Cottonwood Canyon Road	7-13
7.9 Street Section E Lost Road	7-14
7.10 Street Section F Typical Residential Collector Street	7-15
7.11 Street Section G Street "C" - Estate Portion	7-16
7.12 Street Section H/I Typical Local Streets	7-17
7.13 Residential Street Pattern	7-18
7.14 Non-Auto Circulation	7-29
7.15 Trail Section	7-34

LIST OF FIGURES (continued)

8.1 Zoning Map

Page

8-2

9.1 Development Processing

9-3

9.2 Phasing Plan

9-9

10.1 Planning Units

10-2

(1)

(2)

(3)

EXECUTIVE SUMMARY

Cottonwood Hills is a planned residential community situated on 1,968.7 acres of undeveloped land in the western portion of Riverside County. The property is located between Interstates 15 and 215, and is one of a series of existing and proposed projects along the Railroad Canyon Road - Newport Road corridor connecting the two freeways. Surrounding land uses include planned communities, estate lots, rural residential, and undeveloped land.

Concurrent Applications

The principal purpose of the Cottonwood Hills Specific Plan is to provide guidelines for private and public development within the specific plan area, over a build-out period of approximately ten years. The Plan includes goals, land use proposals, design guidelines, and zoning, as well as an implementation program. The Plan reflects the City's Specific Plan District ordinance and is coordinated with the City's zoning code.

The City's General Plan does not currently provide land use designations for the Cottonwood Hills area. The Specific Plan addresses conformance with General Plan policies and objectives, and provides the basis for a General Plan Amendment.

The specific plan area is almost entirely located within the City of Lake Elsinore's Sphere of Influence, and directly abuts the existing City boundary. For the sixty acres at the eastern end of the plan area, amendment of the City's Sphere of Influence to add this property is necessary for annexation. Annexation into the City from Riverside County requires approval of the annexation application by the City and by the Riverside County Local Agency Formation Commission (LAFCO). The application includes the Specific Plan as rezoning for the property; the Plan for the Provision of Municipal Services; and the Fiscal Analysis.

A Vesting Tentative Map for Phase I of the Plan has been submitted in conjunction with the Specific Plan. Preparation of a Development Agreement with the City is also being undertaken with Phase I of the community.

An Environmental Impact Report (EIR) accompanies the Specific Plan, General Plan amendment, Sphere amendment, City annexation, and Phase I applications.

Land Use Plan

The specific plan area consists of relatively flat, developable valley areas ringed by foothills which are not readily developable. The Plan preserves the two major drainageways on-site, the San Jacinto River and Cottonwood Creek, minimizing disturbance to associated riparian habitat areas. The undisturbed steep slope areas are designated as permanent open space as well, resulting in retention of approximately 48 percent of the plan area in natural open space.

The Specific Plan proposes development of 4,275 dwelling units at an overall gross density of 2.17 dwelling units per acre. This overall density is appropriate to site development conditions, permitting a mix of housing densities to accommodate a range of site constraints and to respond to market conditions. In addition, the proposed density is similar to or less than densities of approved nearby specific plan projects, and gives flexibility in locating lower density residential at interfaces with neighboring rural residential areas. Cottonwood Hills will accommodate an estimated 11,415 residents.

A network of major community roadways creates the framework for development within the developable portions of the site. Railroad Canyon Road, proposed for upgrading to a regional arterial highway, provides the primary access to the plan area. Cottonwood Hills Road is planned as a new major highway extending from Railroad Canyon Road, at the primary entrance to the community, to Holland Road, the eastern secondary entrance to the plan area. Connections are provided from Cottonwood Hills Road to off-site rural residential areas along Lost Road and Cottonwood Canyon Road. Bicycles and transit buses will be accommodated on the major road system. A network of residential collector streets serve the various development areas.

Proposed residential development emphasizes single-family detached residential, with a total of 2,628 detached units situated on 727.7 gross residential acres. One-half acre estate lots (SF1) are planned at the eastern and southern plan perimeter, totalling 93 units on 69.5 gross acres. Conventional single-family lots (SF2) are located in the east and central portions of the property, representing 1,893 units on 528.7 gross acres. In areas along major roadways next to school/park complexes, smaller lot single-family (SF3) projects are proposed, making up 642 units on 129.5 acres.

Multi-family attached residential is sited in the western portion of the specific plan area, in proximity to major roadways and support uses. MF1 attached residential includes 549 units on 50.8 acres, with a density range of 8 up to 15 DU/acre to accommodate duplex and townhouse development. Denser residential at up to 24 DU/acre is planned for 59.6 gross acres, yielding 1,098 housing units. Recreational facilities will be incorporated into all attached residential projects.

Community support uses totalling about 103 acres include commercial and institutional development and school/park complexes. Most commercial is clustered in two centers at the community entrance at the Railroad Canyon Road/Cottonwood Hills Road intersection. Two institutional sites are located central to the community, and can accommodate private daycare and schools, churches, private clubs, and public facilities. There are three school/park complexes distributed over the plan area to provide convenient access. These complexes meet community requirements for on-site elementary schools and public parklands. No other public facility sites are required within the specific plan area.

Community open space includes both (1) natural and naturalized open space, which consists of property in its natural condition or graded and landscaped to appear natural; and (2) developed open space associated with proposed development projects and streets, with grading and landscaping to achieve a designed aesthetic effect. The permanent natural open space serves multiple functions, including preserving natural habitat; providing a recreational resource; buffering land uses; providing a landmark, scenic backdrop for roadways, or visual resource for nearby development; and retaining natural topography and rock outcrops. The developed open space creates an attractive visual setting; contributes to an aesthetically cohesive community atmosphere; and provides for active and passive recreation.

Provision of Public Services

Both off-site and on-site street and utilities infrastructure will be provided by the Cottonwood Hills development. All streets within the plan area will be designed as outlined in the Specific Plan, phased as specified in the phasing program, and financed and constructed by the development. Cottonwood Hills is also a participant in a Benefit Assessment District to construct Railroad Canyon Road. Utilities will be installed in conjunction with phased development, including water, sewer, gas, electric, and franchise utilities services. Cottonwood Hills will participate in constructing a new trunk sewer in Railroad Canyon as well.

Fire protection, police protection, solid waste collection, and other municipal services will be provided by the City of Lake Elsinore. A Development Agreement will spell out the financing and other commitments of the City and the Cottonwood Hills developer(s) in regard to all public services and facilities.

On-site drainage facilities will be provided in conjunction with development, and are designed to minimize impacts on neighboring uses. Channel enhancement of upper Cottonwood Creek is proposed to control flooding.

Community Identity

The community design goal of the Specific Plan is to create an attractive, aesthetically coherent community emphasizing well-designed streetscapes and development projects against a backdrop of permanent natural open space. Design guidelines together with development standards for zoning districts are provided to guide design and review of individual development projects.

For residential, commercial, and institutional land uses, design guidelines are set out for project appearance, site relationships, circulation and parking, and interfaces with adjacent uses. Zoning includes standards for setbacks, lot sizes, building height, parking, and signage as well as permitted uses. These guidelines and standards are designed to create attractive planning unit projects, each contributing to the overall identity and character of the Cottonwood Hills community while providing for the public health, safety, and welfare.

Design guidelines are also outlined for public facilities, including schools, parks, and utilities. In addition, streetscape design objectives and guidelines are described in the Specific Plan. The principal streetscape concept is to create visual continuity community-wide through parkway design, including elements such as landscaping, street lighting, signage, and fencing.

Environmental Impacts and Mitigation

All the environmental impacts of the Cottonwood Hills project identified in the Environmental Impact Report have been found to be either not significant, or mitigated by the planning proposals and design guidelines set out in the Specific Plan.

The impacts with mitigation measures spelled out in the Specific Plan include grading; visual quality; biological resources; cultural resources; traffic circulation; services and utilities; hydrology and water quality; geology and soils; noise; energy conservation; and air quality. Mitigation of these environmental impacts will be made conditions of project approvals for planning units.

The plan involves the development of a new planned community to accommodate the residential growth anticipated in the region. In this respect, it contributes to the cumulative environmental effects which are always associated with land development, including increased demands for services and energy; increased traffic and air emissions; and regional reductions in biological habitat. The Specific Plan incorporates several measures which promote the conservation of resources — a balance of land uses and large areas of open space including natural habitat areas. These measures do not, however, reduce these cumulative impacts below a level of significance. The community will contribute substantially to the regional, cumulative adverse conditions.

Community Implementation

The Specific Plan includes an implementation program to provide for orderly build-out of the community within the context of market conditions; ensure the timely provision of adequate infrastructure and other public facilities and services; and delineate implementation processes which insure each planning unit is developed consistent with the provisions of the Specific Plan.

An implementation process is outlined proceeding from the Specific Plan, to tentative tract mapping, to final mapping and improvement plans, to permitting. A Site Plan Review process accompanying tentative mapping is required, to ensure consistency with the Specific Plan. The City's usual Design Review process is utilized during final mapping and construction document review, to provide conformance at a detailed level with the zoning standards and design guidelines of the Specific Plan and tentative map, as well as other City guidelines and requirements.

The Zoning Element of the Specific Plan contains zoning districts for the various proposed land uses. Each planning unit is assigned a zoning district, and the permanent natural open space is placed in an open space zone. The zoning districts are modeled after the City Zoning Code with adjustments for the site parameters, planning goals, and anticipated market conditions of the Cottonwood Hills community. Each zoning district includes permitted uses and development regulations along with references to the current City Code where appropriate.

The proposed phasing program calls for staged development of the community in eight phases. Generally, development would be sequenced from west to east along Cottonwood Hills Road, with projects northwest of Railroad Canyon Road in an open phase. Development is tied to phased transportation improvements, as well as phased provision of public facilities such as schools, parks, and utilities.

The facilities financing program outlines financing mechanisms to fund construction of public facilities, infrastructure, and other capital improvements, and to provide for subsequent maintenance and operations of improvements and open space. Key components to the financing program include:

- o Road Benefit District.
- o City Development Agreement.
- o School District Agreements.
- o Lighting and Open Space Maintenance District.
- o Property Owner Associations.

While the Specific Plan lists options for the financing of improvements and maintenance for each public facility and service, the components above will be developed to spell out precise financing commitments.

Introduction

Chapter 1



Chapter 1 - INTRODUCTION

1.1 Community Description

Cottonwood Hills is a planned residential community situated on approximately 1,969 acres of undeveloped land in the western portion of Riverside County. The property is located between Interstates 15 and 215, and is one of a series of existing and proposed projects along the Railroad Canyon Road - Newport Road corridor connecting the two freeways.

As shown in Figure 1.1, Cottonwood Hills immediately abuts the existing eastern boundary of the City of Lake Elsinore, and is proposed for annexation into the City. Adjacent projects and land uses are summarized in Table 1.1, for both existing and planned conditions. Surrounding land uses include planned communities; estate lots; rural residential; and open, undeveloped acreage.

The Cottonwood Hills Specific Plan proposes development of 4,275 dwelling units at an average gross density of 2.17 dwelling units per acre. While predominantly a single-family detached residential community, a range of housing densities is planned. Support uses including schools, parks, commercial, and institutional are provided for the estimated 11,415 residents. An extensive open space system is proposed, incorporating steep slope areas and prominent landforms as well as the creek and river drainage ways.

Cottonwood Hills encompasses all of Sections 11 and 12 of Township 6 South, Range 4 West, and all of Section 7 and portions of Section 8 of Township 6 South, Range 3 West, San Bernardino Base and Meridian.

Table 1.1

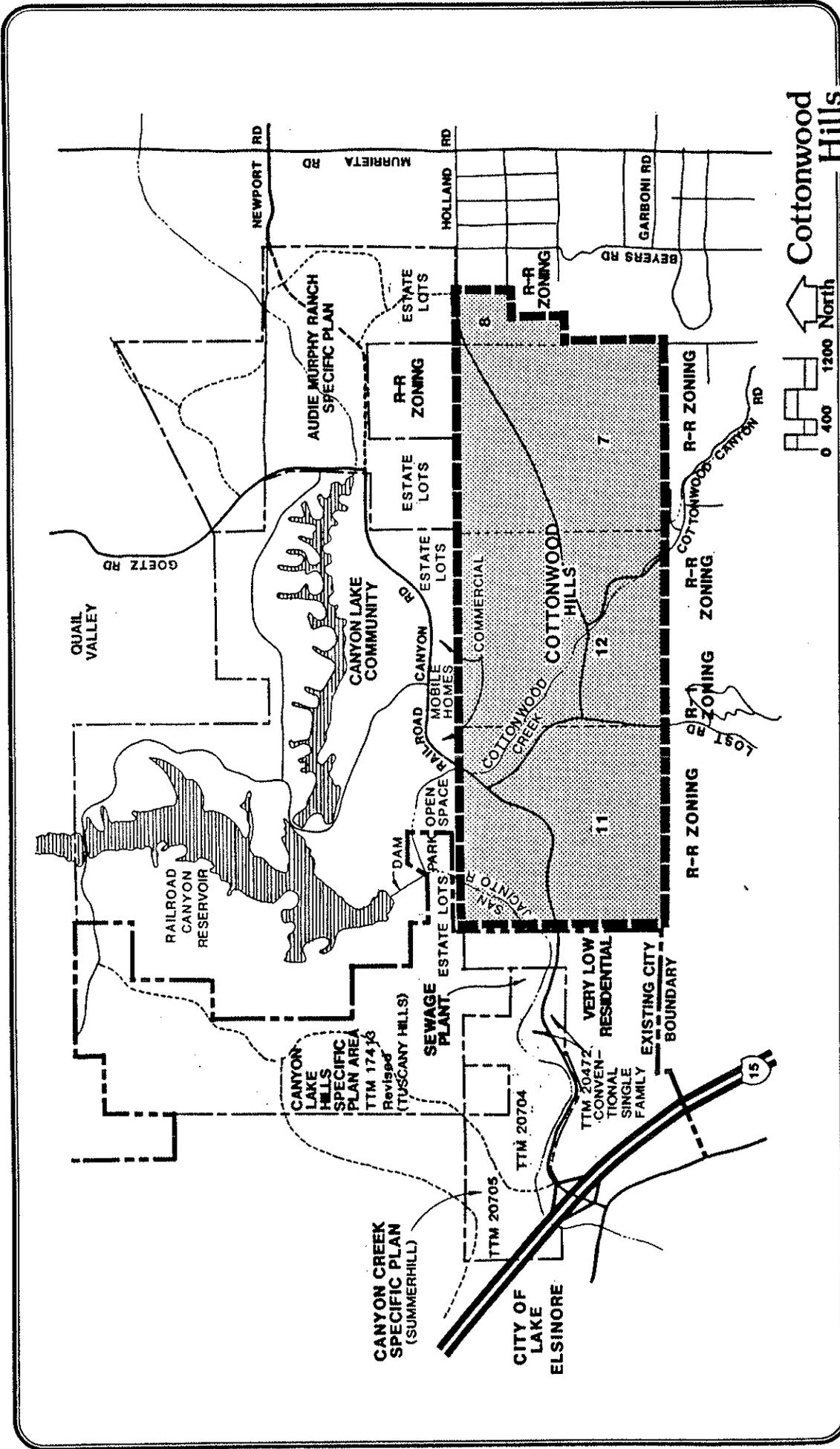
SURROUNDING LAND USES

	Existing Conditions	Planned Conditions
Surrounding North	(a) City of Lake Elsinore -- vacant, under development. Under General Plan, Specific Plan Area (2 du/acre), Flood Plain and Floodway.	(a) City of Lake Elsinore, Canyon Lake Hills (Tuscany Hills) Specific Plan Area (Approved Tentative Tract Map for 856 detached units, Development Agreement for total of 2,000 units) -- estate lots (average 2.75-acre lots), natural park.
	(b) Riverside County, Canyon Lake Community -- under development. Mobile home park, golf course, neighborhood commercial center, equestrian estate lots.	(b) Same.
	(c) Riverside County -- vacant.	(c) Riverside County, Audie Murphy Ranch Specific Plan No. 209 -- estate lots (10-acre lots) on west portion, estate lots (1/2-acre lots with 2-acre lots at boundary) on east portion.
	(d) Riverside County -- vacant. R-R zoning (rural residential with minimum 1/2-acre lots).	(d) Same. Sun City/Menifee Valley Plan under preparation.
Surrounding East	(a) Riverside County -- rural. R-R Zoning (rural residential with minimum 1/2-acre lots).	(a) Same. Sun City/Menifee Valley Plan under preparation.

Table I.1 (continued)

SURROUNDING LAND USES

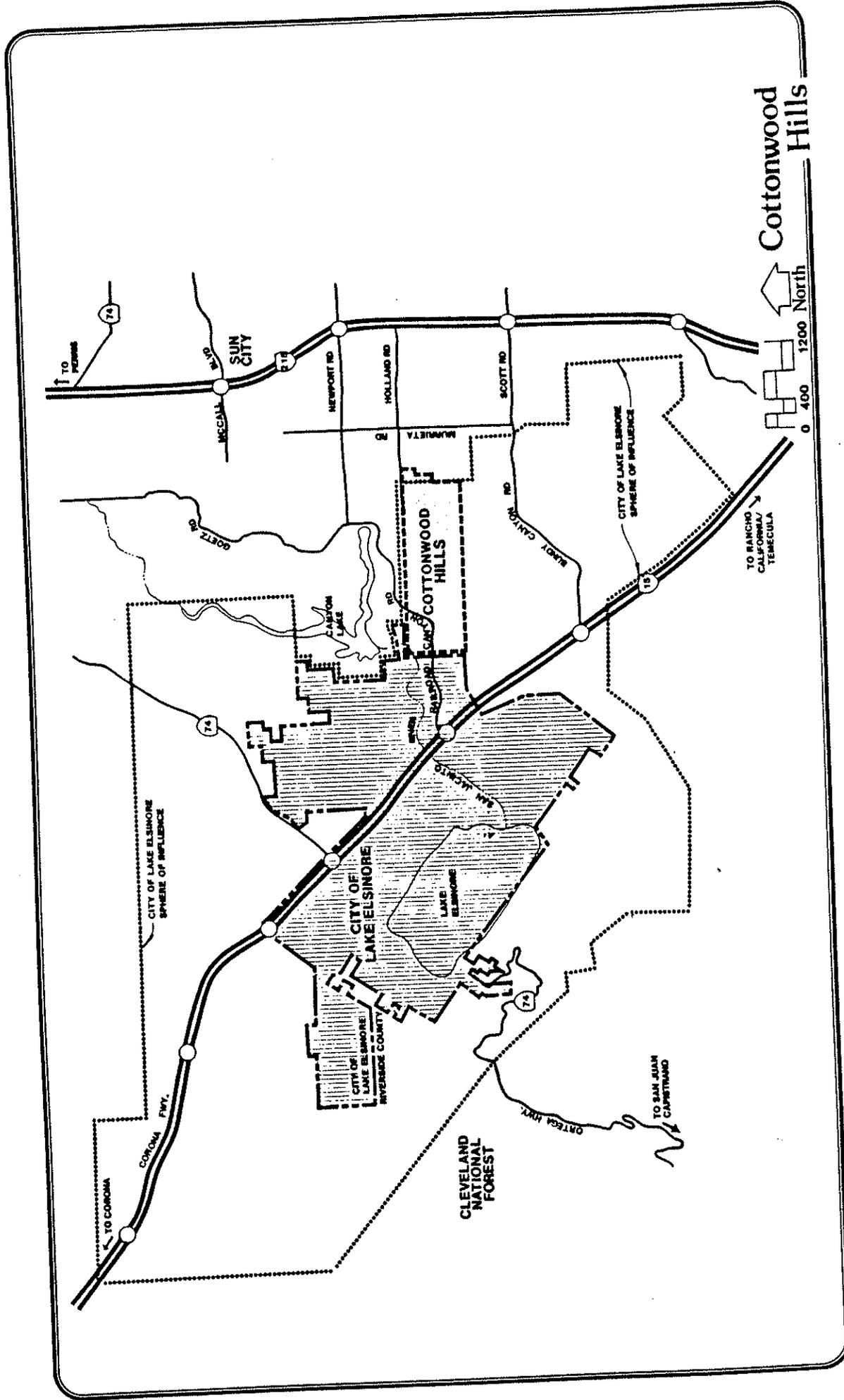
	Existing Conditions	Planned Conditions
Surrounding South	<p>(a) Riverside County -- rural residential, farmland, or vacant. Most in R-R zoning (rural residential with minimum 1/2-acre lots). R-1 zoning in vicinity of Lost Road (one-family dwellings with minimum 7,200 square foot lots). Existing lot sizes from 8,990 square feet and up.</p>	<p>(a) City of Lake Elsinore Sphere of Influence--no designated land use as yet.</p> <p>(b) Riverside County -- Sun City/Menifee Valley Plan under preparation for portion east of Cottonwood Canyon Road.</p>
Surrounding West	<p>(a) City of Lake Elsinore -- Vacant. Under General Plan, Very Low Density Residential (2-acre lots) in R-1 Zoning, Flood Plain and Floodway to immediate west.</p> <p>(b) Further west, Specific Plan Area (3 du/acre), sewage treatment plant.</p>	<p>(a) Immediate west, same.</p> <p>(b) Further west, Canyon Creek (Summerhill) Specific Plan (3 Approved Tentative Tract Maps) -- residential and support land uses.</p>



0 400 1200 North

Cottonwood Hills

Location And Surrounding Uses 1.1



City Sphere Of Influence 1.2

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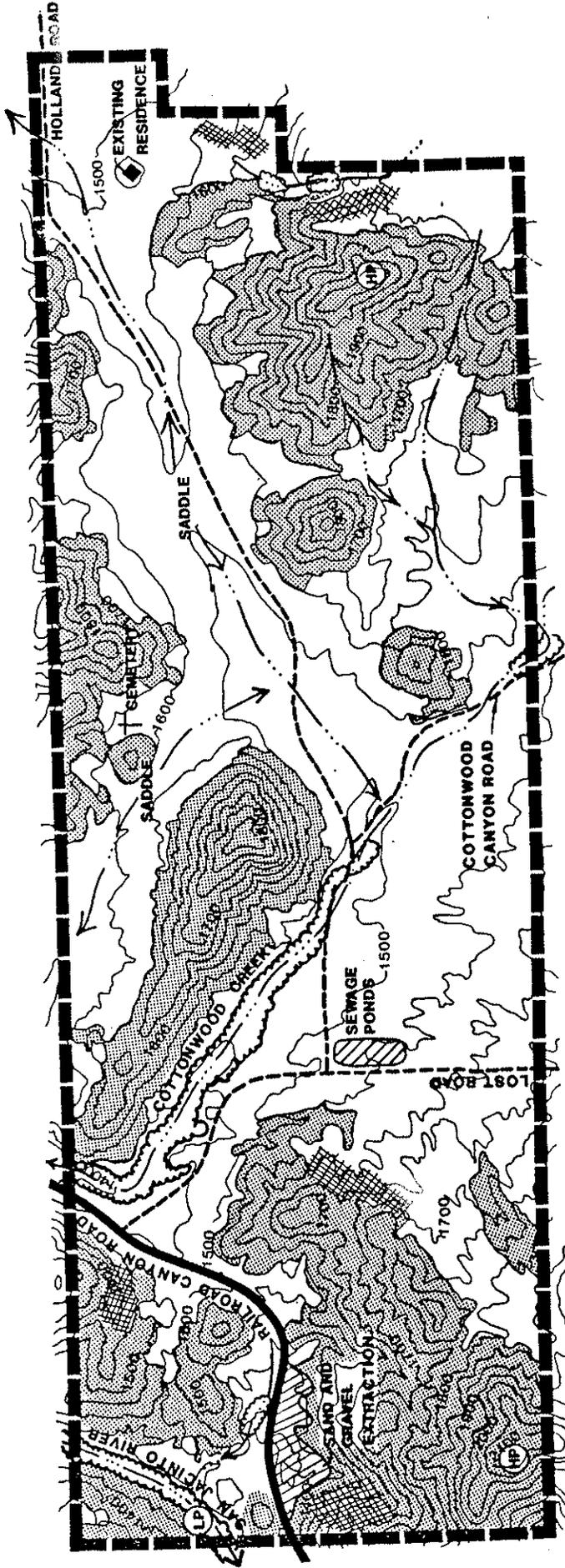
1-5

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PAVED 2-LANE ROAD

DIRT ROAD

DRAINAGE WAY

RIPARIAN HABITAT

ROCK OUTCROPPINGS

HIGH POINT

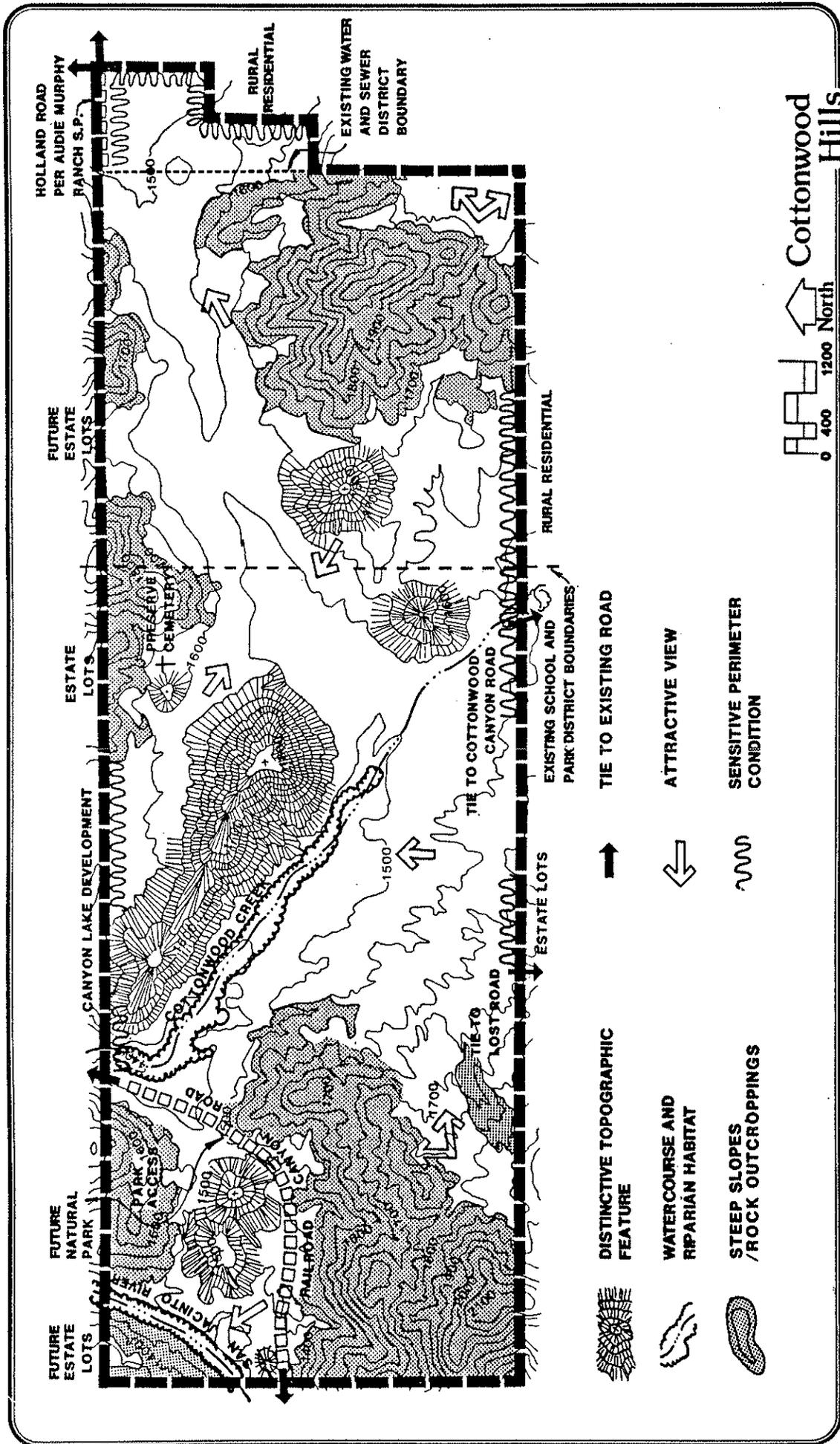
LOW POINT

STEEP SLOPES



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Existing Site Conditions 1.4



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Development Factors 1.5

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1.2 Significance of Specific Plan

The principal purpose of the Cottonwood Hills Specific Plan is to provide guidelines for private and public development within the specific plan area, over a build-out period of approximately ten years. The Plan includes a series of goals and development proposals which are embodied in the Specific Plan Map. In addition, development standards are set out to provide the zoning framework for approval of individual projects. Implementation mechanisms for public facilities, improvements, and open space are outlined, as well as design review guidelines for each land use type.

The Specific Plan meets the requirements of the City's Specific Plan District Ordinance, Chapter 17.99 of the Zoning Code. It is intended to implement the City of Lake Elsinore General Plan for the specific plan area.

Annexation

As illustrated in Figure 1.2, the Cottonwood Hills plan area is almost entirely situated within the City of Lake Elsinore's Sphere of Influence, and directly abuts the existing City boundary. For this reason, the Specific Plan has been prepared for review, approval, and implementation through the City of Lake Elsinore, and assumes the property is wholly annexed into the City. The Plan reflects the requirements of the City's "SPD Specific Plan District" Ordinance and is coordinated with the City's Zoning Code and Subdivision and Grading Ordinances. While the City's General Plan does not currently provide land use designations for recently added portions of the Sphere of Influence, the Specific Plan is consistent with General Plan policies and objectives, and provides the basis for a General Plan Amendment.

In this context, the approval of the Specific Plan by the City of Lake Elsinore constitutes the pre-zoning ordinance for annexation from Riverside County into the City. The pre-zoning is a necessary component of the annexation application when it is reviewed by the Riverside County Local Agency Formation Commission (LAFCO). In addition, the Specific Plan provides the basis for the Plan for the Provision of Municipal Services, another annexation application component.

For the 60 acres at the eastern end of the specific plan area, amendment of the City's Sphere of Influence to add this property is necessary for the annexation. The City of Lake Elsinore must be the initiating party for the sphere amendment, and it may be processed through LAFCO concomitantly with the annexation application.

When annexed into the City of Lake Elsinore, the Specific Plan replaces the General Plan and Zoning Ordinance of Riverside County as the development controls for the Cottonwood Hills site. However, to the extent feasible, the Plan has been coordinated with County circulation requirements and neighboring County land uses.

Subdivisions

Under the City's Specific Plan District Ordinance, an overall master subdivision map encompassing the entire property, and/or tentative subdivision maps and site plans for individual projects may be approved concurrently with the Specific Plan. The Specific Plan provides the guidelines for preparation and review of all subdivision maps and site plans within the specific plan area. A site plan review process will ensure conformance with the Specific Plan for each development project.

Environmental Review

The Specific Plan is accompanied by an Environmental Impact Report (EIR) which describes existing site conditions, identifies project impacts, and prescribes mitigation measures. The mitigation measures have been incorporated into the Specific Plan document in order to enhance the plan as well as facilitate review of individual development projects by the City's Planning Division. The EIR is also a required component for the annexation application.

Specific Plan Changes

The City's Specific Plan District Ordinance specifies that minor changes to the approved Specific Plan may be made by the Community Development Director, provided the proposed changes are consistent with the purpose and character of the approved plan. All modifications or amendments to the Specific Plan other than minor changes must be processed as a Specific Plan Amendment and are subject to all Specific Plan procedures.

Public Participation

Approvals by the City of Lake Elsinore Planning Commission and City Council, and the Riverside County LAFCO, are generally subject to public hearings. This includes adoption of the Specific Plan; initiation and approval of the Sphere of Influence Amendment; approval of the City Annexation; amendment of the City General Plan; certification of the Environmental Impact Report; and approval of Tentative Tract Maps.

1.3 Site Analysis

This section summarizes the existing conditions on the Cottonwood Hills property, as well as factors to be considered in planning the community.

1.3a Existing Conditions

Figure 1.3 presents a slope analysis of the specific plan area in three slope categories. Generally speaking, the flattest areas in the 0-10 percent category are considered very developable, other factors permitting. Areas in the intermediate category of 10 to 25 percent slopes can usually be developed with proper design and engineering. Areas exceeding 25 percent in slope are ordinarily not developable in standard subdivisions, although small areas may be utilized to connect more developable areas or for special uses such as estate lots or water storage facilities. As Figure 1.3 illustrates, the central valley portions of the site are the flattest and most easily developed. There are large portions of the specific plan area in steep slopes which are not readily developable.

Figure 1.4 provides a generalized overview of the topographic conditions, drainage ways, and site features existing on the property. The natural character of the plan area is set by the major drainage ways--the San Jacinto River and Cottonwood Creek--in combination with the topographic contrast of the valleys and relatively level areas with steep slopes and peaks. The low and high points in the plan area are 1,300 and 2,170 feet, respectively, an elevation differential of approximately 870 feet. Most of the flatter areas have been disturbed by dry farming, grazing, and/or hunting activities, while the steeper foothills remain generally undisturbed.

There are four types of water course areas in the specific plan area:

- o The San Jacinto River, running in a riparian-vegetated, relatively narrow valley through the northwest corner of the site.
- o The lower portion of Cottonwood Creek, through a moderately flat area in the west central portion of the site, with considerable riparian oak woodland vegetation.
- o The upper portion of Cottonwood Creek, over a flat area with flooding potential and with relatively few trees, in the south central part of the site.
- o Scattered small drainage courses with some riparian oak woodland vegetation, in steeper areas of the site.

The more vegetated river and upper Cottonwood Creek areas constitute visual and habitat resources.



SLOPES 0 TO 10%



SLOPES 10% TO 25%



SLOPES 25% AND GREATER



0' 400 1200 North Cottonwood Hills

Project Design Consultants

3-14-89

Slope Analysis 1.3

Railroad Canyon Road, a two-lane paved road, runs through the northwest corner of the specific plan area. Cottonwood Canyon Road, Holland Road, and Lost Road are all existing dirt roads which traverse the property and connect off-site to the larger regional circulation system. There are additional minor dirt roads on the property, such as parallel to the San Jacinto River and in the foothills in the southeast portion of the site.

Among existing site features are a series of sewage ponds in the west central area of the property, which can be removed for development. The present sand and gravel extraction operations on the south side of Railroad Canyon Road will be eventually discontinued. A small historic cemetery is located in the north central portion of the specific plan area. At the eastern end of the property, an existing residence can be removed for future development.

Existing neighboring uses off-site are summarized in Section 1.1.

1.3b Development Factors

A series of development factors shape the planning proposals for the Cottonwood Hills Specific Plan. These factors are described below and depicted in Figure 1.5:

- (1) Realignment and widening of Railroad Canyon Road, to accommodate projected cumulative regional and community traffic.
- (2) Consideration of off-site circulation planning and arrangement of the specific plan circulation system to tie into off-site roadways, including Lost, Cottonwood Canyon, and Holland Roads; including utilities linkages where needed.
- (3) Consideration of projected noise levels along Railroad Canyon Road and other major roads through the project.
- (4) Recognition of the existing and proposed land uses adjacent to the specific plan area, identifying and treating sensitive perimeter conditions.
- (5) For the drainage ways,
 - o Preservation of the San Jacinto River and lower Cottonwood Creek riparian areas as habitat and scenic resources.
 - o Enhancement of the upper Cottonwood Creek area as a riparian resource.
 - o Mitigation of impacts to those scattered minor riparian areas utilized for development.
 - o Consideration of off-site impacts and future off-site development requirements in drainage design and improvements.

- (6) Recognition of the valley - foothills topography in the designation of relatively flat developable areas, the preservation of steep slopes and distinctive features, and the overall grading concept.
- (7) Maximization of attractive off-site and on-site views in the grading and planning of residential areas and the public street system.
- (8) Minor adjustments to service district boundaries where logical to best serve the proposed community. This may include the water and sewer district boundary, the school districts' boundaries, and park service jurisdictions.

Development of portions of the specific plan area will result in the loss of prime agricultural soils with the potential for production. These soils are located in the flatter, developable portions of the site. The loss in lands and potential agricultural income is not considered significant in terms of regional agricultural production. The property has not been dry farmed for over five years, because it is not considered profitable. There are no irrigation systems or other capital improvements for agricultural use of the property. The surrounding areas are being converted or have been converted to development so the project does not constitute premature conversion of agricultural lands to urbanization. The urbanized use of the specific plan area represents long-term benefits to City, compared to continued use for hunting activities.

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Overview Of Specific Plan

Chapter 2

2

1

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Chapter 2 - OVERVIEW OF SPECIFIC PLAN

2.1 Specific Plan Goals

The overall goal for Cottonwood Hills is TO CREATE A MASTER PLANNED RESIDENTIAL COMMUNITY WHICH IS SENSITIVE TO THE PHYSICAL ENVIRONMENT AND ENHANCES THE QUALITY OF LIFE OF THE CITY AND REGION. The goals subsumed under this overall goal are outlined below.

City General Plan: Insure the Specific Plan and subsequent development are consistent with the policies and objectives of the City of Lake Elsinore General Plan.

Residential Land Use: Create a balanced community, with a range of housing products emphasizing single-family detached residential, located in areas appropriate to site conditions, the circulation network, and surrounding land uses.

Commercial/Institutional Land Use: Encourage attractive, accessible commercial and institutional development which meets community needs, and complements the full range of activities available in central Lake Elsinore.

Public Facilities: Develop public school and park facilities and utilities infrastructure adequate to support and enhance community residential development.

Open Space: Establish an extensive system of open space as an aesthetic amenity, environmental preserve, and recreational resource for the community.

Circulation: Provide an adequate system for vehicular, transit, bicycle, and pedestrian circulation within the community, while tying to the larger region.

Community Design: Create an attractive, aesthetically coherent community, emphasizing well-designed streetscapes and development projects against a backdrop of permanent natural open space.

Implementation: Insure the timely provision of adequate facilities and services to meet community needs, and provide for the ongoing operation and maintenance of community facilities and open space.

2.2 Overall Specific Plan

This section provides a summary description of the proposed land use and circulation plan for the Cottonwood Hills community.

2.2a Planning Concepts

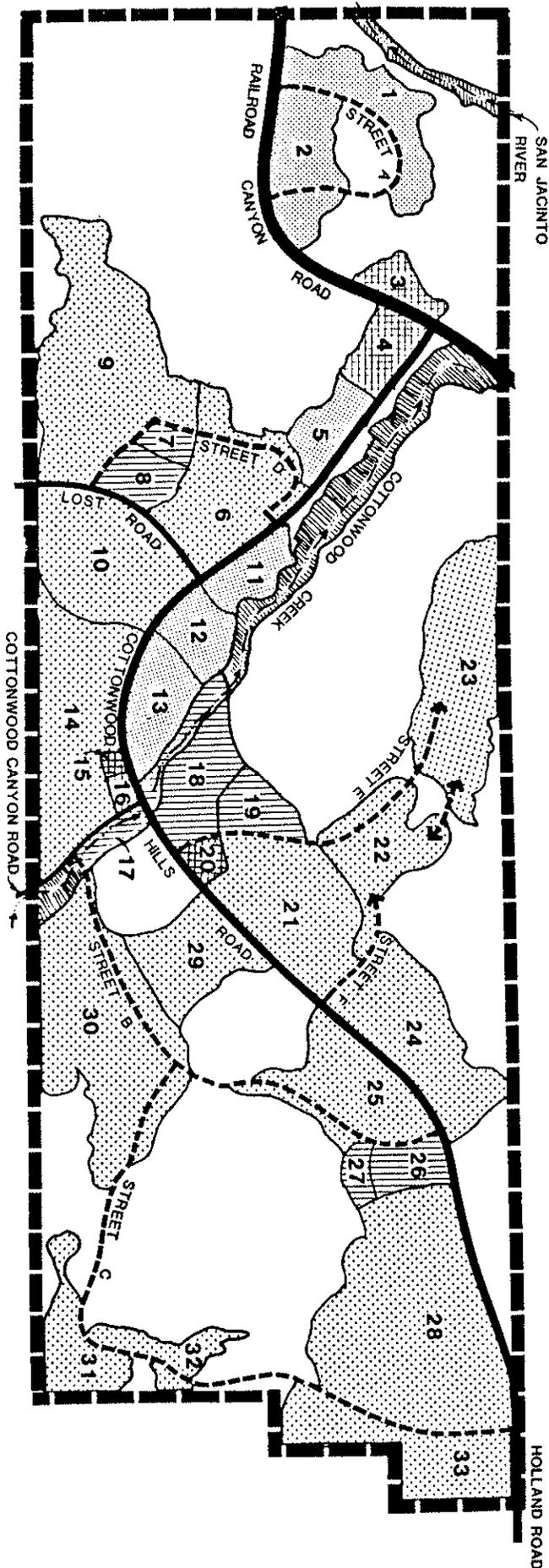
The Cottonwood Hills Specific Plan is organized around a series of planning concepts. These are based on the development factors outlined in Section 1.3, as well as market considerations and public services requirements. The planning concepts are set out below:

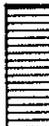
- o Develop a master planned residential community at an overall density of approximately two dwelling units per acre.
- o Provide a range of residential densities to accommodate a variety of housing products targeted for a range of income levels, with emphasis on single-family detached residential.
- o Locate higher density residential and commercial uses in the western portion of the community, in proximity to Railroad Canyon Road and Cottonwood Hills Road.
- o Situate lower density residential uses in the eastern portion of the community, more distant from major transportation facilities and the center of Lake Elsinore.
- o Site lower density residential adjacent to existing rural residential areas, to provide a transition buffer.
- o Designate institutional and commercial sites to serve the community.
- o Locate the three school/park sites to best serve the community in terms of access, project phasing, topography and natural features, and service districts.
- o Preserve a natural open space system made up of sensitive riparian areas, steep slopes, distinctive topographic features, and buffers.
- o Given the topography, accessibility, and neighboring uses of the specific plan area, provide no industrial development sites within the community.



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-  DETACHED RESIDENTIAL
0 UP TO 8 DU/AC
-  ATTACHED RESIDENTIAL
8 UP TO 24 DU/AC
-  COMMERCIAL/INSTITUTIONAL
-  SCHOOLS/PARKS
-  NATURAL OPEN SPACE
-  WATER COURSE
-  MAJOR ROAD
-  MINOR ROAD
-  PLANNING UNIT NUMBER



Land Use Plan 2.1

A total of 4,275 residential units is proposed. This includes 2,628 or 61.5 percent in detached units, and 1,647 or 38.5 percent in attached units. The housing is expected to accommodate an estimated population of 11,415 residents. In total, about 103 acres of commercial, institutional, and public facilities uses support the residential development. No industrial development is proposed.

Figure 2.1 illustrates the land use plan for the specific plan area, while Table 2.1 tabulates the land use areas by parcel number and use type. Approximately 1,018 acres, or 52 percent, of the plan area is developed, devoting 838 acres or 82 percent of the developed acreage to residential uses. About 951 acres, or 48 percent, of the plan area is designated for natural and naturalized open space. All acreages are subject to minor modification during detailed engineering and design, as addressed in Section 9.1.

2.2b Land Use Summary

- o Create a backbone street network of major roads to meet the circulation needs of the community, while transitioning into the existing and proposed street system for the surrounding area.
- o Provide for path systems to accommodate bicyclists and pedestrians.

Table 2.1
LAND USE TABULATION

Land Use Designation	Planning Unit	Gross Acreage	Density DU/Gross AC	Dwelling Units
<u>RESIDENTIAL</u>				
SF1 Single-family detached residential 1 (0 up to 2 DU/gross Ac)	31 32 33	21.9 9.8 <u>37.8</u>	1.2 1.3 1.4	26 13 <u>54</u>
Subtotal		69.5		93
SF2 Single-family detached residential 2 (2 up to 4 DU/gross Ac)	9 10 14 22 23 24 28 29 30	76.4 45.4 58.9 30.0 55.0 46.8 105.3 29.0 <u>81.9</u>	3.6 3.4 3.7 3.4 3.6 3.6 3.6 3.5 3.6	275 156 218 102 198 168 379 102 <u>295</u>
Subtotal		528.7		1,893
SF3 Single-family detached residential 3 (4 up to 8 DU/gross Ac)	6 21 25	46.1 46.5 <u>36.9</u>	4.8 5.0 5.1	221 233 <u>188</u>
Subtotal		129.5		642
MF1 Multi-family attached residential 1 (8 up to 15 DU/gross Ac)	1 11 12	26.1 10.9 <u>13.8</u>	10.5 10.0 12.0	274 109 <u>166</u>
Subtotal		50.8		549
MF2 Multi-family attached residential 2 (15 up to 24 DU/gross Ac)	2 5 13	26.9 12.8 <u>19.9</u>	18.0 16.9 20.0	484 216 <u>398</u>
Subtotal		59.6		1,098
TOTAL RESIDENTIAL		838.1	5.1 Average	4,275
<u>COMMERCIAL/INSTITUTIONAL</u>				
<u>COMMERCIAL</u>				
C2 General Commercial	3	7.8		
C1 Neighborhood Commercial	4	13.4		
C1 Neighborhood Commercial	16	<u>2.0</u>		
		23.2		

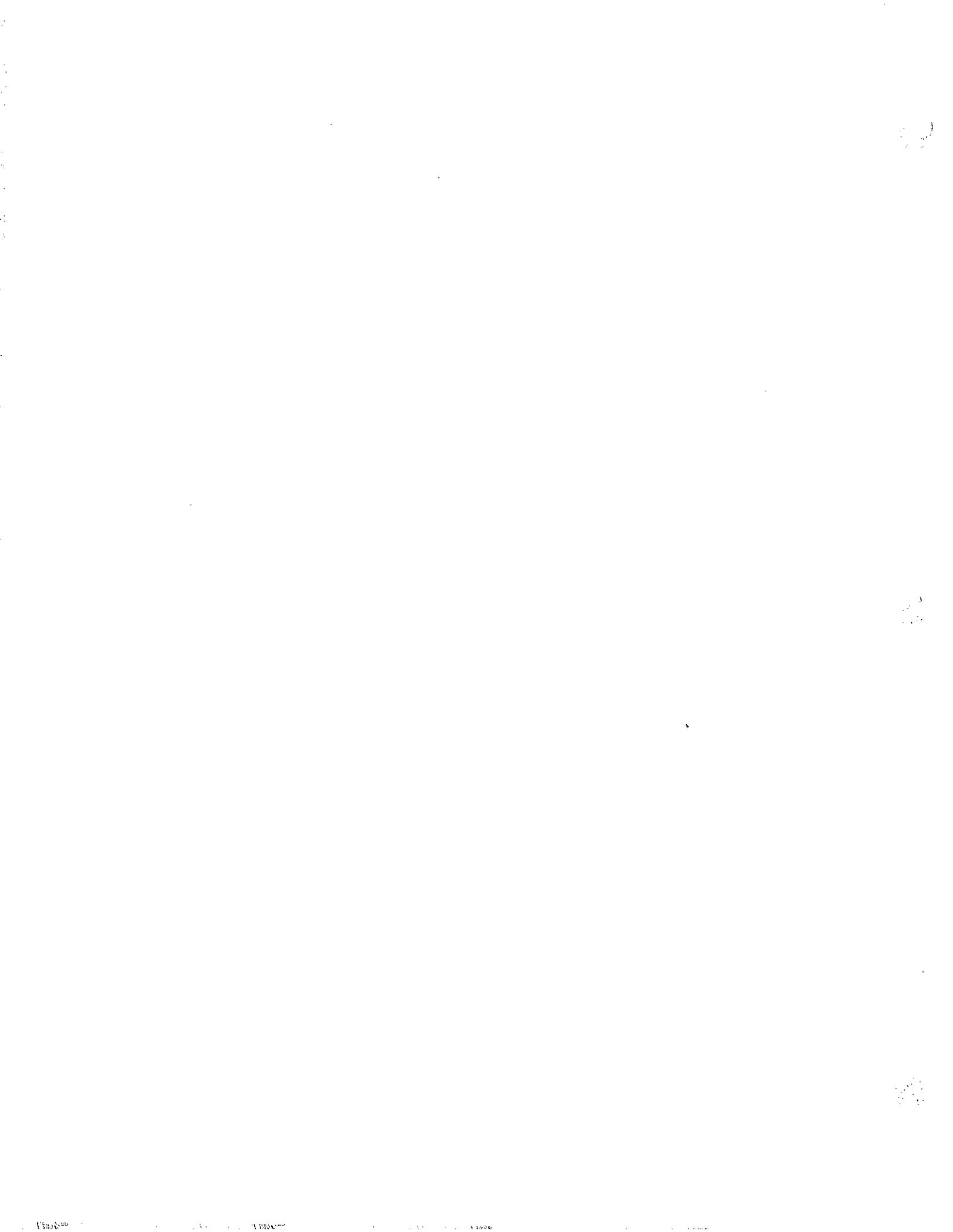


Table 2.1 (continued)
LAND USE TABULATION

Land Use Designation	Planning Unit	Gross Acreage	Density DU/Gross AC	Dwelling Units
IN INSTITUTIONAL	15	1.3		
Subtotal	20	<u>3.9</u>		
		5.2		
TOTAL COMMERCIAL/ INSTITUTIONAL		28.4		
<u>PUBLIC USES</u>				
ES ELEMENTARY SCHOOLS				
West Elementary School	8	10.6		
Central Elementary School	19	11.3		
East Elementary School	26	<u>10.7</u>		
Subtotal		32.6		
PUBLIC PARKS				
NP West Neighborhood Park	7	5.0		
CP Community Park	18	27.7		
NP East Neighborhood Park	27	5.0		
PP Parkway Park	17	<u>4.4</u>		
Subtotal		42.1		
OS OPEN SPACE				
Including Natural Open Space and Naturalized Open Space Outside of Planning Units		950.7		
MAJOR ROADS and Minor Roads Not Included in Planning Units		76.8		
TOTAL PUBLIC USES		1,102.2		
<u>COMMUNITY TOTAL</u>		1,968.7	2.17 Overall	4,275

2.3 Specific Plan Designation

Under Chapter 17.99 SPD Specific Plan District in the City of Lake Elsinore Zoning Code, any Specific Plan Area requires a "specific plan designation" that is specified in the City's General Plan. The associated development intensity policy is designated on the General Plan Land Use Map or in the text of the General Plan. Any project that has a Specific Plan designation in the City's General Plan must be developed under an approved Specific Plan and must have (SP) Specific Plan zoning.

The specific plan area for the Cottonwood Hills Specific Plan has no current land use designation in the General Plan and no zoning in the City of Lake Elsinore. As described in Section 1.2, approval of the Specific Plan will provide Specific Plan (SP) zonings. In addition, the General Plan will be amended to designate the property "specific plan," as described by process (2) in Section 17.99.070 of the Zoning Code. The proposed residential development intensity is an average of 2.17 dwelling units per gross acre. On the General Plan Land Use Map, this would be indicated by the designation "Specific Plan Area (SPA) 2.17 DU/Acre." For the 1,968.7 acres, this designation yields a total of 4,275 dwelling units to be clustered in the developable portions of the specific plan area. Transfer of units within the property is permitted under the conditions specified in Section 3.3a. However, no additional units may be transferred to the Cottonwood Hills plan area from off-site.

An overall density of 2.17 dwelling units per acre is appropriate to the specific plan property based on both site development conditions and on existing and proposed development adjacent to or near the community. Approximately 1,018 acres or 52 percent of the plan area is considered developable. At 4,275 dwelling units, this yields an overall density of 4.2 units per developable acre. This density permits a mix of housing densities to accommodate a range of site constraints and off-site interface conditions, and to respond to market conditions as well. The proposed specific plan density of 2.17 dwelling units per acre is similar to or less than existing or approved nearby large scale projects: Canyon Lake Hills (2 du/acre); Canyon Creek (Summerhill) (3 du/acre); Audie Murphy Ranch (3.6 du/acre); and Canyon Lake (2.4 du/acre including the lake). With its location at the perimeter of the City after annexation and its adjacency to rural residential in the County, a significantly higher density than 2.17 dwelling units per acre is not considered appropriate.

2.4 General Plan Conformance

The Cottonwood Hills Specific Plan has been prepared in conformance with the goals, policies, and objectives of the City of Lake Elsinore General Plan. Appendix C provides a detailed discussion of plan conformance, with references to the Specific Plan as applicable. This includes the Land Use Policies of the 1982 General Plan, as well as the adopted draft policies and objectives for land use of the revised General Plan presently under preparation.

The General Plan 1982 Land Use Map does not encompass the Cottonwood Hills specific plan area. A General Plan Amendment accompanies the Specific Plan to add the planning area to the Land Use Map.

2.5 Conformance with Specific Plan District Ordinance

The following discussion addresses conformance of the proposed Specific Plan with the criteria set out in the Specific Plan District (SPD), Chapter 17.99 of the Zoning Code:

- (1) "The location and design of the proposed development shall be consistent with the goals and policies of the City's General Plan and with any other applicable plan or policies adopted by the City, or in the process of being prepared and adopted."

The Specific Plan is consistent with the goals, policies, and objectives of the City's General Plan, as addressed in Section 2.4 and in detail in Appendix C.

- (2) "The proposed location shall allow the development to be well integrated with or adequately buffered from its surroundings, whichever may be the case."

The specific plan area is surrounded by a mix of existing and proposed land uses, as described in Section 1.1. The proposed Specific Plan demonstrates sensitivity to surrounding areas through the following measures:

- o Selection of similar or compatible land uses for areas with neighboring off-site uses.
 - o Creation of buffers where appropriate, using land forms, landscaping and/or open space, and fencing.
 - o Utilization of development standards to insure proper permitted uses and lot sizes, yards, parking, and so on.
 - o Use of design guidelines to insure attractive development, view enhancement and/or protection, grading, and drainage.
- (3) "All vehicular traffic generated by the development, either in phased increments or at full build-out, is to be accommodated safely and without causing undue congestion upon adjoining streets."

Chapter 7 of the Specific Plan describes the street system necessary to support the proposed residential development. This includes upgrading Railroad Canyon Road as a regional facility, as well as community streets tying into the County street system, and local and residential collector streets solely serving the Cottonwood Hills community. Chapter 9 outlines the proposed phasing program for the specific plan area. This program is designed to insure that adequate street infrastructure is available to support the community as it is built out.

- (4) "The Final Specific Plan shall identify a methodology (s) to allow land uses to be adequately serviced by existing or proposed public facilities and services. In appropriate circumstances, and as provided elsewhere by City Code, the City may require that suitable areas be reserved for schools, parks and pedestrian ways; or public open spaces shall be dedicated or reserved by private covenant for the common use of residents, establishments or operations in the development."

Cottonwood Hills is planned to include on-site elementary schools, parks, and utilities to adequately serve the project. Chapter 5 outlines financing mechanisms for community and off-site public facilities. A phasing program for their timely provision is presented in Chapter 9 as well. Chapter 6 describes open space preservation, including both natural and developed open spaces and their implementation.

- (5) "The overall design of the Specific Plan will produce an attractive, efficient and stable development."

The Specific Plan provides guidelines for the orderly development of the Cottonwood Hills community. Through the development standards prescribed in the plan, appropriate land uses meeting coverage, height, and parking requirements are insured. The plan also includes design guidelines, for the purpose of creating an aesthetically attractive, integrated community. Development standards and design guidelines are provided for each land use type, together with a Site Plan Review process to ensure project conformance with the Specific Plan.

- (6) "In accordance with the requirements of the California Environmental Quality Act (CEQA), impacts have been reduced to a level of non-significance, or in the case where impacts remain, a statement of overriding considerations must be adopted to justify the merits of project implementation."

The Environmental Impact Report accompanying the Specific Plan addresses impacts of the project on the environment, and lists mitigation measures. Mitigation measures are incorporated into the Specific Plan Map and design guidelines where necessary. Appendix B of this Plan contains a table summarizing environmental impacts, mitigation, and Specific Plan references for mitigation measures.

23

24

25

Residential Element

Chapter 3

10

11

12

Chapter 3 - RESIDENTIAL ELEMENT

3.1 Residential Land Use

Residential is the predominant land use for the developable acreage in the Cottonwood Hills specific plan area. Figure 3.1 depicts the proposed distribution of residential development in the community. Each residential planning unit is designated for development at a certain density (dwelling units per gross residential acre), placing it within a housing category. Table 2.1 in Chapter 2 lists each residential planning unit together with the gross residential acreage, density, and anticipated dwelling units. A total of 4,275 dwelling units is proposed on 838.1 gross residential acres, for a community average of 5.1 units per residential acre. The overall average community-wide is 2.17 dwelling units per gross acre, consistent with an SPA 2.17 designation.

3.1a Housing Mix

Five housing categories based on density and product types are utilized in the specific plan. These include SF1, SF2, SF3, MF1, and MF2 densities as described in Table 3.1. A density range is specified for each category, for example, 2 up to (but not including) 4 DU/gross AC.

The SF1, SF2, and SF3 categories constitute the single-family detached units, while the MF1 and MF2 classifications make up the multi-family attached units. Consistent with the goal of emphasizing single-family detached residential, about 61.5 percent, or 2,628 of the units are detached, utilizing 87 percent, or 727.7 acres, of the residential developable area. The remaining 1,647 units, or 38.5 percent are attached, on a total of 110.4 gross residential acres.

A total population of approximately 11,415 residents is estimated for the community at build-out. This represents an average of 2.67 persons per dwelling unit, the average household size for the City of Lake Elsinore.

Single-Family Residential 1 (SF1) provides detached estate lot development on one-half acre lots. This type of development is appropriate in steeper areas where standard subdivisions cannot be well accommodated by conventional grading techniques and where accessibility is limited; and at sensitive interfaces with off-site rural residential areas. Lots may be entirely or partially padded out, depending on location. Planning units with this density designation may be constructed conventionally by a builder or built as custom homes.

Single-Family Residential 2 (SF2) is specified for detached residential planning units with the City's conventional (R-1) lot size of 7,260 square feet. Residential at this density is suitable for developable areas adjacent to off-site residential and rural residential areas; and in locations somewhat removed from major community facilities and transportation. This type of development can be readily terraced to suit moderately steep topographic conditions and to take advantage of views.

Single-Family Residential 3 (SF3) is designed to provide affordable detached residential, filling the niche between SF 2 and MF 1 development. The proposed "small lots" may take the form of deep lots (the conventional configuration) or wide-shallow lots (an innovative configuration emphasizing an attractive streetscape). SF3 residential should be located near school/park complexes and public open space to augment open space opportunities. This type of development is best suited to relatively flat areas, or to moderate slopes where terracing may be utilized.

Multi-Family Residential 1 (MF1) can accommodate a variety of low density attached housing types, including duplexes, patio homes, townhouses, and two-story flats. This type of development is appropriate in relatively flat areas next to major community roadways; and adjacent to riparian open space corridors and public natural open space. Planning units with this designation will feature project recreational facilities and usually condominium ownership.

Multi-Family Residential 2 (MF2) is designated for medium density attached projects. Housing at this density will feature two- to three-story walk-up units with surface parking, occupied as rental or condominium units. Development of this type should be accessible from major community roadways, and is best located in relatively flat areas near community facilities such as commercial centers and parklands. MF2 planning units will contain on-site recreational facilities and common open space areas.

3.1b Housing Location

The distribution of residential areas in the plan and the designation of housing categories evolved from the following considerations:

- o The general location of the specific plan area at the perimeter of the City's sphere of influence, as a suburban community.
- o Primary access to the plan area from the west end, off the Railroad Canyon Road/Cottonwood Hills Road intersection.
- o Interfaces with existing and proposed land uses surrounding the community, as outlined in Chapter 1.

Table 3.1

HOUSING TABULATION

Housing Category	Density Range	Possible Products	Gross Acreage	Number of DU	Percent of Total DU
SF1	0 up to 2 DU/gross Ac	1/2-acre estate lot detached	69.5	93	2.2%
SF2	2 up to 4 DU/gross Ac	7,260 sq. ft. lot detached	528.7	1,893	44.3%
SF3	4 up to 8 DU/gross Ac	small lot detached, deep or wide lot	129.5	642	15.0%
TOTAL SINGLE-FAMILY DETACHED			727.7	2,628	61.5%
MF1	8 up to 15 DU/gross Ac	Attached - duplexes, townhouses, patio homes, flats	50.8	549	12.8%
MF2	15 up to 24 DU/gross AC	Attached - walk-up apartments & condominiums	59.6	1,098	25.7%
TOTAL MULTI-FAMILY ATTACHED			110.4	1,647	38.5%
COMMUNITY TOTAL			838.1	4,275	100.0%

- o The elongated shape of the plan area, together with the primary circulation route in the east-west direction -- Cottonwood Hills Road.
- o The constraints and opportunities of the existing topography, natural features, and rock outcrops, as presented in Chapter 1, plus the grading requirements for different types of residential development.
- o The constraints and opportunities of the watercourses and accompanying vegetation on the property to residential development.
- o Relationships of residential development to the circulation system in terms of access, visual quality, traffic noise, and development phasing.

Based on these considerations, the proposed pattern of residential development is characterized by the following concepts:

- o Placement of the attached housing in the western half of the plan area, resulting in a detached/attached residential mix with an overall density of 7.5 dwelling units per gross residential acre.
- o Designation of the eastern half of the plan area for detached housing only, resulting in an overall density of 3.5 dwelling units per gross residential acre.
- o Siting of SF1 estate residential adjacent to the rural residential County areas east of the community, and SF1 estate and SF2 detached residential next to the rural residential and estate residential areas at the northern and southern boundaries.
- o When consistent with the preceding concepts, utilization of relative flat areas where possible for attached housing and steeper areas for detached housing; and employment of terracing to maximize views and minimize grading.
- o Insurance of the potential for a mix of up to four housing categories (SF2, SF3, MF1, and MF2) in the early phases of community development off Cottonwood Hills Road.

3.1c Balanced Community

Cottonwood Hills is planned as a balanced community, in that housing is proposed for a range of lifestyles at a variety of household income levels.

Section 3.1a describes the type of residential development associated with each housing category. Because of location, topography, density, and product type, each planning unit will have its own character and represent a particular market and lifestyle appeal. A spectrum of housing prices will be provided, ranging from upper-end for SF1 units to moderate income for MF2. Most units accommodate middle and upper-middle income residents, provided in the SF2, SF3, and MF1 projects. The emphasis on middle and upper-income residents is consistent with Policy 5 of the Draft General Plan Land Use Policies. However, the MF2 projects are expected to accommodate moderate income households, contributing to the City's available housing for this level. Residential development within the community will conform with State requirements for handicapped accessibility.

An effective affirmative marketing plan is proposed to be utilized in conjunction with all residential projects to ensure affirmative marketing of sale and rental units. The objective of the program should be to establish a racially balanced community through advertising and other methods, intended to inform minority and majority households that Cottonwood Hills housing is available on an equal opportunity basis.

3.2 Residential Design and Development

The objectives below provide the basis for the residential design guidelines and development standards:

- o Maximize view opportunities.
- o Create project identity while contributing to the overall character of the Cottonwood Hills community.
- o Provide attractive, yet functional, circulation and parking.
- o Buffer housing from noise and traffic.
- o For attached projects, provide common areas such as recreational facilities and shared open space.
- o Provide pedestrian access to nearby amenities.

3.2a Residential Development Standards

Chapter 8 provides development standards for the residential zoning districts associated with each housing category. These standards include permitted uses, uses subject to a conditional use permit, and accessory uses. In addition, standards are specified for lot size and/or area; street frontages, setbacks, and building separations; lot coverage; building height; dwelling unit size; and open space. Parking is subject to Chapter 17.66 of the Zoning Ordinance, while Chapter 17.14, Residential Development Standards, is applicable to all residential projects. Section 8.9 prescribes signage standards for residential planning units.

3.2b Residential Design Guidelines

Tables 3.2 and 3.3 outline guidelines for the design of detached and attached residential planning units, respectively. These guidelines supplement and in a few cases modify the Residential Development Standards in Chapter 17.14 of the Zoning Ordinance.

The design guidelines should be employed in both the engineering and design and City review of residential planning units. City review will occur at both the Site Plan Review and Design Review stages, as described in Chapter 9.

For a discussion of the mitigation of traffic noise impacts on residential projects, see Section 7.2d. Energy conservation and water conservation measures are addressed in Sections 6.6 and 6.9, respectively. The fire management program described in Section 6.2d contains provisions for water utilities, access, and building materials and design which are applicable to all residential projects.

A standard security package shall be provided in all dwelling units. This includes dead bolts for exterior doors, locking devices for all non-fixed windows and garage doors, and smoke detectors. Lighting fixtures shall be installed at all front entries to both detached and attached units.

Table 3.2

DETACHED RESIDENTIAL DESIGN GUIDELINES

Project (Planning Unit) Identity and Appearance

- o Give each planning unit a distinct identity through such measures as common design treatments, delineation of project boundaries, a looped circulation system, and project entries. The following planning units may be designed as separate projects or combined into single projects:

Planning Units 10 and 14
Planning Units 22 and 23
Planning Units 29 and 30
Planning Units 31, 32, and 33

- o Employ a selected landscape palette throughout each project, particularly at project entries, on slopes visible to the public, and in common or public areas along streets.
- o Coordinate the design of buildings, fencing, signage, and street hardscape to create an overall project atmosphere or style, while permitting a variety of floor plans and individuality in dwelling unit exteriors and yards. Utilize similar scale, colors, materials, and architectural style throughout each project. A single color, roof material, style, or other design characteristic is acceptable if there is adequate variation in other design features.

Project (Planning Unit) Streetscape

- o Vary the appearance of residences lining streets by utilizing curvilinear streets; front yard setback variations; front building elevation variations; alternating unit types; reverse plans and driveways; or a combination of these measures. A typical local streetscape is depicted in Figure 3.2.
- o Utilize a curvilinear residential street pattern to create visual interest, as illustrated in Figure 7.13.
- o To prevent a row-house effect, site no more than two adjacent residences with the same front yard setback, and utilize a minimum setback variation of three feet. This requirement is not applicable to fan cul de sacs, where variation is introduced by the cul de sac configuration.
- o Provide landscaped strips between street curbs and sidewalks along residential lots, as shown in Figures 7.10 and 7.12; and install street canopy trees to provide summer shade. For fan cul de sacs, ensure adequate planting areas are provided for trees between unit driveways.
- o Employ a selected landscape palette throughout each project, to give continuity and visual unity to the planning unit. Repeat a theme canopy tree(s) throughout each project in a formal spaced pattern.
- o Cluster mailboxes to reduce visual clutter where approved by the postal service.

Table 3.2 (continued)

DETACHED RESIDENTIAL DESIGN GUIDELINES

View Opportunities

- o Maximize residential view opportunities through site planning that is adaptive to topographic conditions and natural open space.
- o Utilize terracing where appropriate to the natural grade to create view opportunities, as illustrated in Figure 6.7.
- o Maintain significant on-site topographic features as attractive open space vistas or backdrops from residential units, as described in Chapter 6.
- o Where feasible, stagger lots in flatter areas to maximize views.
- o Utilize open fencing such as tubular steel to permit views where noise, safety, and privacy considerations permit.
- o During initial construction, carefully select and site landscape materials so as to frame and preserve views at their maturity, and to not block sight lines.

Off-site Interfaces

- o Consider land uses outside the specific plan area when designing projects at the community boundary. For the planning units listed below, refer to the designated typical section:

Planning Unit 9	Typical Section c
Planning Unit 10	Typical Section c
Planning Unit 14	Typical Sections c, d
Planning Unit 23	Typical Section a
Planning Unit 24	Similar to Typical Section c
Planning Unit 30	Typical Section d, similar to Typical Section a
Planning Unit 31	Similar to Typical Sections b, c
Planning Unit 33	Typical Section b

- o Create buffer zones made up of building setbacks (yards); expanded setbacks; slope differentials; screen trees or plantings; uniform fencing treatments; or preferably a combination of these measures.
- o For Planning Units 31 and 33, provide a buffer along the specific plan area boundary a minimum of fifty (50) feet in width, including side or rear yard setbacks as appropriate.
- o Consider views from existing off-site residential areas into the Cottonwood Hills community in designing the appropriate buffer treatment for a residential project.
- o For slopes running downhill from Cottonwood Hills lots into off-site areas, such as in Planning Units 23, 31 and 33, utilize an open space easement to preserve the open space condition and designate open space maintenance responsibility to ensure an attractive appearance.

Table 3.2 (continued)

DETACHED RESIDENTIAL DESIGN GUIDELINES

On-Site Interfaces

- o Separate detached residential uses from other denser residential projects and non-residential uses through use buffers.
- o See the appropriate design guidelines for non-residential use buffers.
- o For buffers between residential areas, utilize two or more of the following measures:
 - building setbacks (yards)
 - slope differentials
 - screen trees and plantings
 - uniform fencing treatments
 - building height variations
 - permanent natural open space and fire lanes

These measures should augment the specific plan layout, which often provides road or open space separations between density categories; and zoning standards, which prescribe minimum setbacks between planning units and height limitations for attached projects next to detached projects. The front yards of two residential housing types should not face each other across a shared public street.

Open Space

- o Where possible, buffer residential areas from highly trafficked major roads using open space such as slope banks or berms within the expanded street parkway.
- o Provide "touch-downs" of natural and/or naturalized open space next to residential streets to give visual interest and provide fire vehicle access.
- o For yards abutting steep slope areas of natural open space, see Section 6.2c.
- o For residential dwelling units adjacent to natural open space, apply the standards of the fire management zone outlined in Section 6.2d.

Circulation and Parking

- o Adapt street alignments to topographic conditions and the character of each project site. Generally, residential streets should be curvilinear with gentle transitions between tangents and radii, as shown in Figure 7.13. Vertical alignments should be carefully coordinated with horizontal curves to permit safe and continuous movements.
- o Give detailed attention to the design of intersecting streets, stopping points, sight distances, curved alignments, and view overlooks, not only to meet City standards and policies, but also to maximize visual appeal.

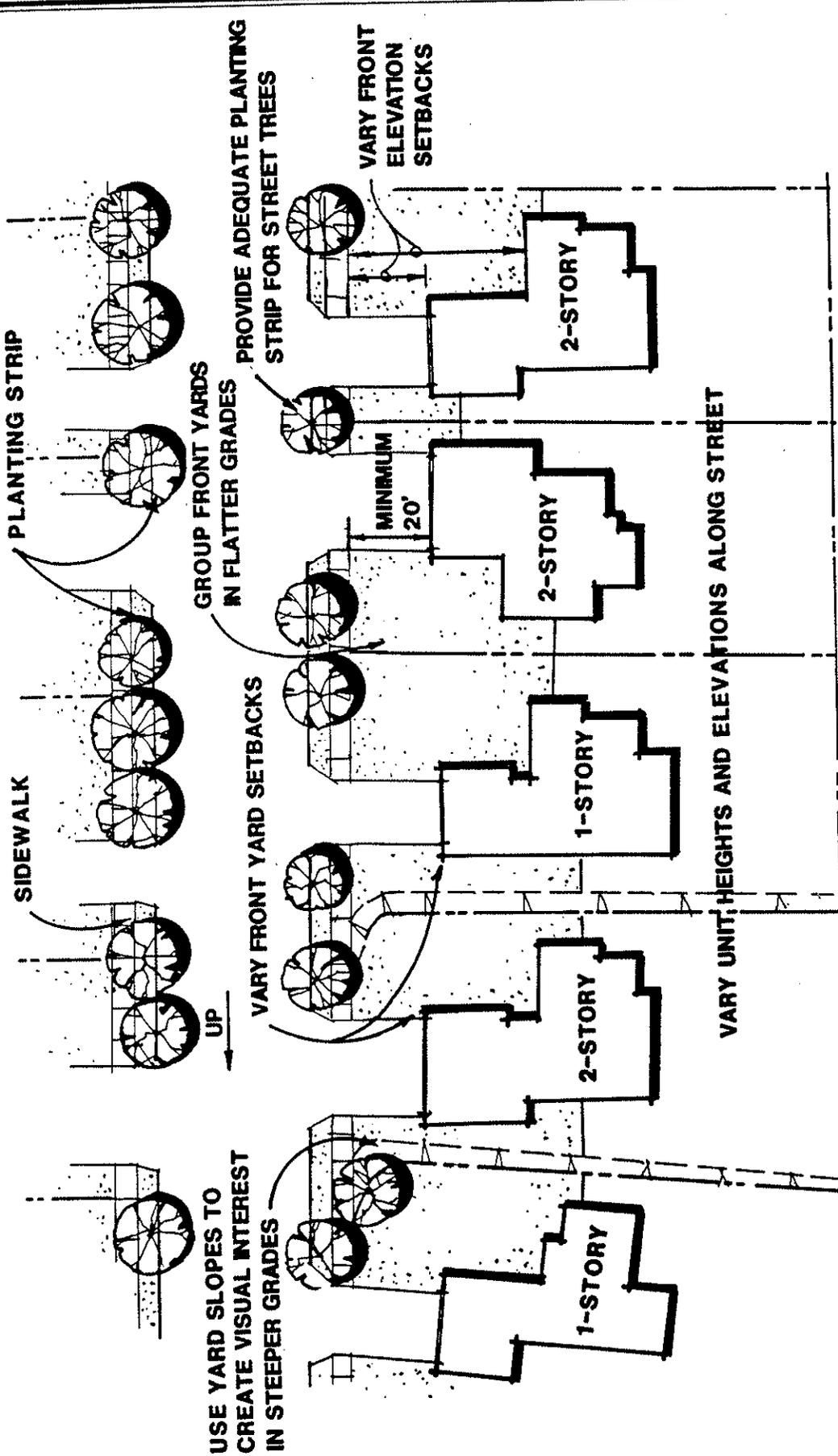
Table 3.2 (continued)

DETACHED RESIDENTIAL DESIGN GUIDELINES

- o Limit access to individual residential lots from local or residential collector streets, not from major community streets.
- o Give special attention to the siting of houses on lots and the placement of driveways to permit adequate distances between driveways to accommodate on-street parking and landscaped strips.
- o See Section 7.1 for street design guidelines.

Fencing

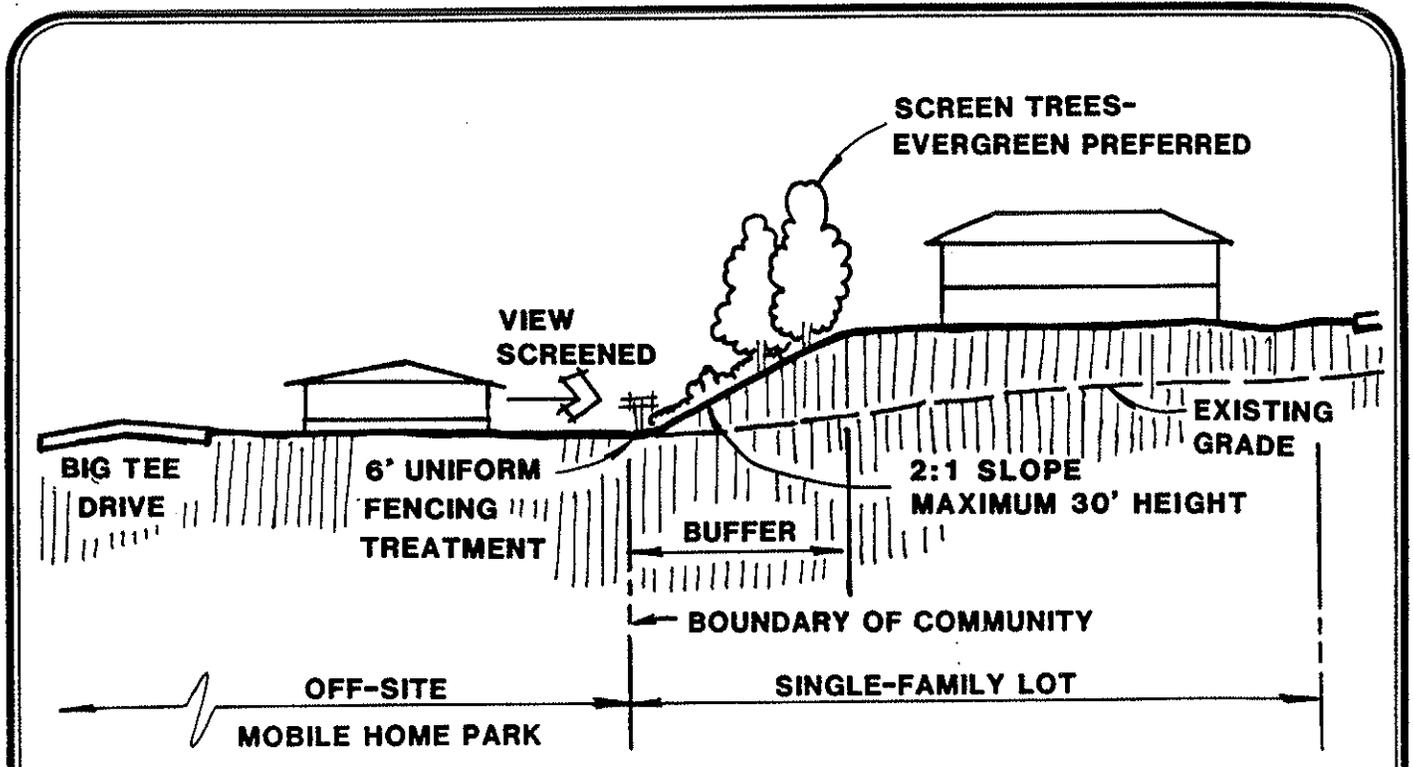
- o At project perimeters fronting major community roads, employ the community-wide fencing treatments to separate planning units from parkways or expanded parkways. Utilize a solid wall treatment such as masonry where noise and privacy control are required, and an open fencing treatment such as tubular steel to capture view opportunities.
- o At project perimeters next permanent natural open space, use non-flammable fencing as part of the fire management zone, per Section 6.2d.
- o Within detached residential projects, utilize solid noise walls at side yards abutting public streets, except where views may merit open fencing. Interior fencing along lot lines may be wood.
- o Covered chain link fencing is permitted in open fencing conditions within the SF1 Zone only.
- o Where school district boundaries run along residential collector streets, employ fencing along with lot orientation and subdivision layout to separate planning units into separate "territories."



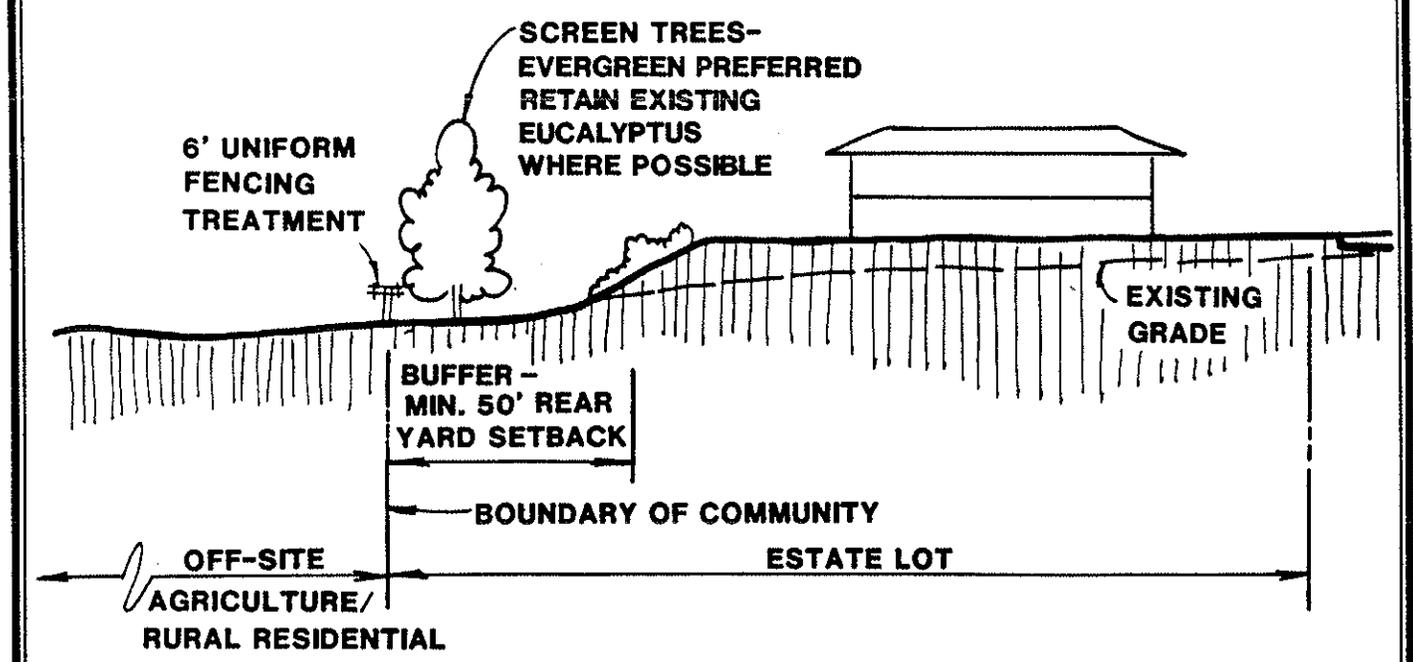
Cottonwood Hills

SINGLE-FAMILY RESIDENTIAL ZONES

Typical Local Streetscape 3.2



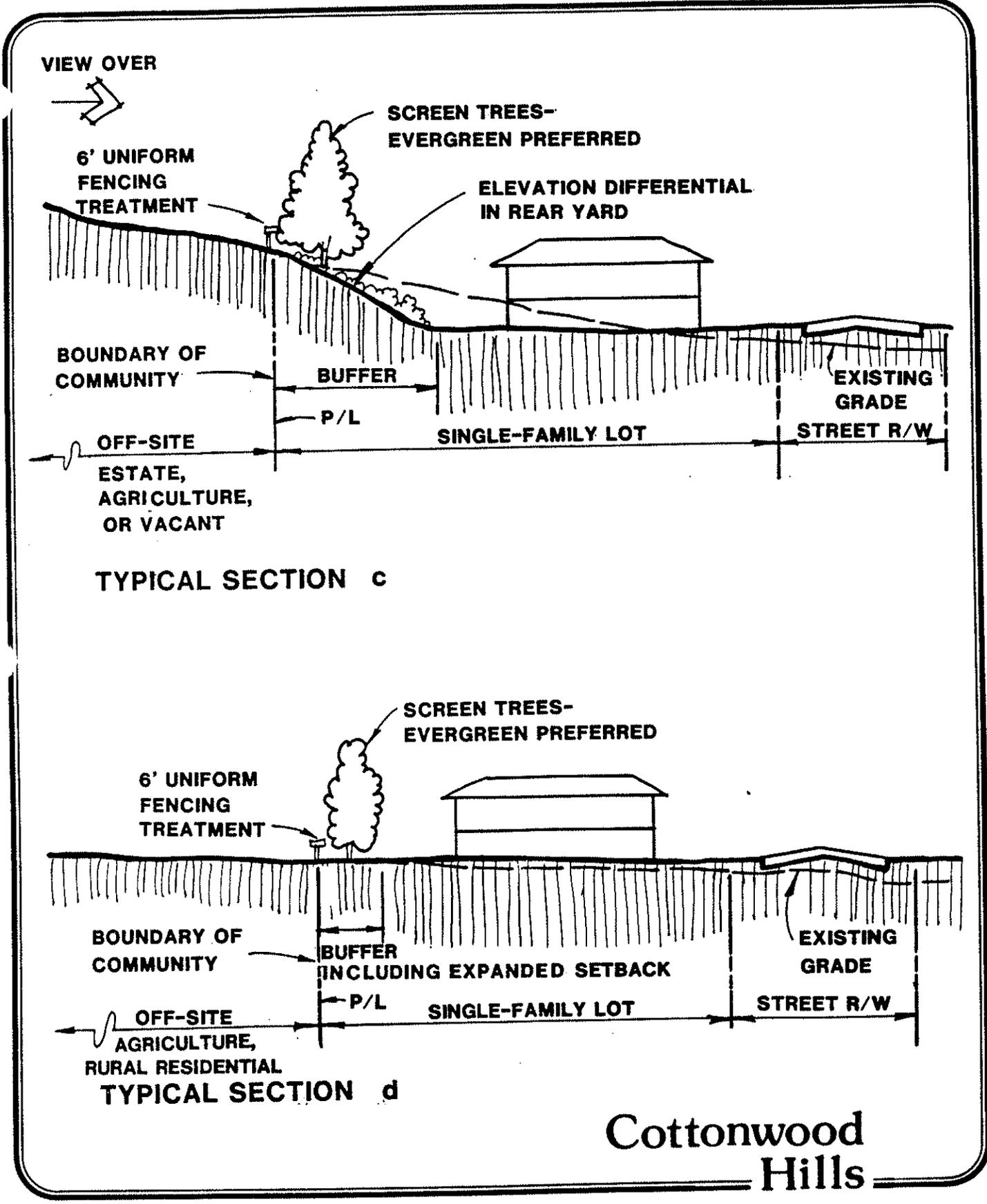
TYPICAL SECTION a.



TYPICAL SECTION b

Cottonwood Hills

Off-Site Interface 3.3



Off-Site Interface 3.4

Table 3.3

ATTACHED RESIDENTIAL DESIGN GUIDELINES

Project (Planning Unit) Identity and Appearance

- o Give each planning unit an identity through common design elements or treatments, delineation of project boundaries, distinctive entries, and shared recreational areas or other focal points. The scale, colors, materials, design details, and architectural style of buildings, signage, mailbox areas, and furnishings should be similar for the entire project.
- o Utilize a selected landscape palette throughout each project. The purpose is to give continuity and unity to the project while ensuring compatibility with the overall community.
- o Create distinct project sub-areas within larger attached housing projects by varying architectural and landscape elements (e.g., roof style or color, building elevations, use of materials, material colors) in order to create clusters with identity, achieve some variety, and avert visual monotony. This is applicable to Planning Units 1, 2, and 13.
- o Along project streets, avoid visual monotony by utilizing curvilinear streets; building setback variations; building elevation variations; variety in building massing; a varied planting palette and/or pattern; or a combination of these measures.
- o Coordinate project entry monuments or walls, and other project signage, to produce an attractive appearance.
- o For Planning Units 11, 12, and 13, carry the riparian planting theme from Cottonwood Creek across Cottonwood Hills Road, utilizing a riparian plant palette and an informal cluster pattern.

View Opportunities

- o In projects or project sub-areas with view opportunities, use site planning to maximize views for as great a number of units as possible. Possible design measures include terraced building pads, split level products, building staggering, unit clustering, and view corridors. Landscaping should frame or enhance views.
- o In projects or project sub-areas with limited or no view opportunities, emphasize creating attractive internal views within the project. Areas with external view opportunities should be considered for common areas, to enhance the entire project. Landscaping should be employed to generate internal vistas and visual excitement.

Buffer Conditions

- o Separate attached residential projects from other residential and non-residential uses through use buffers, employing two or more of the following measures:
 - building setbacks (yards)
 - slope differentials

Table 3.3 (continued)

ATTACHED RESIDENTIAL DESIGN GUIDELINES

- screen trees and plantings
- uniform fencing treatments
- building height variations
- permanent natural open space

These measures should augment the specific plan layout, which often provides road or open space separations between land uses; and zoning standards, which provide minimum setbacks between planning units or land uses, and height limitations for attached projects next to detached projects.

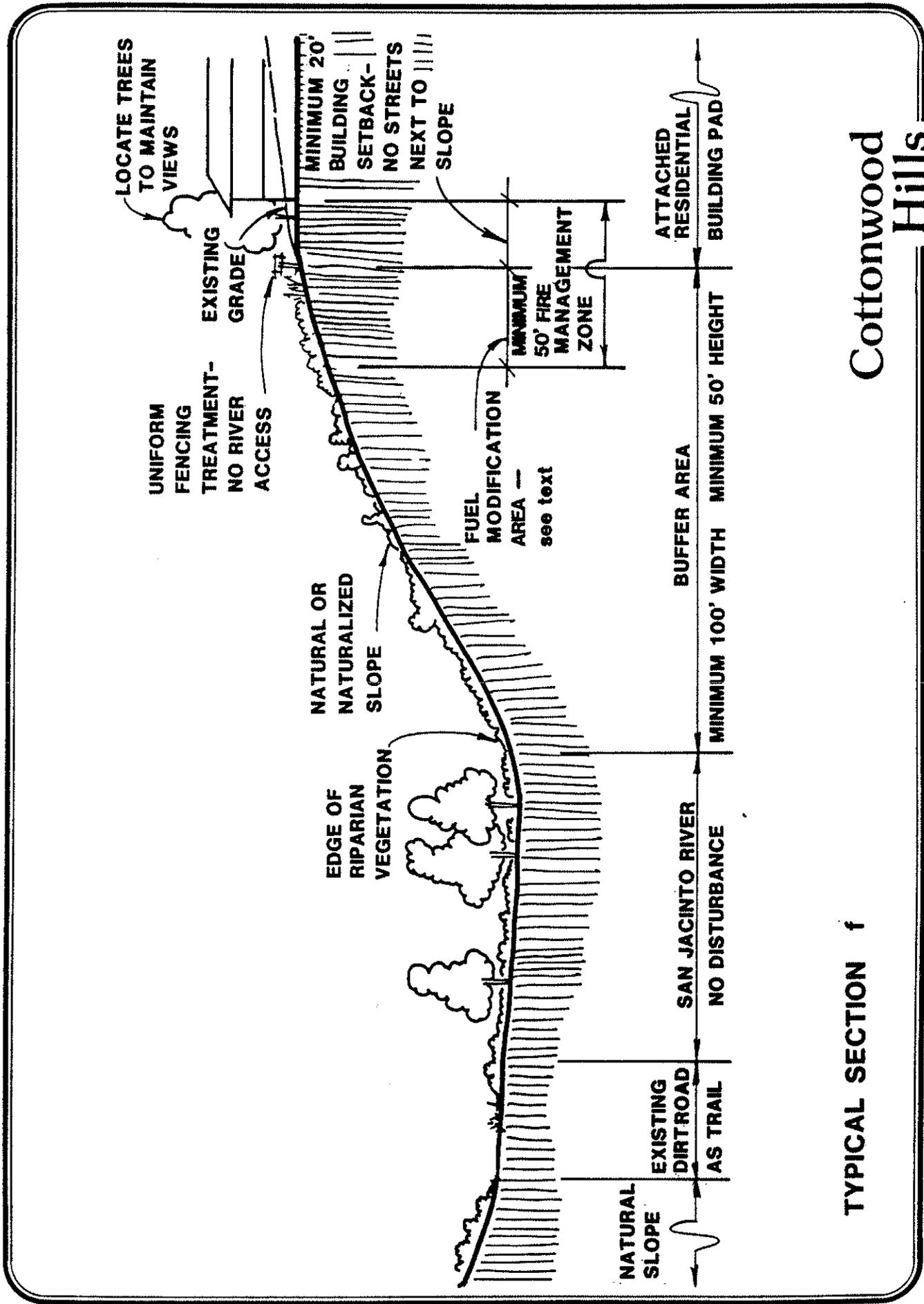
- o For projects next to major community roads, design buffers to provide visual and noise privacy. Refer to the Grading and Fencing design guidelines for streetscapes in Table 7.1c, and Noise Mitigation in Section 7.2d.

Riparian Open Space Buffers

- o For attached projects adjacent to riparian areas, provide a buffer to protect the resource from undue encroachment.
- o As shown in Typical Section "f" (Figure 3.5), protect the San Jacinto River habitat in the design of Planning Unit 1 through the use of a substantial buffer area; access control through fencing; building and road setbacks; and daylight grading to the extent possible.
- o For Planning Units 11, 12, and 13 illustrated in Typical Section "g" (Figure 3.6), protect the Cottonwood Creek riparian area via building setbacks; uniform fencing; a buffer area including a trail corridor where appropriate; and blended landscaping. The buffer area should extend from the property line of the planning unit (generally at the fence) to the edge of the riparian area, defined by the dripline created by the majority of the trees (isolated trees are not included).
- o See Figure 6.5 for a depiction of the desired grading along wetlands.
- o See Section 6.2b, Riparian Habitat Areas; and Section 6.2d, Fire Management.

Open Space

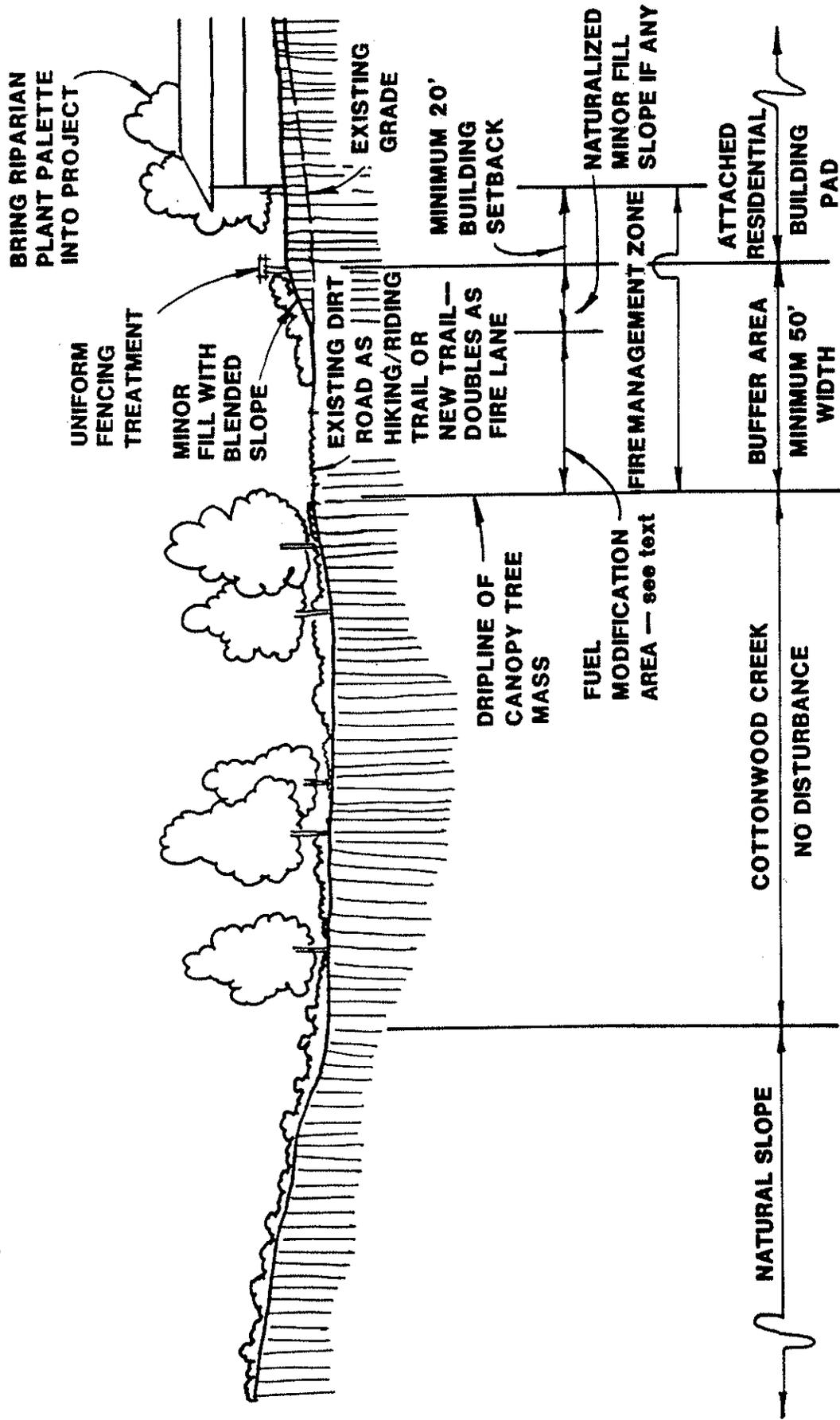
- o See also "Buffer Conditions" and "Riparian Open Space Buffers" above.
- o For yards abutting steep slope areas of natural open space, see Figure 6.2 and Section 6.2c.
- o For fire management zones adjacent to natural open space, see Section 6.2d.
- o Provide each residential unit with private outdoor open space in the form of private yards, patios, decks, or balconies, with good views if feasible.



TYPICAL SECTION f

Cottonwood Hills

River Interface 3.5



TYPICAL SECTION 9

Cottonwood Hills

Creek Interface 3.6

Table 3.3 (continued)

ATTACHED RESIDENTIAL DESIGN GUIDELINES

- o Provide each attached residential project with usable, common open space in the form of recreational facilities, walking and seating areas, and/or children's play areas. See Section 5.2e concerning Project Recreational Facilities.

Circulation and Parking

- o Design project roads to be curvilinear in nature, in order to slow traffic and to provide visual interest. There should be adequate provision for bicycle and pedestrian circulation within projects and linkages to the neighborhood bicycle and pedestrian path networks.
- o Make parking bays small in size with screening where possible. Parking areas adjacent to another residential project should be screened by a fencing and landscaping, and lighting should minimize light spillover.
- o Place identifiable project entries at entrances into the planning unit from major community roads and residential collector streets.
- o Provide adequate parking per Chapter 17.66 of the Zoning Code. Shade parking with landscaping, through building and wall placement, and/or provision of covered parking or garages.
- o Utilize a theme canopy tree(s) for parking areas and roadways throughout each project, to provide visual continuity and shade.

Crime Preventive Design

- o Utilize the principle of "defensible space" by designing "territories" with which people associate themselves and neighbors. Examples include clustering front entries of dwellings around common open space areas; facing windows and private open spaces onto common recreational facilities; and utilizing cul de sac or loop street patterns.
- o Incorporate crime prevention as a factor in the design of projects, such as limiting the height of landscaping and fencing to maintain visibility of common areas from streets and other trafficked areas; providing elevation differentials between public and private spaces; and locating parking and common use facilities near destinations.
- o Provide night lighting for all parking lots, covered parking (carports), walkways, project entries, and dwelling unit front entries.

Fencing

- o At project perimeters fronting major community roads, employ the community-wide solid wall treatment to provide noise and privacy control.
- o At project perimeters next to permanent natural open space, use non-flammable fencing as part of the fire management zone, per Section 6.2d.
- o Interior fencing between dwelling units may be wood.
- o Covered or uncovered chain link fencing is not permitted at project perimeters.

3.3 Residential Implementation

All residential projects will be implemented by private builders with the approval of the City of Lake Elsinore. Section 9.1 outlines the implementation process from the Specific Plan to certificate of occupancy. A residential phasing program is presented in Section 9.3a.

3.3a Unit Transfer

The gross and net acreages of individual planning units are subject to modification during detailed engineering and design. Consequently, the dwelling unit yields provided in the Specific Plan may be subject to some corresponding modification at the time subdivision maps and site plans are submitted.

Accordingly, dwelling units may be transferred between planning units under this Plan. However, the following criteria must be met in order to effect a transfer:

- o The unit transfer will not result in an increase in housing density category or the upzoning of any planning unit receiving transferred units.
- o The basic intent of the Specific Plan in regard to development standards and design guidelines must be adhered to.
- o Under no circumstances shall the total dwelling unit count of 4,275 be exceeded for the overall specific plan area, except in redesignations of land uses as specified in Section 5.1 for school sites and Section 4.2 for institutional sites.

Approval of the transfer of units from one planning unit to another shall be based on the following schedule:

<u>Number of Dwelling Units to be Transferred</u>	<u>Approving Entity</u>
1 through 20 units	Community Development Director
21 through 50 units	Planning Commission
51 units and above	City Council

The maximum acreage change due to dwelling unit transfer shall conform to the schedule below:

SF 1	.70 acres per DU transferred
SF 2	.27 acres per DU transferred
SF 3	.20 acres per DU transferred
MF 1	.08 acres per DU transferred
MF 2	.05 acres per DU transferred

The residential monitoring program shall be utilized to maintain an accurate count of dwelling units and their location within the Specific Plan area. This program is described in Section 9.3e.

3.3b Residential Review

Residential planning units should be designed and reviewed in accordance with (1) the designated residential zoning district, as specified in Chapter 8; (2) appropriate residential, signage, and parking development standards as specified in Section 3.2a; and (3) the residential design guidelines provided in Section 3.2b above. The Site Plan Review process detailed in Section 9.2 should be utilized to ensure development is in conformance with the Specific Plan.

**Commercial/
Institutional Element**
Chapter 4



Chapter 4 - COMMERCIAL/INSTITUTIONAL ELEMENT

4.1 Commercial Land Use

This section addresses commercial land uses in the Cottonwood Hills community.

4.1a Commercial Sites

A total of 23.2 acres of commercial development is proposed within the specific plan area. This commercial is intended to meet the convenience commercial requirements of community residents, as well as supplementing existing and proposed commercial development in Canyon Lake and Audie Murphy Ranch. Other commercial needs are expected to be served by specialty commercial in the downtown area of Lake Elsinore; general and convenience commercial at the Interstate 15/Railroad Canyon Road interchange; and a proposed regional shopping mall west of that interchange.

Figure 4.1 depicts the locations of the three proposed commercial sites, and Table 4.1 outlines appropriate uses for each location. Most of the commercial acreage is situated at the Railroad Canyon Road/Cottonwood Hills Road intersection. As the primary entrance into the community, this location best serves most community residents while being easily accessible to motorists on Railroad Canyon Road. A small neighborhood commercial site is also provided central to the community to meet convenience commercial needs.

4.1b Commercial Development Standards

As shown in Table 4.1, a zoning district shall apply to each commercial planning unit. Chapter 8 provides development standards for the C1 (Neighborhood Commercial) and C2 (General Commercial) districts based on the City's C-1 and C-2 zoning districts. These standards include permitted uses and uses subject to a conditional use permit; lot area, street frontage width, setback, and building height requirements; and landscape improvements. Signage standards are prescribed in Section 8.9, Community Signage. Parking is subject to Chapter 17.66 of the Zoning Ordinance. Chapter 17.38, Non-Residential Development Standards, is applicable to all commercial projects.

Table 4.1

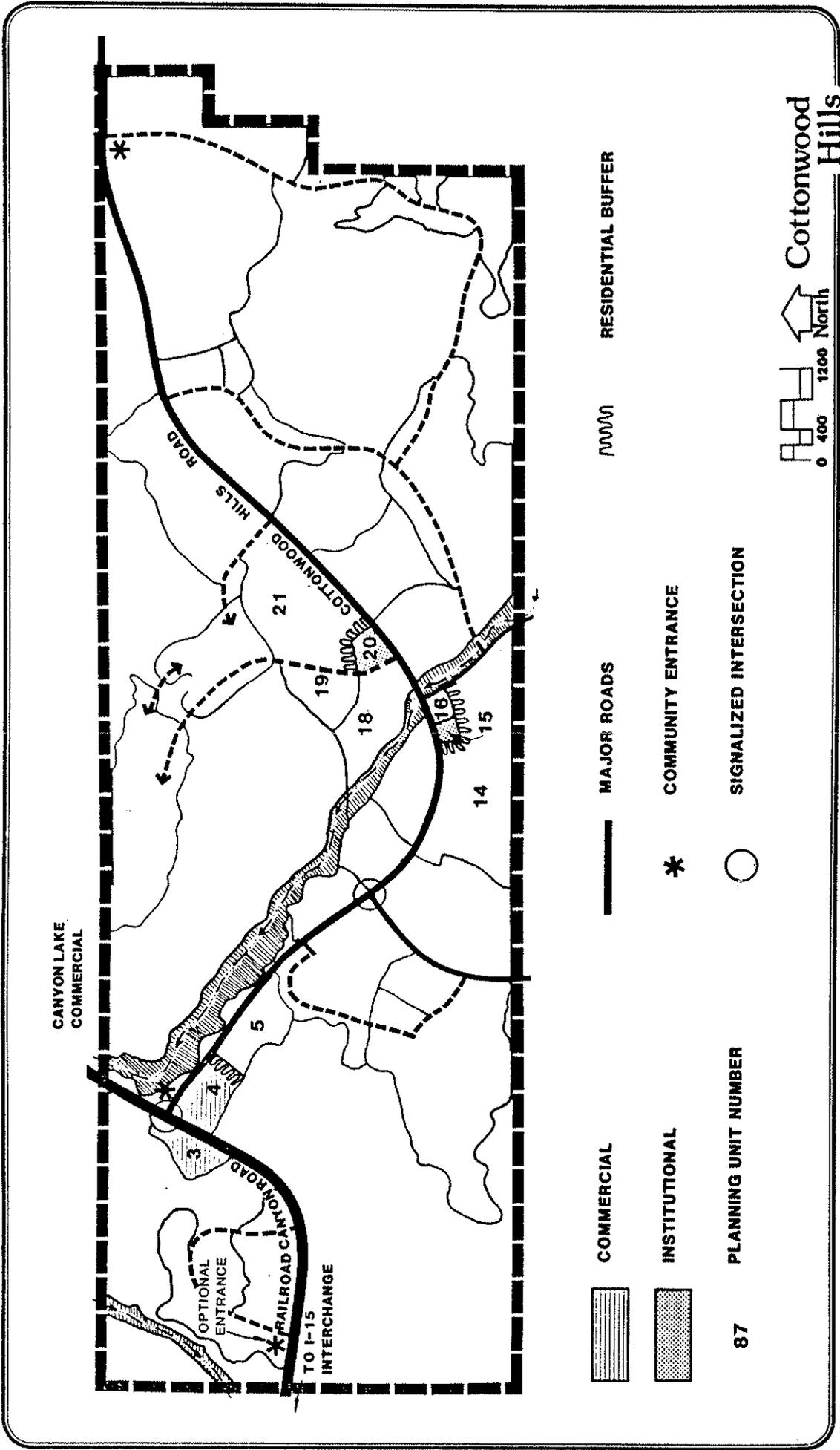
COMMERCIAL LAND USE

Planning Unit	Location	Gross Acreage	Appropriate Uses	Development Standards
3	Railroad Canyon Road, west side of intersection with Cottonwood Hills Road.	7.8 ac.	<ul style="list-style-type: none"> o Commercial services -- personal, professional, repair, financial. o Major retail, such as home improvement center. o Specialized retail, such as bakery, boutique. o Restaurants. o Commercial recreation, such as bowling alley, theater. o Convenience services -- auto service station, drive-thru establishments. 	<p>C2 Zone (General Commercial District).</p> <p>Conditional use Permit as required by Zone.</p>
4	Railroad Canyon Road and Cottonwood Hills Road, south-east of intersection.	13.4 ac.	<ul style="list-style-type: none"> o Neighborhood commercial, such as supermarket, drug store. o Specialized retail, such as video store, apparel store. o Commercial services -- personal, professional, repairs, financial. o Restaurants. o Convenience services -- auto service station, drive-thru establishments. 	<p>C1 Zone (Neighborhood Commercial District).</p> <p>Conditional Use Permit as required by Zone.</p>

Table 4.1 (continued)

COMMERCIAL LAND USE

Planning Unit	Location	Gross Acreage	Appropriate Uses	Development Standards
16	Cottonwood Hills Road and Cottonwood Canyon Road, southwest corner.	2.0 ac.	<ul style="list-style-type: none"> o Convenience shopping, such as mini mart. o Convenience services, such as auto service station. o Commercial services -- personal, professional, repair, financial. 	<p>C1 Zone (Neighborhood Commercial District)</p> <p>Conditional Use Permit as required by Zone.</p>



P Project Design Consultants

Commercial/Institutional Land Use 4.1

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4.1c Commercial Design Guidelines

The objectives below provide the basis for the design concept and guidelines for commercial projects:

- o Design commercial development to contribute to the overall visual character of the community, especially as viewed from the major road system.
- o Provide adequate vehicular and pedestrian access to commercial projects.
- o Minimize any possible conflicts with nearby residential uses.
- o For Planning Units 3 and 4, design the commercial projects to complement each other as well as the community entrance treatment.

The overall commercial concept is to design each project as a functionally and aesthetically integrated building complex. Typical strip commercial development is inappropriate, as is a series of stand alone buildings in various architectural styles surrounded by parking. Instead, each commercial complex should function and appear as a coordinated unit.

Table 4.2 outlines guidelines for the design of commercial development projects. These guidelines should be employed in the design and review of commercial planning units. The guidelines supplement the development standards in the City's Zoning Ordinance.

Table 4.2

COMMERCIAL DESIGN GUIDELINES

Functional Considerations

- o Where development abuts a major road, keep the pads as close to the street elevations as possible, to facilitate vehicular access, project visibility, and drainage.
- o Consider crime preventive design and ease of surveillance in site planning and access design. Exemplary measures include visual corridors into the project from major roadways; landscaping to maintain views of pedestrian areas from drive aisles; visibility of parking and pedestrian areas from onlookers inside of commercial establishments; and sufficient night lighting of pedestrian and parking areas.
- o Use shading devices extensively in parking and pedestrian areas, such as canopy trees, arcades, decorative awnings, and porticos.

Appearance

- o Design all commercial buildings and accompanying signage to be architecturally compatible within the project.
- o Coordinate the design of projects and associated expanded parkways with the parkways and medians of adjacent roadways including landscaping, project entries, street furniture, and fencing.
- o Utilize buildings and landscaping to screen major manufactured slopes from view.

Use Buffers

- o Provide a buffer between commercial development and other land uses, to act as an access control, visual screen, and noise barrier.
- o For commercial developments next to residential projects, utilize fencing and slope banks as well as the City - required landscaped strip to create a use buffer.
- o Per the City's development standards, minimize undesirable impacts on nearby residential areas by screening delivery, storage, and trash areas; limiting spillover lighting; carefully siting driveway entrances; and screening roof top equipment.
- o For Planning Unit 16, consider the impacts of any proposed design on the institutional site to the west (Planning Unit 15).

Signage

- o Establish a uniform signage program for each commercial center to ensure adequate functionality and aesthetic quality. See Section 8.9p.
- o Coordinate permanent commercial center signage along street frontages with merchant identification signage on buildings, to create a cohesive signage program.
- o Consider proximity to nearby residential uses in locating and designing signage for Planning Units 4 and 16.

Table 4.2 (continued)

COMMERCIAL DESIGN GUIDELINES

Access and Parking

- o Where possible, provide project access from each public roadway bounding the commercial site, including both left and right turn access into the planning unit where feasible.
- o Coordinate project access with adjacent intersection design and also the median cuts in abutting arterial and major highways as addressed in Section 7.1.
- o Lay out car and pedestrian flow patterns carefully within the site, minimizing auto/pedestrian conflicts if possible and insuring adequate fire and delivery vehicle access.
- o Provide adequate on-site parking per Chapter 17.66 of the Zoning Code. Parking for hikers is not permitted in the commercial centers, as described in Section 7.6.
- o Soften the visual impact of parking areas on- and off-site by using planted islands; screening with landscaping, berms, and walls; breaking up parking into sub-lots or into areas associated with particular uses; utilizing textured paving and walkways; or similar design measures.
- o Utilize a theme canopy tree(s) for parking areas and drives throughout each commercial center, to provide visual continuity and shade.
- o Accommodate non-motorized transportation by providing on-site bicycle parking, and sidewalks and seating areas.
- o Coordinate commercial project planning with transit stop accommodation, per Section 7.4.

Open Space Interfaces

- o Provide a minimum 50-foot wide fire management zone at the interface of commercial development with natural open space. This width may include parking areas; service roads acting as a fire lane; manufactured slopes and benches with irrigated landscaping; fuel modification areas; or a combination of these or similar measures. See Sections 6.2c and 6.2d.
- o Landscape all manufactured slopes abutting permanent natural open space areas with naturalized plantings having a sufficiently low fuel volume and high moisture content to act as a firebreak, while producing a natural appearance.
- o For the high cut slope along the southern edge of Planning Unit 4, utilize "contoured landscaping" to augment grading techniques and building placement to produce an attractive appearance.

Table 4.2 (continued)

COMMERCIAL DESIGN GUIDELINES

Planning Units 3 and 4

- o Make the commercial projects for Units 3 and 4 compatible in architecture, landscaping, and signage.
- o Where possible, emphasize views of buildings and screen vistas of parking areas by motorists traveling Railroad Canyon Road. This can be accomplished through building siting and massing; pad elevations and gradients; and slopes, berms, walls, and landscaping.
- o Carefully design commercial signage in relation to Railroad Canyon Road, so as to minimize visual clutter and to coordinate with the community entrance treatment at the Railroad Canyon Road/Cottonwood Hills Road intersection, while recognizing the requirement for sufficient project and merchant identity through visible signage.

4.2 Institutional Land Use

This section describes institutional land uses in the Cottonwood Hills community.

4.2a Institutional Sites

Within the specific plan area, a total of 5.2 acres is set aside for development of institutional sites.

As pictured in Figure 4.1, Planning Unit 15 is located near the intersection of Cottonwood Hills Road and Cottonwood Canyon Road. The 1.3 acre site receives primary access from Cottonwood Hills road via a local street at a median break. Secondary access may be provided directly from Cottonwood Hills Road on a right-in/right-out basis. If within three years of occupancy of the adjacent neighborhood commercial site (Planning Unit 16) the property is not purchased for institutional use, it may be redesignated for neighborhood commercial (C1) use under the C1 Zone.

Planning Unit 20 is situated near the community park/school complex on Cottonwood Hills Road, central to the community. This 3.9 acre site may be developed as one or two institutional parcels. The property is accessible from Street "E", and alternately from Cottonwood Hills Road. The site is visible to travelers along Cottonwood Hills Road, the major east-west route through the community. If within three years of completion of community park (Planning Unit 18) the property is not purchased for institutional use, it may be redesignated for Single-Family Detached (SF3) residential use at 4 up to 8 dwelling units per acre.

Appropriate uses for the institutional site(s) include the following:

- o Public facilities.
- o Private or commercial day-care, nursery, and elementary schools.
- o Private non-commercial clubs, lodges, and fraternal organizations excepting fraternities and sororities.
- o Churches and religious institutions, including attached schools.
- o Non-profit institutions whose primary purpose is the promotion of the public health and welfare.

4.2b Institutional Development Standards

Institutional projects shall conform to the C1 (Neighborhood Commercial) zone outlined in Section 8.6. Development standards are provided for lotting, setbacks, building height, and landscape improvements. Signage standards are prescribed in Section 8.9, Community Signage. Parking and non-residential development standards are subject to Chapters 17.66 and 17.38, respectively, of the City of Lake Elsinore's Zoning Ordinance.

4.2c Institutional Design Guidelines

Institutional project design should be based on the following objectives:

- o Design institutional development to contribute to the overall visual quality of the community, especially as viewed from the major road system.
- o Provide adequate vehicular and pedestrian access to institutional projects.
- o Consider nearby residential areas in designing institutional uses.
- o Design institutional projects to complement nearby non-residential uses.

The most important concept in designing the institutional projects is creation of an integrated complex, site-wide. In the case of possible multiple uses on one site, such as two public facilities or institutions, coordination is required in access, building design, and landscape treatment to achieve a unified appearance. For religious institutions, the relationship of the principal building or sanctuary to related buildings and facilities is a consideration. Integration of various recreational and outdoor facilities with buildings is a factor in school and private club design. Whatever the institutional use(s), a series of unrelated buildings and outdoor facilities does not represent appropriate design. This means a master plan for the site is important, since institutional facilities are often built in phases. For non-public facilities, a master plan shall be required as part of the Conditional Use Permit process.

Institutional design guidelines are outlined in Table 4.3.

Table 4.3

INSTITUTIONAL DESIGN GUIDELINES

Appearance

- o Design all institutional buildings and accessory uses (including signage) to be architecturally compatible within the project.
- o Coordinate the design of the project(s) and associated expanded parkways and/or slopes adjacent to the street with the parkway and median of Cottonwood Hills Road, including landscaping, project entry and signage, street furniture, and fencing.

Access and Parking

- o For Planning Unit 15, provide vehicular and bicycle access directly and/or indirectly from Cottonwood Hills Road.
- o For Planning Unit 20, provide vehicular and bicycle access from the residential collector Street "E" adjacent to the project and/or from Cottonwood Hills Road. Coordinate project access with the design of the adjacent Street "E"/Cottonwood Hills Road intersection.
- o Lay out car and pedestrian flow patterns carefully within the site, minimizing auto/pedestrian conflicts if possible and ensuring adequate emergency access.
- o Provide adequate on-site parking per Chapter 17.66 of the Zoning Code.
- o Soften the visual impact of parking areas on- and off-site by using planted islands; screening with landscaping, berms, and walls; breaking up parking into small lots; utilizing textured paving and walkways; or similar design measures.
- o Use shading devices extensively in parking and pedestrian areas, such as canopy trees, arcades, decorative awnings, and porticos.
- o Utilize a theme canopy tree(s) for parking areas and drives throughout the planning unit, to provide visual continuity and shade.

Site Planning

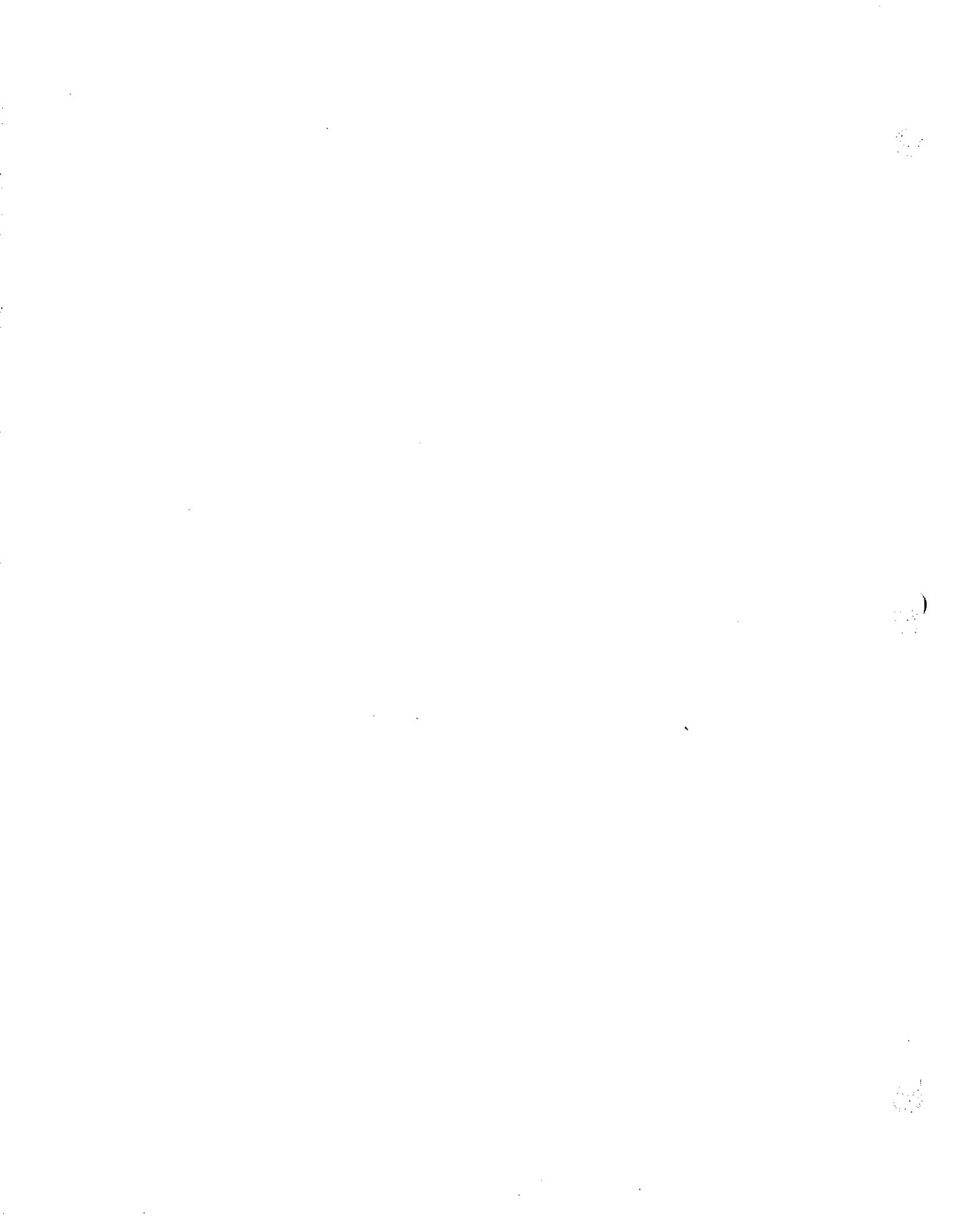
- o Consider crime preventive design and ease of surveillance in site planning and access design.
- o Be sensitive to nearby residential areas in the location and design of driveways; buildings; delivery, storage, and trash areas; and lighting. For Planning Unit 15, this applies to residential Planning Units 13 and 14; for Planning Unit 20, this applies to residential Planning Unit 21.
- o Provide a buffer between institutional projects and other land uses, to act as a visual screen, access control, and noise barrier.
- o For institutional projects next to residential projects, utilize fencing and slope banks as well as the City-required landscaped strip to create a use buffer.

4.3 Commercial/Institutional Implementation

All commercial and institutional development will be implemented through a prescribed development process. Section 9.1 outlines the implementation process from the Specific Plan to certificate of occupancy. A development phasing program is presented in Section 9.3a.

Commercial and institutional planning units should be designed and reviewed in accordance with (1) the designated commercial zoning district, as specified in Chapter 8; (2) the Community Signage standards prescribed in Section 8.9; and (3) the commercial and institutional design guidelines provided in Section 4.1 and 4.2 above. The Site Plan Review process detailed in Section 9.2 should be utilized to ensure proposed development is consistent with the Specific Plan. A conditional use permit (CUP) is required for certain uses, such as churches, private clubs, and commercial day care.

**Public Facilities
And Services Element**
Chapter 5



Chapter 5 - PUBLIC FACILITIES AND SERVICES ELEMENT

5.1 Public Schools

This section addresses provision of public schools for the Cottonwood Hills community.

5.1a School Districts

The specific plan area is currently split between two sets of school districts, as shown in Figure 5.1.

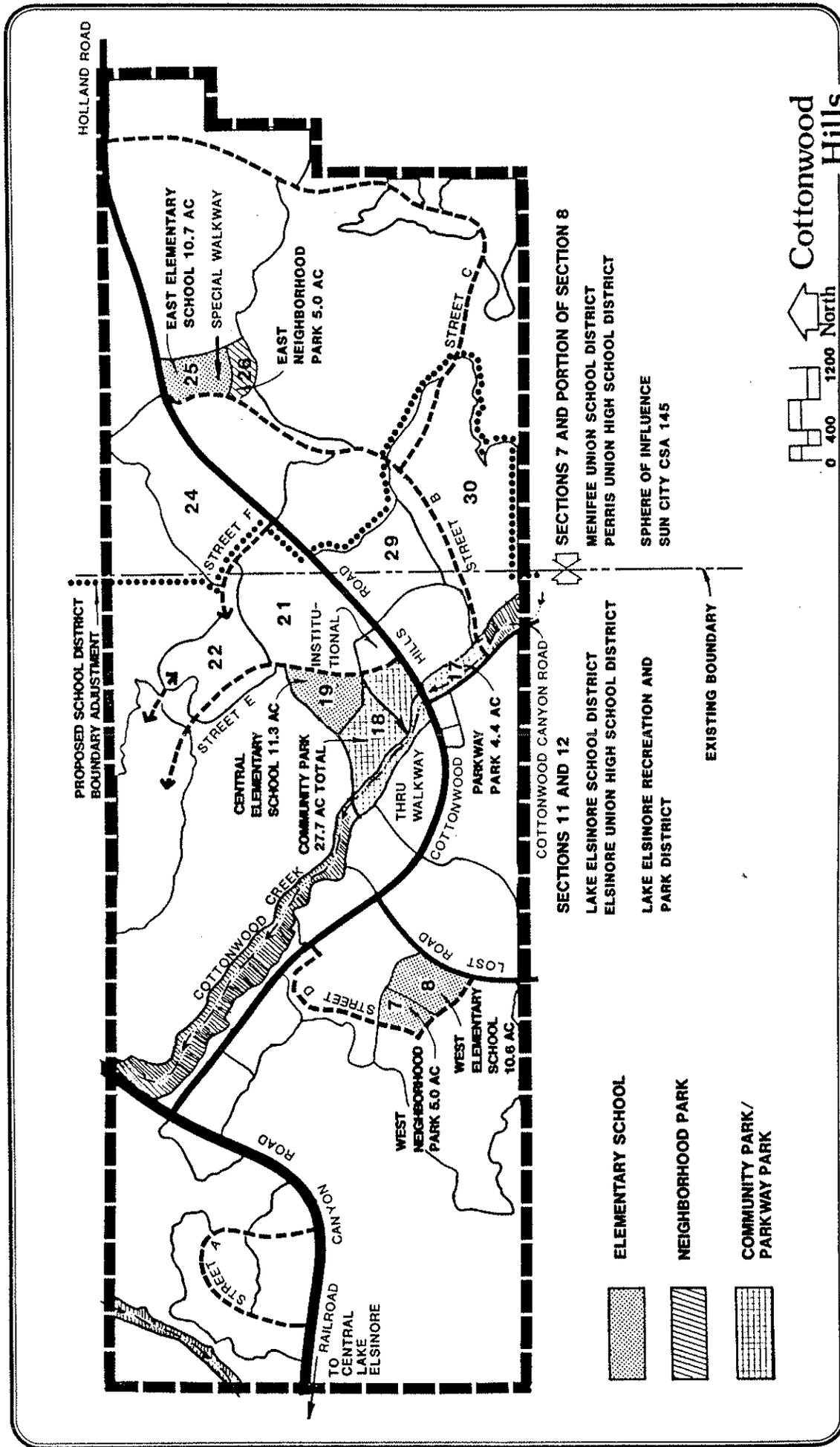
The western two-thirds of the community is situated within the Lake Elsinore School District (elementary) and the Elsinore Union High School District. The elementary school district currently accommodates grades K through six for the City of Lake Elsinore and adjacent areas. The high school district serves grades seven through twelve, and presently encompasses the Murrieta, Temecula/Rancho California, and Lake Elsinore areas. A reorganization and unionization of the elementary and high school districts into the Lake Elsinore Unified School District has been approved, effective July, 1989.

The eastern third of the specific plan area is located within the Menifee Union School District and the Perris Union High School District. Currently the Menifee district serves grades K through seven, with plans underway to add grade eight, and possibly the high school grades in the future. The high school district provides for grades nine through twelve for the specific plan area. It presently serves a total of five feeder elementary school districts.

A minor district boundary adjustment is proposed for the specific plan area, as shown in Figure 5.1. The proposed boundary is run along logical road and/or subdivision boundaries, resulting in reasonable service areas for the proposed schools in the community.

5.1b School Facilities

In Table 5.1, the number of students anticipated for each school district is estimated. This is based on current student generation rates, and the number of dwelling units planned in each district with the proposed boundary change. Totals of 1,718 elementary students (K-6) and 856 high school students (7-12) are forecasted.



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3-14-89

Public Facilities Plan 5.1

Table 5.1

STUDENT GENERATION

School District	Grades	Dwelling Units	Generation Rate Students/DU	Total Students
LAKE ELSINORE SCHOOL DISTRICT (1)	K - 6	3,447	.40	1,379
MENIFEE UNION SCHOOL DISTRICT	K - 6(2)	828	.41	339
TOTAL ELEMENTARY	K - 6	4,275	-	1,718
ELLSINORE UNION HIGH SCHOOL DISTRICT (1)	7 - 12	3,447	.205	707
PERRIS UNION HIGH SCHOOL DISTRICT	7 - 8(2)	828	.08	66
Subtotal	<u>9 - 12</u> 7 - 12	<u>828</u> 828	<u>.10</u> .18	<u>83</u> 149
TOTAL HIGH SCHOOL	7 - 12	4,275	-	856

- (1) Unification of the Lake Elsinore School District and the Elsinore Union High School District is effective in July, 1989.
- (2) Grades 7 - 8 to be phased into Menifee Union from Perris Union by 1989, at a generation rate of .128 students per dwelling unit.

To accommodate the elementary students, three elementary school sites totalling 32.6 gross acres are designated in the specific plan area. These sites are distributed over the community to maximize opportunities for walking to school. The West and Central Elementary Schools are situated in the Lake Elsinore School District, while the East Elementary School lies in the Menifee Union School District. Each school site is large enough to provide approximately 10.0 net acres for school development. This 10-acre size meets both districts' standards for school sites to accommodate a 500-to 700-student elementary school building, necessary bus and parking areas, and outdoor play facilities. In addition, this meets the recommended site size of 10 acres for 650 students of the California State Department of Education in its draft School Site Selection and Approval Guide.

The West Elementary School (Planning Unit 8) is located next to Lost Road, with access received from Street "D" off Lost Road. The West Neighborhood Park is sited next to the school, to facilitate joint usage of recreational and parking facilities. In the event the site is not required for school purposes, it should be designated as 5.0 acres for Neighborhood Park usage; and the remainder as SF3 (Single-Family Detached) residential, at 4 up to 8 dwelling units per acre.

The Central Elementary School (Planning Unit 19) is situated along the Street "E" collector. To allow potential sharing of recreational and parking facilities, the Community Park is sited immediately adjacent to the school site. If the property is not needed for an elementary school, it should be designated as 5.0 acres for Community Park usage; and the remainder as SF3 (Single-Family Detached) residential, at 4 up to 8 dwelling units per acre.

The East Elementary School (Planning Unit 26) is placed along Cottonwood Hills Road, with access provided by the Street "B" collector. The East Neighborhood Park is located immediately to the south, to permit sharing of recreational and parking facilities. If the site is not required for school purposes, it should be designated as 5.0 acres for Neighborhood Park usage; and the remainder as SF2 (Single-Family Detached) residential, at 2 up to 4 dwelling units per acre.

The high school students living in Cottonwood Hills will attend school facilities off-site. For the Elsinore Union High School District, the probable schools include the Terra Cotta Junior High School or a new junior high school east of Interstate 15; and the Temescal Canyon High School. In the eastern portion of the plan area, students are likely to attend the new middle school on Garboni Road east of Murrietta Road; and a future high school in either the Perris Union High School District or the Menifee Union School District, depending on the jurisdiction at that time.

5.1c. School Design Guidelines

Design guidelines for the three on-site elementary schools are provided in Table 5.2.

Table 5.2

ELEMENTARY SCHOOL DESIGN GUIDELINES

School Building

- o Design school building(s) to the requirements of the school district, taking into account permanent and portable facilities.
- o Coordinate architectural style and features with nearby residential areas.
- o Coordinate school building design with the community park recreation building to be compatible functionally and aesthetically (Central Elementary School).

Recreation Facilities

- o Coordinate the selection and design of facilities with the adjacent public park.
- o Possible facilities:
 - Turf playground
 - Paved playground
 - Play apparatus for tots
 - Play apparatus for children
 - Outdoor seating/classroom area

Access and Parking

- o Auto access from adjacent residential collector street or local linkage street, off of major community road.
- o Possible shared drives with adjacent public park.
- o Adequate parking per Chapter 17.66 of the Zoning Ordinance, Parking Requirements. Theme canopy shade tree(s) for parking areas and drives.
- o Possible joint usage or overflow usage of park's off-street parking per Section 7.3.
- o Adequate bicycle parking and access.
- o Handicapped parking and accessibility.
- o School entry monumentation and signage coordinated with community-wide signage program.

Interface with Adjacent Residential

- o Provide a buffer between the school and adjacent detached residential planning units, utilizing slope differentials; screen trees and plantings; uniform fencing treatments; or a combination of these measures.
- o Design night lighting to minimize spillover into adjacent residential areas through proper location and fixture selection.

Table 5.2 (continued)

ELEMENTARY SCHOOL DESIGN GUIDELINES

Crime Preventive Design

- o To facilitate surveillance, grade the site to be as visible as possible from adjacent public roadways, school drives, and parking areas.
- o Incorporate crime prevention as a factor in the design of school buildings, lighting, landscaping, and parking areas.

5.2 Public Parks

This section concerns the provision of population-based public parks. Chapter 6 describes the community open space system, which includes natural areas to be preserved for passive recreational use.

5.2a Parks Requirements

The City of Lake Elsinore General Plan addresses parks requirements in the Outdoor Recreation chapter of the Environmental Resources Management Element. As a new community, the Cottonwood Hills Specific Plan represents an opportunity to insure adequate recreational facilities are provided on-site for community residents. These facilities are proposed to include the following:

- o Public parks.
- o Public school play facilities.
- o Project recreational facilities for MF1 and MF2 (Multi-Family Attached) residential planning units.

Current City standards call for 2.5 acres of neighborhood park and 2.5 acres of community (district) park per 1,000 population, for a total of 5.0 acres of parkland per 1,000 persons. Based on an estimated community population of approximately 11,415 residents, 57.1 acres of parkland is needed. Under the Specific Plan, 10.0 gross acres of neighborhood parks, 27.7 gross acres of community park, and 4.4 acres of parkway park are proposed, totalling 42.1 acres. In addition, there are 15.0 acres of adjacent school recreational facilities, about 5.0 acres on each of three elementary school sites or 50% of net school acreage. The parks are sized to meet City "size range" and "population served" standards, and distributed over the community to maximize accessibility to residents. Also, the school/park sites are laid out to facilitate joint design and usage.

As shown in Figure 6.1, the western portion of the specific plan area is currently located within the Lake Elsinore Recreation and Park District service area, while the eastern part is situated within the Sphere of Influence of the Sun City CSA 145. However, as part of the annexation into the City of Lake Elsinore, the City is expected to assume responsibility for the parklands and recreational services within Cottonwood Hills.

5.2b Neighborhood Parks

Two neighborhood parks are proposed to serve the specific plan area, one in the western portion and one in the east. Design guidelines for the parks are provided in Table 5.3.

The West Neighborhood Park (Planning Unit 7) is situated next to the West Elementary School, and receives access from Street "D" and possibly from the West Elementary School. The site is 5.0 acres in size and provides a large flat pad for recreational facilities. There are attractive views from the site to the north and possibly the east.

The East Neighborhood Park (Planning Unit 27) is placed adjacent to the East Elementary School, and is accessible from the Street "B" residential collector. The 5.0-acre site can include a large flat pad for playfields plus naturalized slopes to transition to the natural open space to the south. The park and school recreational facilities can be interconnected. The park is directly accessible to the SF3 (Single-Family Detached) residential project in Planning Unit 25. Additional foot access is proposed via a special walkway from Planning Unit 28.

5.2c Community Park

A 27.7-acre community park (Planning Unit 18) is proposed to serve Cottonwood Hills and other City residents. The park is centrally located in the community at the intersection of Cottonwood Hills and Cottonwood Canyon Roads. The site is accessible from Cottonwood Hills Road and the Street "E" collector.

The park is segmented into two primary areas: the Cottonwood Creek naturalized open space corridor to the west, and the developed pad area to the east. This arrangement permits a diversity of recreational opportunities, both passive and active.

The western creek corridor of about 7.6 acres is proposed to be as natural appearing and functioning as possible. Portions will require regrading to create a natural appearing creek channel as needed for flood control. Existing vegetation should be retained where possible, and augmented by native and naturalized species. A hiking/riding trail corridor is proposed, utilizing the existing dirt road where feasible. The creek area is envisioned as primarily a passive seating/picnicking area and visual resource. This park area is further discussed as Open Space "C" in Chapter 6.

The developed east area of about 20.1 acres is proposed for active recreation uses on flat pads. Opportunities exist to share parking and/or recreational facilities with the adjacent elementary school. Park facilities will be especially accessible from the SF3 (Single-Family Detached) residential project in Planning Unit 21 to the east of Street "E".

Design guidelines for the community park are set out in Table 5.4.

5.2d Creek Park

A 4.4-acre parkway park (Planning Unit 17) is proposed along Cottonwood Canyon Road between Street "B" and Cottonwood Hills Road. This area includes a portion of Cottonwood Creek, regraded as needed for flood control to create a natural appearing creek channel. The park will receive vegetative enhancement under the riparian replacement program outlined in Section 6.2b.

The creek park is planned as primarily a passive park and visual resource. A hiking/riding trail corridor is proposed through the park, utilizing the existing dirt road where feasible. At-grade trail crossings should be planned at Street "B" and Cottonwood Hills Road.

5.2e Project Recreational Facilities

Approximately 39 percent of Cottonwood Hills residents are anticipated to reside in multi-family attached (MF1 and MF2) residential projects. These projects will provide private recreational facilities for resident use, as follows:

- o Minimum of one pool complex per attached residential planning unit, at a ratio of one pool per up to 200 dwelling units. The pool should be rectangular in shape to maximize usability.
- o Weight room as part of pool complex for MF2 attached projects.
- o Passive recreational areas.

Other possible facilities include tennis courts; ball courts; picnicking and bar-b-que areas; and recreational buildings. Project facilities will be maintained by project owners for rental projects, or homeowner associations for condominium projects.

All residential areas will include private open space for recreational pursuits. This includes private yards for detached residential units, and yards, patios, balconies, and decks for attached housing units.

Table 5.3

NEIGHBORHOOD PARK DESIGN GUIDELINES

(Planning Units 7 and 27)

Recreational Facilities - for each park/school complex

- o Multi-sports field accommodating two softball diamonds and soccer field.
- o Paved multi-purpose court complex and/or paved play area.
- o Use of school building for restrooms and indoor recreation; if school is not built, restroom facility to be provided.
- o Children's and tots' play apparatus areas.
- o Meadow/lawn area for informal sports and play.
- o Picnicking and seating areas, taking advantage of view opportunities and providing landscaping for summer shade.

Access and Parking - West Neighborhood Park

- o Auto access from linkage Street "D".
- o Possible shared drive with West Elementary School.
- o Adequate on-site parking. Parking overflow in West Elementary School.
- o Adequate bicycle parking and access.
- o Handicapped parking and accessibility.

Access and Parking - East Neighborhood Park

- o Auto access from collector Street "B" off Cottonwood Hills Road.
- o Possible shared drive with East Elementary School.
- o Adequate on-site parking. Parking overflow in East Elementary School or possible joint usage of parking per Section 7.3.
- o Adequate bicycle parking and access.
- o Special walkway from Planning Unit 28 to provide additional foot accessibility.
- o Handicapped parking and accessibility.

Coordination with Adjacent Elementary School

- o Master plan school/park site to facilitate joint usage of outdoor facilities.
- o Provide for joint usage of outdoor facilities.
- o Arrange for indoor use of school facilities, including restrooms and meeting spaces.

Interface with Adjacent Residential

- o Provide a buffer between the park and adjacent detached residential, utilizing slope differentials; screen trees and plantings; uniform fencing treatments; or a combination of these measures.
- o Design night lighting to minimize spillover into residential areas through proper location and fixture selection.

Table 5.3 (continued)

NEIGHBORHOOD PARK DESIGN GUIDELINES

Appearance and Landscaping

- o Create a relatively flat pad(s) consistent with the proposed recreational facilities.
- o Coordinate the landscaping design of the park with adjacent roadways, including plantings, park entries and signage, and any street furniture and fencing.
- o Utilize a theme canopy tree(s) for parking areas and drives within the park, to provide visual continuity and shade.
- o Coordinate park entry monumentation and signage with the community-wide signage program.
- o For the East Neighborhood Park, landscape the manufactured slope abutting natural open space with groundcover and plantings to control erosion and produce a natural appearance.

Crime Preventive Design

- o To facilitate surveillance, grade the site to be as visible as possible from adjacent roads. Also from park drives and parking areas.
- o Incorporate crime prevention as a factor in the design of lighting, landscaping, and parking areas.

Table 5.4

COMMUNITY PARK DESIGN GUIDELINES

(Planning Unit 18)

Recreational Facilities - for park/school complex

- o Multi-sports field accommodating two baseball diamonds and two soccer fields.
- o Paved multi-purpose court complex.
- o Multi-use paved area.
- o Tennis court complex.
- o Children's and tots' apparatus areas.
- o Large meadow/lawn area for informal sports, play, and events.
- o Picnicking and seating areas, arranged along riparian preserve and providing landscaping for summer shade.
- o Recreation building housing restrooms, concession, office, and storage area. Possibly cluster with elementary school building.

Access and Parking

- o Auto access from Cottonwood Hills Road and/or Street "E".
- o Possible shared drive with Central Elementary School site.
- o Adequate off-street parking to accommodate hikers of Cottonwood Creek trail corridor as well as community park users.
- o Adequate bicycle parking and access.
- o Special walkway from Planning Unit 21/Street "E" through park to creek corridor.
- o Cottonwood Creek trail corridor (See Section 7.7).
- o Handicapped parking and accessibility.

Coordination with Central Elementary School

- o Master plan school/park site if possible to facilitate joint usage of outdoor facilities.
- o Provide for shared drive and/or joint usage or overflow usage of off-street parking per Section 7.3.
- o Coordinate school and recreation building design, to be compatible functionally and aesthetically.
- o Coordinate landscaping of the manufactured slope abutting Open Space "I" to ensure visual continuity.

Cottonwood Creek Riparian Area (Open Space "C")

- o Preserve existing riparian habitat in the northwest corner of the park site.
- o Enhance the riparian habitat area in the southwestern portion of the site, as described in Section 6.2b.
- o Provide a trail corridor, as discussed in Section 7.7.

Table 5.4 (continued)

COMMUNITY PARK DESIGN GUIDELINES

Appearance and Landscaping

- o For the non-riparian portion of the park, create a relatively flat pad(s) consistent with proposed recreational facilities.
- o Coordinate the landscaping design of the park with adjacent roadways, including plantings, park entries and signage, street furniture, and fencing.
- o Carry the riparian planting theme from Cottonwood Creek across Cottonwood Hills Road, utilizing a riparian plant palette and informal clusters.
- o Utilize a theme canopy tree(s) for parking areas and drives within the park, to provide visual continuity and shade.
- o Landscape the manufactured slope abutting natural open space with groundcover and plantings to control erosion and produce a natural appearance.
- o Coordinate park entry monumentation and signage with the community-wide signage program.

Crime Preventive Design

- o To facilitate surveillance, grade the site to be as visible as possible from Cottonwood Hills Road and collector Street "E", and from park drives and parking areas.
- o Incorporate crime prevention as a factor in the design of the recreation building, lighting, landscaping, and parking areas.

5.3 Public Services

This section addresses public services to be provided to the Cottonwood Hills community. Transit service is described in Section 7.4. Implementation of public services is discussed in Section 5.5.

5.3a Fire Protection

Fire protection at an urban level of service will be provided through the City of Lake Elsinore, via a contract with Riverside County or by its own City fire department. Currently, the California Department of Forestry supplies manpower to the Riverside County Fire Department.

Under a regional service concept, the specific plan area is served by two fire stations: Station #60, an existing facility in Canyon Lake; and Station #68, a new facility scheduled for construction at Garboni and Murrietta Roads southeast of the plan area. For the early phases of development, these facilities are adequate to serve the community. However, a new City fire station in the vicinity of Railroad Canyon Road and Interstate 15 will be required, to service Cottonwood Hills and other new projects east of Interstate 15 as well as existing southern Lake Elsinore. Several sites have been designated within other specific plan areas; and no fire station site is required for the Cottonwood Hills property. However, financial participation by Cottonwood Hills in provision of a new fire station is anticipated.

For a discussion of the Fire Management Program for the community, see Section 6.2d.

5.3b Police Protection

Police protection for the community will be provided by the Riverside County Sheriff's Department under contract to the City of Lake Elsinore. Cottonwood Hills will be within the City service area served on a dispatch basis, with police service provided from the Lake Elsinore Sheriff's Station in central Lake Elsinore.

Additional police staff and equipment will be required to serve the community. However, a new police substation on-site is not anticipated. Any financial participation by Cottonwood Hills in the provision of police services will be determined by the development agreement with the City.

5.3c Library Services

Library services for the community will be provided by the Riverside City/County Library System. Services will be provided from two existing facilities: the Lake Elsinore Branch Library in downtown Lake Elsinore and Canyon Lake Branch Library at the main gate commercial center in Canyon Lake. Additional library staff and resources will be needed, but a new branch library building on-site is not projected.

Cottonwood Hills will contribute property taxes allocated through the County to the library system. Additional financial participation by the community, if any, will be determined by the Cottonwood Hills development agreement with the City.

5.3d Solid Waste Service

Solid waste will be collected by the City or its contractor within the community. Commercial and attached projects may contract for private trash collection. Disposal is anticipated at Riverside County landfills within reasonable proximity to the community. Design of trash enclosures shall meet City requirements.

5.3e Postal Service

Public postal service will be supplied by the U.S. Postal Service to the community. Service will be provided from the post office in downtown Lake Elsinore.

5.4 Utilities

A number of utilities services and facilities serving Cottonwood Hills will be operated by public and semi-public agencies. Figure 5.2 depicts the locations of major utilities within or nearby the community.

5.4a Water Service

Potable water will be provided to the specific plan area by the Elsinore Valley Municipal Water District (EVMWD). As shown in Figure 5.2, the plan area is located almost wholly within the sphere of influence of the district, except for the easternmost sixty acres which are located in the Eastern Municipal Water District. The entire community is proposed for annexation into the EVMWD preceding development, with the eastern portion of plan area to be deannexed from the Eastern Municipal Water District.

An existing 33-inch water transmission line runs along the San Jacinto River from the Canyon Lake Filtration Plant. It is proposed to pump community water from this pipeline to two pressure zones at the elevations of 1750 and 1940 feet. Three water reservoirs are anticipated, two sited in the steep southwestern portion of the site (Open Space "G") and one in Open Space "M2". The actual number and location of tanks will depend upon final engineering. From the tanks water will be distributed via water mains, most within public street rights-of-way.

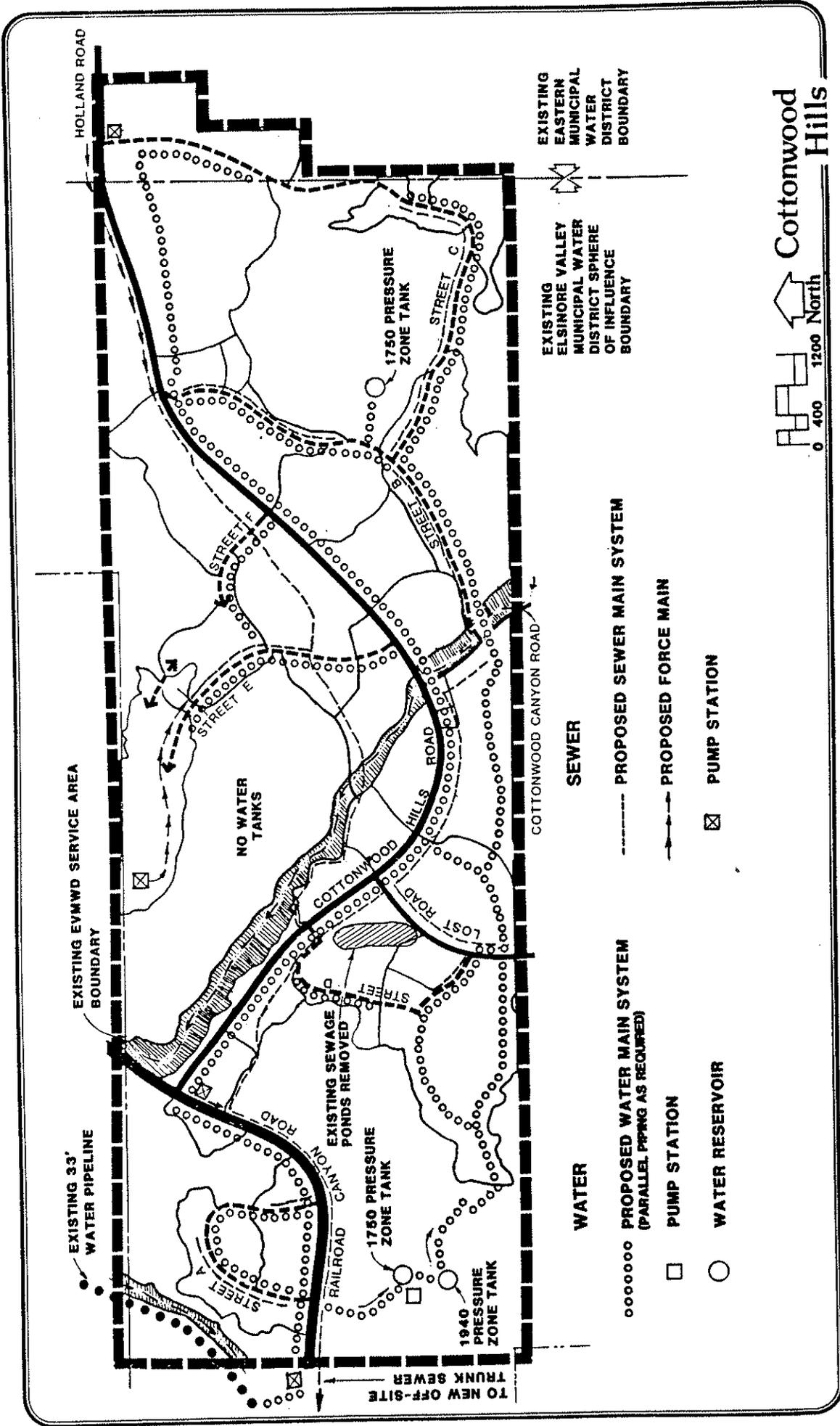
Adequate fire flows and fire hydrants will be provided for in the design of water services.

All water tanks should receive aesthetic treatment to minimize visual disruption. Reservoirs should be sited to the extent practical in locations with little or no visibility from residential areas, and if possible, from public thoroughfares. No tanks should be placed on the central promontory (Open Space "I"). Partial burial or berms may be a consideration. Tanks should be painted to match the background as seen by nearby observers, whether earthtone or skytone. Screening with landscaping may provide effective camouflage in some locations. Siting of water reservoirs should be coordinated with the City and EVMWD.

5.4b Sewer Service

As for water service, sewer service is expected to be provided by the Elsinore Valley Municipal Water District.

A new off-site trunk sewer will be required, connecting the community to an existing trunk sewer west of Interstate 15, which runs onto the Regional Wastewater Treatment Plant. The sewer will generally follow the San Jacinto River, partly within the new Railroad Canyon Road right-of-way.



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3-14-89

5-17

Community wastewater will not be routed to the existing Railroad Canyon Wastewater Reclamation Facility west of the plan area. The existing sewage ponds on-site will not be required after the trunk sewer is installed. They may be relocated on an interim basis and ultimately will be removed during the course of development.

A system of sewer mains, largely within public street rights-of-way, will collect sewage within the community. Most of the specific plan area is served by gravity sewers. However, as noted in Figure 5.2, forced mains with small pump stations will be required in several locations.

5.4c Gas and Electric Service

Electric service within the specific plan area will be provided by the Southern California Edison Company. All electrical distribution lines will be undergrounded, usually within public street rights-of-way.

Above ground electrical vaults should be located to be as unobtrusive as possible, especially from public thoroughfares and parklands. If visually intrusive, vaults should be screened by a solid wall; fencing in combination with landscaping; berming with landscaping; or a dense planted hedge. Accessibility should be maintained as required by the power company.

Gas service will be provided to the community by the Southern California Gas Company. Any gas lines will be undergrounded, generally within public street rights-of-way.

5.4d Communications Services

Franchise telephone and cable television services are expected to be provided by General Telephone Company and other franchise operators. Development will be served by lines installed underground concurrently with other utility distribution lines, generally within public street rights-of-way.

5.4e Utility Access Roads

Under the Specific Plan, several utility access roads will be required running through natural open space. Where possible, existing dirt roads and other disturbed areas should be utilized for pipeline alignments and maintenance access. Where new roads are required, they should be engineered to the extent possible to avoid major scarring of natural open space areas. Any manufactured slopes resulting from utility road grading should be landscaped to control erosion and restored to produce as natural an appearance as possible.

Utility access roads not doubling as trails within the riparian open space corridors should have access control from public streets, such as gates. This is to discourage casual off-road vehicle use.

5.5 Public Facilities and Services Implementation

Implementation mechanisms for public facilities and services are outlined in Tables 5.5a (On-Site Facilities), 5.5b (Services), and 5.5c (Utilities). This includes land, improvements, and operations and maintenance as required. Selection of an option, where there are choices, will occur with approval of a development agreement or final maps as appropriate.

Implementation phasing is addressed in Section 9.3.

Table 5.5a

PUBLIC FACILITY IMPLEMENTATION OPTIONS
ON-SITE FACILITIES

Public Facility	Land	Improvements
ELEMENTARY SCHOOLS	<ul style="list-style-type: none"> o Deeded in fee to school district by developer in lieu of school impact fees. funding sources. o Reservation by developer. Purchase by school district through school impact fees, or state or local funding sources. o Deeded in fee to school district through purchase via Mello Roos. o Deeded in fee to City by developer. Leaseback by school district. o Deeded in fee to City through purchase via Mello Roos. Leaseback by school district. o Dedication to City, reverting to developer if not used. Leaseback by school district. 	<ul style="list-style-type: none"> o Built by school district through school impact fees, and state or local funding sources. o Turnkey - developer built, and developer or partially developer funded. o Turnkey - developer built, and Mello Roos funded or partially Mello Roos funded.

Table 5.5a (continued)

PUBLIC FACILITY IMPLEMENTATION OPTIONS
ON-SITE FACILITIES

Public Facility	Land	Improvements	Operations & Maintenance
JUNIOR/SENIOR HIGH SCHOOLS	o School impact fees.	o School impact fees.	o School tax assessments and other funding sources.
	o State or local funding sources.	o State or local funding sources.	
PUBLIC PARKS	o Deeded in fee to City by developer.	o Built by City through park capital improvement fund fees.	o User fees collected by City.
	o Dedicated to City in lieu of park fees.	o Turnkey - developer built and funded.	o City general fund.
	o Reservation by developer. Purchase by City through park fees.	o Turnkey - developer built, Mello Roos funded.	o School district for leased portions.
	o Deeded in fee to City through purchase via Mello Roos or facilities benefit assessment.	o Turnkey - developer built, Mello Roos funded.	o City-wide Lighting and Landscaping District.
	o City ownership, school district leaseback or joint powers agreement.	o Built through facilities benefit assessment.	o Cottonwood Hills Lighting and Open Space Maintenance District.
		o Built by City through Capital Improvement Program.	

Table 5.5b

PUBLIC FACILITY IMPLEMENTATION OPTIONS
SERVICES

Public Service	Mitigation of Impacts	Operations & Maintenance
FIRE PROTECTION SERVICE	<ul style="list-style-type: none"> o Public safety impact mitigation fee. o Community facilities district. o Financial participation in provision of fire station. 	<ul style="list-style-type: none"> o City general fund. o Special service district. o County Structure Fire Protection allocation from property taxes.
POLICE PROTECTION SERVICE	<ul style="list-style-type: none"> o Public safety impact mitigation fee. o Community facilities district. o Indirectly through property taxes allocated to City. 	<ul style="list-style-type: none"> o City general fund. o Special service district.
LIBRARY SERVICE	<ul style="list-style-type: none"> o Community facilities district. o State bond funding. o Indirectly through property taxes allocated to County. o Impact mitigation fee. 	<ul style="list-style-type: none"> o County library property tax. o User fines.
SOLID WASTE COLLECTION SERVICE	---	<ul style="list-style-type: none"> o General fund, contracted to private service. o Private collection services for attached residential and commercial projects.

Table 5.5c

PUBLIC FACILITY IMPLEMENTATION OPTIONS
UTILITIES

Utility	Land	Improvements	Operations & Maintenance
Off-Site Sewer Trunk Sewer developer fees; possible	<ul style="list-style-type: none"> o Land or easement acquired by Elsinore Valley Municipal Water District. 	<ul style="list-style-type: none"> o Improved by Elsinore Valley Municipal Water District, funded by charges. reimbursement agreement. o Improved by Water District; funded by Mello Roos or facilities benefit assessment. 	<ul style="list-style-type: none"> o Elsinore Valley Municipal Water District user
Off-site Utility Facilities o Water o Sewer o Gas o Electricity	-	<ul style="list-style-type: none"> o Hook-up fees. 	<ul style="list-style-type: none"> o Elsinore Valley Municipal Water District user charges. o Southern California Edison user charges. o Southern California Gas Company user charges.

Table 5.5c (continued)

PUBLIC FACILITY IMPLEMENTATION OPTIONS
UTILITIES

Utility	Land	Improvements	Operations & Maintenance
On-Site Water Reservoirs and Connection to Transmission Line Off-Site	<ul style="list-style-type: none"> o Land or easement dedicated to Elsinore Valley Municipal Water District. 	<ul style="list-style-type: none"> o Improved and funded through subdivision process. o Improved by developer, funded through Mello Roos. 	<ul style="list-style-type: none"> o Elsinore Valley Municipal Water District user charges.
On-Site Utilities	<ul style="list-style-type: none"> o Land or easement dedicated to appropriate agency. Distribution lines generally within public street rights-of-way. 	<ul style="list-style-type: none"> o Improved and funded through subdivision process. 	<ul style="list-style-type: none"> o Elsinore Valley Municipal Water District user charges. o Southern California Edison user charges. o Southern California Gas Company user charges. o General Telephone Company user charges. o Franchise cable television user charges.

Resources
Management Element
Chapter 6



Chapter 6 - RESOURCES MANAGEMENT ELEMENT

6.1 Open Space System

A substantial portion of the Cottonwood Hills community is reserved as open space. As shown in Figure 6.1, this includes both natural and naturalized open space, and developed open space along roadways and within projects. Table 6.1 provides a description of the various community open space areas, including the open space type and functions for each open space. Public parks may be considered an additional component of the open space system, and are discussed further in Chapter 5.

Natural open space and naturalized creek and roadside open space (exclusive of natural parklands and open space interfaces within planning units) together constitute approximately 951 acres, or 48 percent, of the specific plan area. Natural open space consists of property preserved in its existing state, usually with native vegetation and undisturbed landforms. The unique characteristics of each natural open space are described in Table 6.1, utilizing the City General Plan's nomenclature for special area overlays. Naturalized open space is made up of areas graded to a natural or near-natural appearance. These areas are revegetated with native or naturalized species which once established require little maintenance and irrigation. Naturalized open space provides a visual transition from developed property to natural open space, and often functions as a fuel break as well.

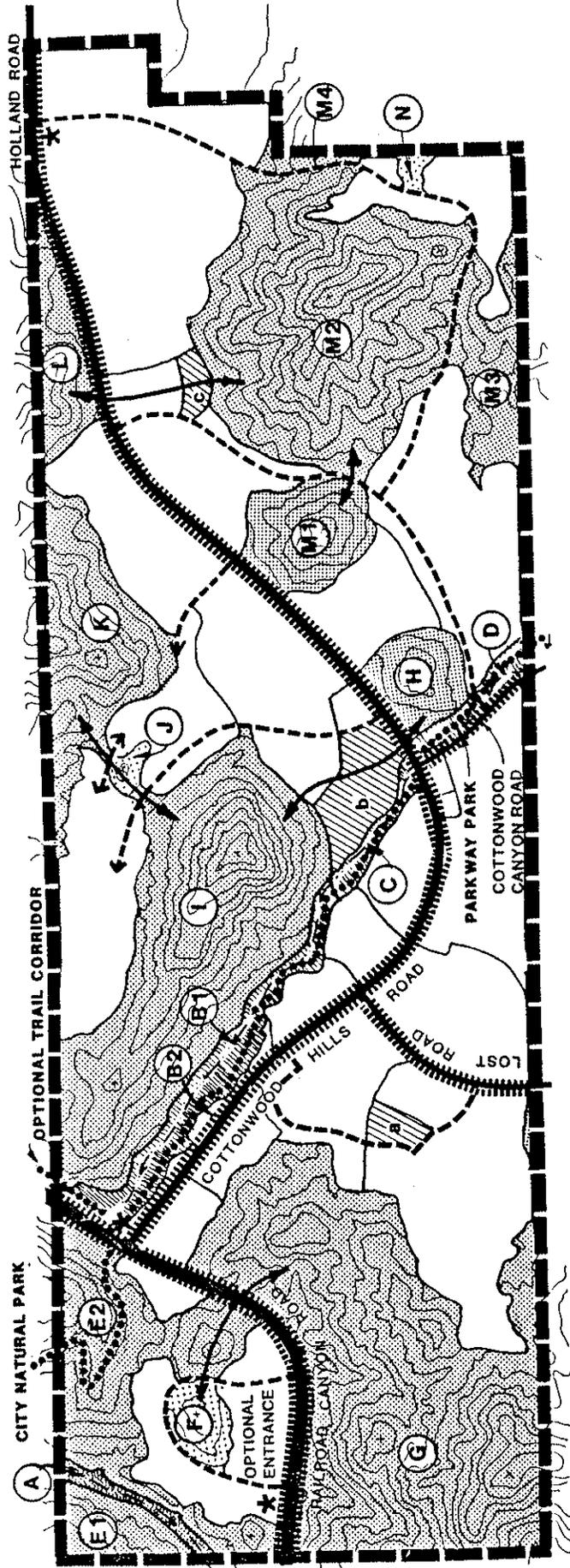
The natural and naturalized open space areas make up an integrated system of community open space. Much of this open space is linked along the Cottonwood Creek and San Jacinto River corridors. Considerable community open space is part of larger open space areas off-site in surrounding properties. Several relatively small open space areas function as common areas within residential enclaves. In some locations, linkages of open space or school/park lands provide visual and/or physical connections between major open space areas, usually across roadways.

In laying out the specific plan area, special care has been given to the provision of natural/naturalized open space vistas from roadways. For both Railroad Canyon Road and Cottonwood Hills Road, significant stretches of roadway are either single-loaded or unloaded by development. This approach provides users of these streets and parallel pathways with scenic visual opportunities. In addition, all residential collector streets are proposed to have some adjacent natural open space or move through large areas of natural open space.

Developed open space consists of much of the open space next to major roads and residential collector streets, plus most of the open space within individual development projects. This type of open space is graded and landscaped to achieve a designed aesthetic effect, usually with ornamental or ornamental and naturalized plantings requiring extensive irrigation and maintenance.

Roadways with parallel pathways provide a linked network of open space parkways within the community. In most cases, street parkways include both naturalized open space transitioning into natural open space, as in the open space vistas described above; and also developed open space, including medians, street rights-of-way, and transitions to adjacent development projects (expanded parkways). The streetscape design guidelines in Chapter 7 are proposed to create visual continuity in the overall street parkway network of the community.

Developed open space within development projects may include recreational facilities; open space interfaces in fire management zones; landscaped buffers between projects or project areas; screening of trash collection, outdoor storage, and delivery areas; parking lot landscape treatments; and seating and passive areas. Project open space can be visually and functionally linked to the street parkways, parklands, and natural/naturalized open space via roads, paths, and landscape treatments.



- | | | | | | |
|--|--|--|--|--|--|
| | NATURAL/NATURALIZED OPEN SPACE | | DEVELOPED / NATURALIZED OPEN SPACE | | VISUAL &/OR PHYSICAL OPEN SPACE LINKAGE |
| | PUBLIC OPEN SPACE- WETLANDS/CREEK | | NEIGHBORHOOD OR COMMUNITY PUBLIC PARK | | HIKING/RIDING TRAIL CORRIDOR |
| | PUBLIC OPEN SPACE- STEEP SLOPES/ROCK OUTCROPS/ TOPOGRAPHIC FEATURES | | PARKWAY OF MAJOR ROAD | | COMMUNITY ENTRANCE |

Community Open Space 6.1

Table 6.1

COMMUNITY OPEN SPACE SYSTEM

Figure Reference Name	Type of Open Space	Open Space Functions	Unique Characteristics Under City General Plan
"A" in Figure 6.1	San Jacinto River, Railroad Canyon Natural Open Space/ Wetlands Public	<ul style="list-style-type: none"> o Component of regional open space corridor including river and dirt road/trail. o Wetlands habitat preservation. o Visual resource for adjacent project and surrounding areas. 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Flood Prone Area
"B1" in Figure 6.1	Lower Cottonwood Creek Natural Open Space/ Wetlands Public	<ul style="list-style-type: none"> o Component of area open space corridor including creek and trail. o Wetlands habitat preservation. o Visual resource for the community and nearby projects. o Visual entrance to the community. 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Flood Prone Area
"B2" in Figure 6.1	Lower Cottonwood Creek Addition Naturalized Open Space/Creek Public	<ul style="list-style-type: none"> o Component of area open space corridor including trail. o Riparian replacement area with enhanced habitat value. o Visual resource for the community and nearby projects, next to Cottonwood Hills Road. o Visual entrance to the community. 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Flood Prone/Area

Table 6 (continued)

COMMUNITY OPEN SPACE SYSTEM

Figure Reference Name	Type of Open Space	Open Space Functions	Unique Characteristics Under City General Plan
"C" in Figure 6.1	Naturalized Open Space/Creek Public	<ul style="list-style-type: none"> o Component of area open space corridor including creek and trail. o Riparian replacement area with enhanced habitat value. o Visual resource for recreational areas in community park. o Visual linkage between Open Spaces "I" and "H". o Scenic resource in heart of community, next to Cottonwood Hills Road. o Visual resource for adjacent projects. 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Flood Prone Area
"D" and "Parkway in Figure 6.1	Naturalized Open Space/Creek Public	<ul style="list-style-type: none"> o Components of area open space corridor including creek and trail. o Riparian replacement area with enhanced habitat value. o Visual resource for adjacent community streets and projects. o Addition to Open Space "H", enhancing adjacent hill. 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Flood Prone Area
"E1" in Figure 6.1	Natural Open Space/ Steep Slopes Public	<ul style="list-style-type: none"> o Scenic backdrop for San Jacinto River. o Buffer to contiguous Canyon Lake Hills estate lots. o Diegan coastal sage scrub habitat preservation. o Steep slope preservation. 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Hillside Area

Table 6.1 (continued)

COMMUNITY OPEN SPACE SYSTEM

Figure Reference Name	Type of Open Space	Open Space Functions	Unique Characteristics Under City General Plan
"E2" in Figure 6.1	Natural Open Space/ Steep Slopes Public	<ul style="list-style-type: none"> o Scenic backdrop for San Jacinto River from off-site. o Addition to and trail corridor access to Canyon Hills "natural park". o Diegan coastal sage scrub habitat preservation. o Steep slope and rock outcrop preservation. 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Hillside Area
"F" in Figure 6.1	Natural Open Space/ Steep Slopes Common Area	<ul style="list-style-type: none"> o Landmark for Railroad Canyon Road developments. o Visual resource for adjacent collector street and regional arterial. o Steep slope preservation. 	<ul style="list-style-type: none"> o Scenic Value o Hillside Area
"G" in Figure 6.1	Natural Open Space/ Steep Slopes Public	<ul style="list-style-type: none"> o Extraction operations, to be discontinued. o Scenic backdrop for Railroad Canyon Road. o Visual resource for adjacent projects. o Buffer to contiguous very low density residential areas to west and south. o Diegan coastal sage scrub and chamise chaparral habitat preservation. o Steep slope and rock outcrop preservation. o Water storage site(s). 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Hillside Area

Table 6.1 (continued)

COMMUNITY OPEN SPACE SYSTEM

Figure Reference Name	Type of Open Space	Open Space Functions	Unique Characteristics Under City General Plan
"H" in Figure 6.1 Central Mound Public	Natural Open Space/ Steep Slopes Public	<ul style="list-style-type: none"> o Visual landmark for central community. o Scenic backdrop for Cottonwood Creek, Cottonwood Hills Road and Cottonwood Canyon Road. o Backdrop and addition to Parkway Park. o Buffer between development areas. o Diegan coastal sage scrub habitat preservation. o Steep slope preservation. 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Hillside Area
"I" in Figure 6.1 Central Promontory Public	Natural Open Space/ Steep Slopes Public	<ul style="list-style-type: none"> o The prominent landform of the community, visible from most of the plan area. o Scenic backdrop for Cottonwood Creek. o Support to creek habitat area. o Visual resource for adjacent projects. o Linkage to Open Space "K" via Open Space "J". o Diegan coastal sage scrub habitat preservation. o Steep slope preservation. 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Hillside Area

Table 6.1 (continued)

COMMUNITY OPEN SPACE SYSTEM

Figure Reference Name	Type of Open Space	Open Space Functions	Unique Characteristics Under City General Plan		
"J" in Figure 6.1 Cemetery Hill	Natural Open Space	o Topographic feature as land-mark.	o Scenic Value		
	Common Area	o Buffer between Planning Units 22 and 23.	o Resource Conservation Area		
		o Linkage between Open Spaces "I" and "K".	o Hillside Area		
		o Diegan coastal sage scrub habitat preservation.			
"K" in Figure 6.1 North Central	Natural Open Space/ Steep Slopes	o Visual resource for east valley and adjacent projects.	o Scenic Value		
	Public	o Buffer to contiguous Canyon Lake estate lots (Valley View Estates) and Audie Murphy Ranch estate lots.	o Resource Conservation Area		
		o Cemetery preservation.	o Historic Resource Value		
		o Linkage to Open Space "I".	o Hillside Area		
		o Diegan coastal sage scrub habitat preservation.			
		o Steep slope preservation.			
		"L" in Figure 6.1 Northeast Corner	Natural Open Space/ Steep Slopes	o Visual resource for east valley.	o Scenic Value
			Public	o Scenic backdrop for Cottonwood Hills Road.	o Hillside Area
				o Buffer to contiguous Audie Murphy Ranch estate lots and area zoned Rural Residential.	
				o Visual linkage to Open Space "M2" via eastern school/park complex.	
o Steep slope preservation.					

Table 6.1 (continued)

COMMUNITY OPEN SPACE SYSTEM

Figure Reference Name	Type of Open Space	Open Space Functions	Unique Characteristics Under City General Plan
"M1" in Southeast - Figure Street "B" 6.1 Peak	Natural Open Space/ Steep Slopes Public	<ul style="list-style-type: none"> o Visual resource for central community. o Scenic backdrop for Cottonwood Hills Road and Street "B". o Buffer between residential developments. o Diegan coastal sage scrub habitat preservation. o Steep slope preservation. 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Hillside Area
"M2" in Southeast - Figure Central 6.1	Natural Open Space/ Steep Slopes Public	<ul style="list-style-type: none"> o Visual resource to east valley and adjacent projects. o Accommodation of estate lots. o Scenic backdrop for Streets "B" and "C". o Linkages to Open Spaces "L", "M1", "M3", "M4". o Diegan coastal sage scrub habitat preservation. o Steep slope and rock outcrop preservation. 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Hillside Area

Table 6.1 (continued)

COMMUNITY OPEN SPACE SYSTEM

Figure Reference Name	Type of Open Space	Open Space Functions	Unique Characteristics Under City General Plan
"M3" and "M4" in Figure 6.1	Southeast - Perimeter Natural Open Space/ Steep Slopes Public	<ul style="list-style-type: none"> o Visual resource to adjacent residential projects. o Accommodation of estate lots. o Buffer to contiguous vacant property and rural residential in County. o Scenic backdrop for Street "C". o Linkage to Open Space "M2". o Diegan coastal sage scrub habitat preservation. o Steep slope and rock outcrop preservation. 	<ul style="list-style-type: none"> o Scenic Value o Resource Conservation Area o Hillside Area
"N" in Figure 6.1	East - Southeast Natural Open Space/ Steep Slopes Common Area or Lotted Out with Open Space Easement	<ul style="list-style-type: none"> o Accommodation of and visual resource to estate lots. o Buffer to contiguous vacant property and rural residential in County. o Retention of landform as part of off-site feature. o Scenic backdrop for Street "C". o Steep slope and rock outcrop preservation. 	<ul style="list-style-type: none"> o Scenic Value o Hillside Area

Table 6.1 (continued)

COMMUNITY OPEN SPACE SYSTEM

Figure Reference	Name	Type of Open Space	Open Space Functions	Unique Characteristics Under City General Plan
Figures 7.3 and 7.4	Railroad Canyon Road	Parkway/Developed and Naturalized	<ul style="list-style-type: none"> o Visual resource to motorists, bicyclists, and pedestrians. o Community entrance. o Project entries. o Noise and air pollution buffer. o Visual screen for parking areas. 	--
Figure 7.5 to Figure 7.9	MAJOR COMMUNITY ROADS Cottonwood Hills Road Holland Road Lost Road Cottonwood Canyon Road	Parkway/Developed and Naturalized	<ul style="list-style-type: none"> o Visual resource to motorists, bicyclists, and pedestrians. o Community entrances. o Project entries. o Aesthetic continuity for community. o Noise and air pollution buffer. o Visual screen for parking areas. 	--
a	PUBLIC PARKS West Neighborhood Park	Parkland/Largely Developed Open Space	<ul style="list-style-type: none"> o Active and passive recreation. o Scenic resource to passersby. o Addition and transition to adjacent natural open spaces; linkage between natural open spaces. 	--
b	Community Park		<ul style="list-style-type: none"> o Preservation of distinctive natural features. 	
c	East Neighborhood Park		<ul style="list-style-type: none"> o Component of overall aesthetic character of community. o Visual resource to adjacent projects. 	
For Parkway Park, see above.				

6.2 Open Space Design Guidelines

This section sets out the overall landscape concept for open space in the community. In addition, design guidelines for the interfaces between development and natural open space are outlined as well as provisions for creating naturalized open space areas.

6.2a Community Landscape Concept

Landscaping of the open space system described in Section 6.1, including both naturalized and developed open space, is critical in creating an attractive, aesthetically coherent community. The overall landscape concept for Cottonwood Hills is outlined below:

- o Recognize the permanent natural open space areas as a visual backdrop and design feature of the community, including both the water courses and steep slopes and knolls. Provide a fire management program together with limiting access to open space areas to protect natural open space resources. See Sections 6.2d (Fire Management) and Section 7.6 (Pedestrian/Equestrian Circulation).
- o For the design treatment of Railroad Canyon Road, coordinate parkway and median landscaping from Canyon Lake to Interstate 15. See Section 7.2 (Streetscape Design).
- o Enhance Cottonwood Creek as a community visual resource by carrying the riparian planting theme across Cottonwood Hills Road between Railroad Canyon Road and Street "E", and across Cottonwood Canyon Road; and by enhancing the creek and adjacent areas in Open Spaces "B2", "C", and "D" and the Parkway Park. See Section 6.2b (Riparian Habitat Areas) and Section 7.2 (Streetscape Design).
- o For major community roads not adjacent to riparian areas, develop a landscaped parkway theme emphasizing non-regimented clusters of vertically-oriented trees responding to views, and a naturalized treatment of interfaces with natural open space. See Section 7.2 (Streetscape Design) and Section 6.2c (Natural Open Space Interfaces).
- o Within planning units, provide a uniform planting theme for each project emphasizing repetitive use of shade trees along streets and/or in parking areas. See Section 3.2b (Residential); Section 4.1c (Commercial); Section 4.2c (Institutional).
- o Create community entrances on Cottonwood Hills Road at Railroad Canyon Road (west primary entrance), and at Street "C" (east secondary entrance), and optionally along Railroad Canyon Road (secondary entrance). For individual planning units, develop identifiable project entries along major community roads based on the community entrance theme. See Section 7.2c (Streetscape Design).

- o For manufactured slopes between land uses and natural open space, utilize landscaping to provide a visual transition from developed to natural areas; to act as a fire break as part of the fire management zone; and for erosion control. See Section 6.2c (Natural Open Space Interfaces); Section 6.2d (Fire Management).
- o For manufactured slopes within planning units, provide for view preservation, erosion control, and a high quality appearance in the development of a uniform landscaping treatment.
- o For buffer areas between land uses, utilize landscaping for screening and visual enhancement which is appropriate to the plant palette of the adjacent planning units. See Sections 3.2b (Residential); 4.1c (Commercial); 4.2c (Institutional); 5.1c (Elementary Schools); 5.2b (Neighborhood Parks); 5.2c (Community Park).
- o For the extraction operation areas, and water tanks and utility access roads with associated manufactured slopes, develop landscape treatments to screen or deemphasize any scarring, to control erosion, and to minimize watering. See Section 6.2e (Extraction Operations); Section 5.4a (Water Service); Section 5.4a (Utility Access Roads).
- o Consider water conservation in plant selection and irrigation design. See Section 6.9 (Water Conservation).

In general, the City of Lake Elsinore's Landscape Guidelines shall be utilized in community landscaping, to provide guidelines and minimum standards for landscaping and to define the landscaping review and approval process. However, as to standards, flexibility is expected to be required in street tree selection and spacing to implement the informal parkway themes outlined above. In addition, landscaping will receive review at a conceptual level in the Site Plan Review process outlined in Section 9.2, before proceeding to the Design Review (Preliminary Landscape Plans) and Landscape Construction Documents steps described in the Landscape Guidelines.

6.2b Riparian Habitat Areas

For development adjacent to the San Jacinto River (Open Space "A") and Cottonwood Creek (Open Spaces "B1" and part of "C"), care should be taken to limit incursion into significant riparian habitat areas. Buffers next to residential areas should be created as described in Table 3.3, and illustrated in Figures 3.5 and 3.6. Grading next to riparian areas is addressed in Section 6.4. Any daylighted areas or manufactured slopes transitioning between planning units and riparian areas should be landscaped with native or naturalized plantings to provide a visual transition and erosion control. Trees common to riparian areas should be placed to frame (rather than block) views from adjacent planning units, and to shade and enhance trail corridors.

For the lower portion of Cottonwood Creek, it is proposed to carry the riparian planting theme across Cottonwood Hills Road. This includes informal, clustered plantings of trees such as coast live oak and cottonwood within the parkway and medians of the road (in the right-of-way). The riparian theme should be incorporated into the expanded parkways along Planning Units 4, 5, 6, 10, 14, 15 and 16; and into the landscape design of the project for Planning Units 11, 12, and 13 and the Community Park as well. Pockets of disturbed areas and proposed manufactured slopes along the northern edge of Cottonwood Hills Road (within open space "B2" as it interfaces with the natural open space) should receive landscape enhancement with the riparian palette also.

For the Upper Cottonwood Creek area in the Parkway Park, Open Space "D", and most of Open Space "C", a riparian area enhancement program is proposed. Figure 7.8 illustrates the proposed grass-lined channel to contain flooding over nearby flat lands. The creek property in these areas should be regraded consistent with functional requirements and contoured for a natural creek appearance. Native and naturalized vegetation should be introduced including trees such as coast live oak, fremont cottonwood, and western sycamore, or similar species. Plantings should be arranged in clusters or in a meandering fashion, to create an informal, natural effect. It is anticipated that run-off and drainage patterns should be sufficient to support this habitat. However, during design of the creek areas for the Cottonwood Canyon Road parkway, Parkway Park and the Community Park, the hydrology and soil conditions should be assessed, and these areas augmented with temporary or permanent irrigation as necessary. For further discussion of the proposed channelization, see Sections 6.4b and 6.5. The riparian plant palette should be carried across Cottonwood Canyon Road, in the parkway landscape treatment for the roadway.

While the majority of riparian habitat and mature trees are preserved under the proposed Specific Plan, some habitat will be removed, especially scattered areas of live oaks and willows. Replacement of areas removed is planned in the riparian enhancement areas: Open Space "B2", Open Space "C" within the Community Park, the Parkway Park (Planning Unit 17), and Open Space "D".

6.2c Natural Open Space Interfaces

Figure 6.2 and 6.3 illustrate typical interface conditions of development with steep slopes in natural open space.

For the typical residential interfaces depicted in Figure 6.2, the rear or side yard of individual detached housing lots or the rear or side yard of attached housing projects acts as an interface with adjacent natural open space. Interfaces should be graded and landscaped to be attractive while providing a portion of the firebreak between buildings and natural vegetation. Landscaping should include hydroseeded or planted groundcover to control slope erosion, with high moisture content/low fuel volume

characteristics; and trees placed toward the toes of slopes to provide visual screening while limiting fire hazard. Non-flammable fencing, such as tubular steel fences or masonry walls, should be provided along the property line of the project. If the fencing is in a highly visible location, such as at an off-site interface as described in Section 3.2, the fencing style and materials should be uniform. To the rear of the fencing, a fuel modification area should be provided as required, where native vegetation is thinned and managed to reduce fuel loading.

At open space interfaces with commercial areas, a 50-foot fire management zone should be provided as described in Table 4.2. Landscaping of manufactured slopes transitioning to open space should be provided as for residential interfaces above. For the high cut slope along the southern edge of Planning Unit 4, "contoured landscaping" should augment grading techniques to produce an attractive appearance.

For the roadway interface, the parkway within the right-of-way and any manufactured slope should act as a transition to natural open space. Grading to achieve a natural appearance is addressed in Section 6.4. In general, sidewalks are envisioned along residential collectors next to open space, but not along major roads, as described in Section 7.6a. Landscaping should consist of naturalized groundcover, with shrubs and accent trees clustered at the toes of slopes and at touchdown points where roadway grading daylights natural open space. Plant selection and spacing should emphasize erosion control and a natural appearance. Landscaping should require little or no irrigation, or temporary irrigation, to establish the plantings; the exception is locations where irrigated plantings are important for fire management. At touchdown points, appropriate planting should be utilized to curb casual access by pedestrians and off-road vehicles to natural open space areas.

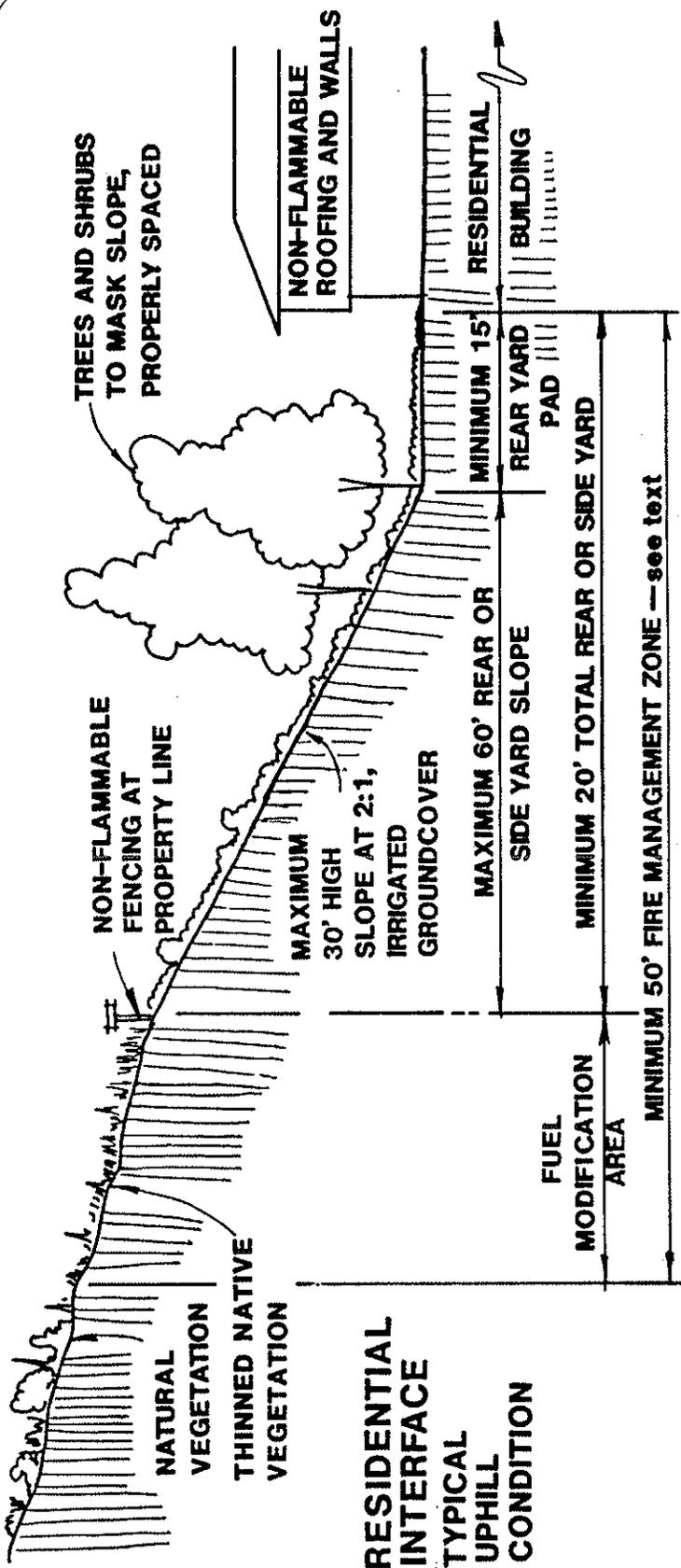
6.2d Fire Management

Cottonwood Hills is located in a hazardous fire area for wildland fires. Accordingly, a fire management program is proposed to protect life and property.

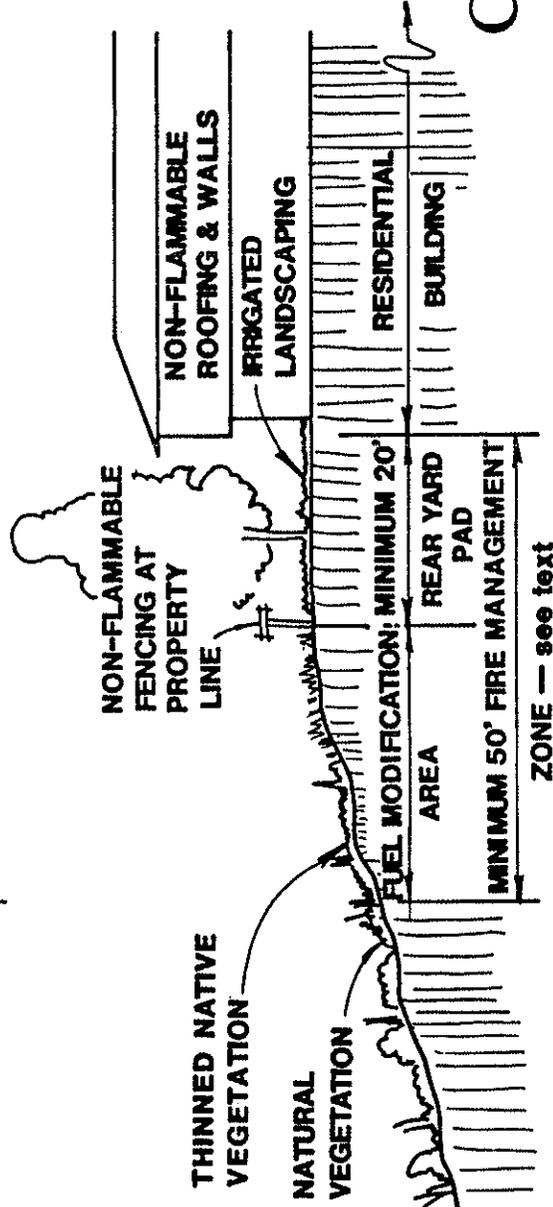
The fire management program for the community consists of five components: fire protection service; water utilities; fire management zones; provision of access; and building design and materials. These are outlined in Table 6.2 and in some cases, described further in other sections of the Specific Plan.

6.2e Extraction Operations

There are two existing sand and gravel extraction operations on the south side of the present Railroad Canyon Road. These activities will be discontinued due to the realignment and upgrading of Railroad Canyon Road, and to the incompatibility of mining operations with nearby proposed residential development. Currently, these are minor operations and their closure does not represent a significant impact on regional sand and gravel resources.



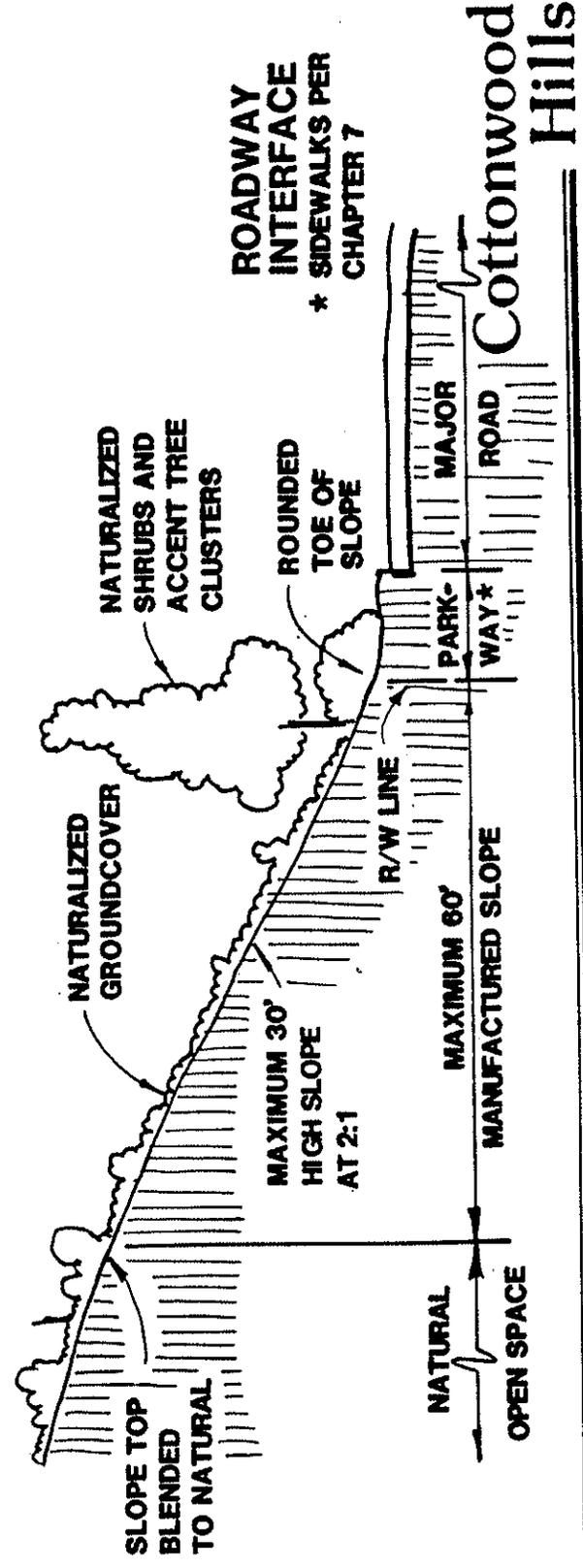
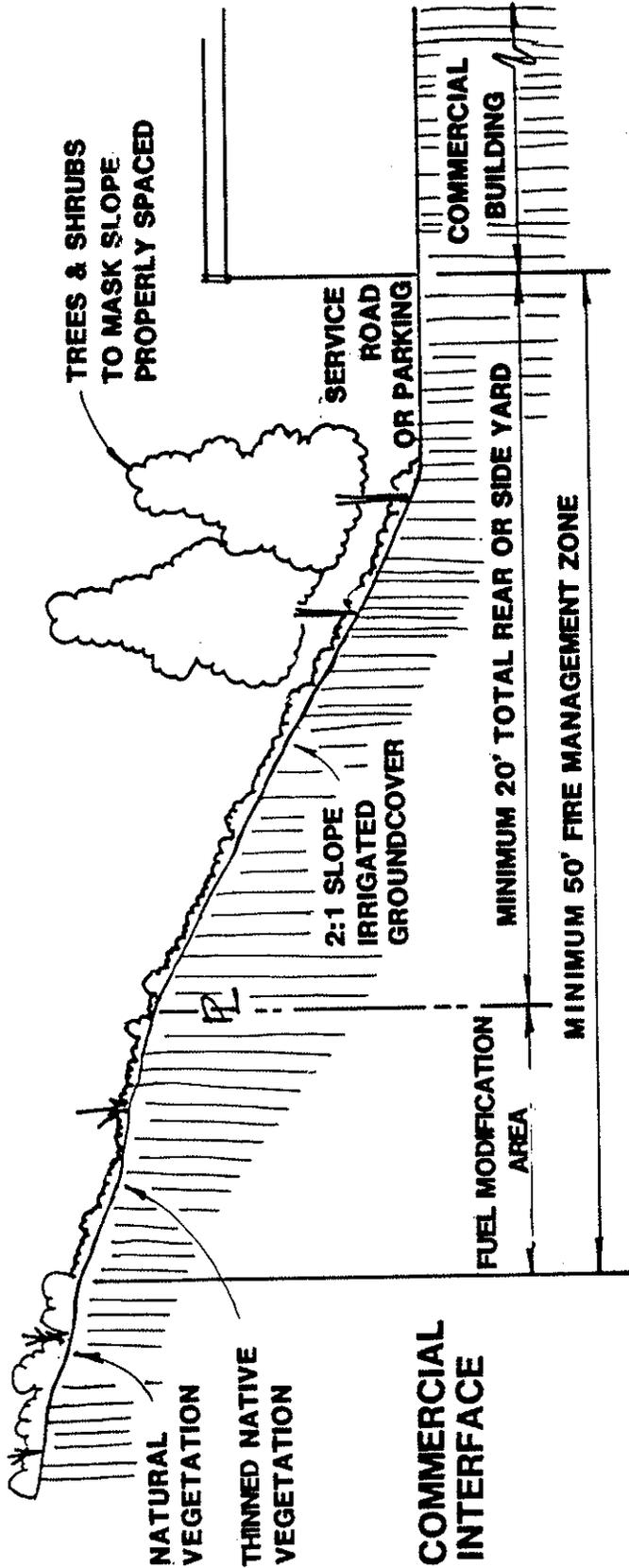
RESIDENTIAL INTERFACE TYPICAL UPHILL CONDITION



RESIDENTIAL INTERFACE TYPICAL DOWNHILL CONDITION

Cottonwood Hills

Open Space Interfaces 6.2



Open Space Interfaces 6.3

During the widening and improvement of Railroad Canyon Road, the roadway is expected to be realigned through the disturbed areas. Any disturbed property remaining should be reclaimed as part of the Railroad Canyon Road improvement project. Disturbed areas should be contour graded to achieve as natural appearance as possible. In addition, naturalized landscaping should be installed, requiring little or no irrigation. Plantings should be selected and placed to achieve a random natural look. Trees should be sited toward the toes of slopes to provide visual screening. Complete landscape coverage of disturbed areas is not necessary, provided erosion control is adequate and a natural appearance is achieved.

Table 6.2

FIRE MANAGEMENT PROGRAM

Fire Protection Service

- o Contribute to a City fire station facility and provide for fire protection services as described in Chapter 5.

Water Utilities

- o Design fire flows for all water mains and fire hydrants to meet the standards of the agency providing fire protection services (City and/or County) and the Elsinore Valley Municipal Water District.
- o Locate fire hydrants in accordance with the standards of the agency providing fire protection services and the Elsinore Valley Municipal Water District.
- o Obtain review and approval of all water system plans by the fire department (City and/or County).
- o Install the required water system improvements with acceptance by the Elsinore Valley Municipal Water District prior to occupancy of any building.
- o Install the required water system improvements or obtain approval of temporary facilities prior to any combustible building material being brought on-site.

Fire Management Zones

- o Create fire management zones a minimum of fifty (50) feet in width at the interfaces of development with natural open space, as set out in Section 6.2c and Section 3.2. The fire management zone should include irrigated yards and slopes; fencing for residential areas; and where required, fuel modification areas. No fire management zone is required next to off-site existing or future development.
- o For fuel modification areas within natural open space areas, thin, prune, and/or clear native vegetation on a periodic basis to reduce fuel loading and fire hazard, while retaining erosion control.
- o For the manufactured slope and yard portions of the fire management zone:
 - Provide a landscaped and/or surfaced rear or side yard depth of a minimum of twenty (20) feet.
 - Select plantings with fire retardant and low fuel volume characteristics, such as ice plant and saltbush.
 - Space trees at a minimum of three (3) times their diameter, and large shrubs at a minimum of eighteen (18) feet on center.
 - Maintain plantings other than trees and large shrubs at no greater than eighteen (18) inches in height; prune tree branches six (6) feet off the ground; and remove dead limbs and plant material on a periodic basis.
 - Adjacent to residential and commercial development, irrigate plantings to ensure high moisture content.

Table 6.2 (continued)

FIRE MANAGEMENT PROGRAM

- o Install non-flammable fencing along common boundaries between rear or side yards and natural open space, as follows:
 - Masonry Wall: Minimum 50-foot fire management zone.
 - Tubular Steel Fencing or Covered Chain Link for view conditions: Minimum 70-foot fire management zone with maximum 50-foot fuel modification area in back of fence; or minimum 50-foot fire management zone entirely irrigated.
- o For maintenance of fire management zones:
 - Provide maintenance of fuel modification zones as part of natural open space maintenance, as described in Section 6.3.
 - Incorporate maintenance requirements for manufactured slopes and yards in CC & R's for development projects.

Provision of Access

- o Consider fire management in the design of local and private residential streets, using the guidelines provided in Section 7.1c.
- o Provide access to open space areas from public and private streets at intervals not to exceed 1,500 feet. Where possible, "double-duty" access ways as hiking/riding trails; utility access roads and easements; touch-down points (daylight areas); and other open space "windows" between residential lots, at the end of cul de sacs, or to the rear of attached projects. A minimum clearance of fifteen (15) feet should be provided between lots or buildings at fire access points designated for use for fire equipment. To discourage casual off-road vehicle use, curb cuts should not be installed except for utility access roads; utility access roads should have access control, such as gates.
- o Provide access to all buildings within commercial, institutional, and residential projects by an all-weather road or driveway in accordance with fire department clearance and distance requirements.
- o Post public and private streets with street name signs.
- o Prohibit flag lots in project design.

Building Design and Materials

- o Adhere to the adopted Uniform Building Code and City of Lake Elsinore fire codes in building design and materials.
- o Utilize fire retardant roofing materials for all buildings, garages, and carports. For single-family detached residential buildings, utilize concrete or clay roof tiles. For attached residential buildings, employ concrete or clay tile and/or built-up roofing with a minimum Class "A" fire rating.
- o For exterior building siding, select materials which are non-flammable (such as stucco) or treated to limit combustibility.
- o Cover chimney outlets or vents with spark arrestors.
- o Sprinkler all commercial buildings.

6.3 Open Space Preservation and Maintenance

Implementation of the community open space system is addressed in Table 6.3. "Figure Reference" refers to Figure 6.1 and other graphics in the Specific Plan. For each open space type, preservation and development mechanisms are outlined, as well as maintenance mechanisms. In some cases, several options are available, usually tailored to specific situations.

To protect its permanent open space status, the majority of natural open space is proposed for deeding, dedication, or open space easement to the City. Most of the natural open space areas and the public portions of the naturalized open space areas are designated for preservation under "OS" zoning. The Open Space District is described in Chapter 8.

The primary vehicle for open space maintenance at the community level is a proposed community-wide Cottonwood Hills lighting and open space maintenance district. This vehicle would assess all property owners in Cottonwood Hills for maintenance of open space areas, which benefit the community as a whole and are important to maintaining the community's overall visual continuity and attractiveness. An alternative is addition of Cottonwood Hills as a distinct zone to Lake Elsinore's City-wide Lighting and Landscaping District. A variation of these alternatives is to place area-level open space areas within the City-Wide District, and community-level open space areas in the Cottonwood Hills District. Should an open space maintenance district(s) prove impractical or unsatisfactory, a master property owners association or master homeowners association would assume maintenance responsibilities.

For individual projects with multiple ownerships, common open space areas will be legally designated. Property owner (homeowner) associations will be responsible for common area maintenance.

Table 6.3

COMMUNITY OPEN SPACE PRESERVATION AND MAINTENANCE

Figure Reference	Type of Open Space	Preservation/Development Mechanism(s)	Maintenance Mechanism(s)
"A", "B1"	NATURAL OPEN SPACE/WETLANDS San Jacinto River, Lower Cottonwood Creek	Ownership decided in fee to City. Zoned "OS".	Cottonwood Hills lighting and open space maintenance district, or equivalent.
"C", Parkway Park	NATURALIZED OPEN SPACE/ CREEK Upper Cottonwood Creek within Community Park, Parkway Park	Developed by City and/or developer. Ownership decided in fee to City. Zoned "OS".	City operation and maintenance. Or Cottonwood Hills lighting and open space maintenance district, or equivalent.
"B2", "D"	NATURALIZED OPEN SPACE/ CREEK Cottonwood Creek within Cottonwood Hills Road and Cottonwood Canyon Road expanded parkways.	Developed by developer. Dedication to or ownership decided in fee to City. Zoned "OS".	Cottonwood Hills lighting and open space maintenance district, or equivalent.
"E", "G", "H", "I", "K", "L", "M"	NATURAL OPEN SPACE/STEEP SLOPES	Ownership decided in fee to City. Zoned "OS".	Cottonwood Hills lighting and open space maintenance district, or equivalent.
"F", "J", "N"	NATURAL & NATURALIZED OPEN SPACE/STEEP SLOPES	Private ownership with open space easement; or common area ownership by private project.	Individual private owner; or project property owners association.

Table 6.3 (continued)

COMMUNITY OPEN SPACE PRESERVATION AND MAINTENANCE

Figure Reference	Type of Open Space	Preservation/Development Mechanism(s)	Maintenance Mechanism(s)
"F", "J", "N"	NATURAL & NATURALIZED OPEN SPACE/STEEP SLOPES	Private ownership with open space easement; or common area ownership by private project.	Individual private owner; or project property owners association.
Figures 6.2, 6.3	NATURALIZED OPEN SPACE Manufactured slopes as transition between natural open space and (a) Public streets. (b) Private development.	(a) Developed by developer. Dedication to or open space easement to City. Zoned "OG". (b) Developed by developer. Private ownership or common area ownership by private project.	(a) Cottonwood Hills lighting and open space maintenance district, or project property owners association, as appropriate. (b) Individual private owner; or project property owners association, as appropriate.
Figures 7.3, 7.4	RAILROAD CANYON ROAD (a) Right-of-way, including median and parkway. (b) Expanded parkway adjacent to private development, including slopes, community entrances, project entries.	(a) Developed by Railroad Canyon Road financing program. Dedicated to City (b) Developed by developer. Private ownership or common area ownership by private project. Open space easement to City as needed.	(a) City - maintained; or City-Wide Lighting and Landscaping District. (b) Individual property owner, or project property owners association, as appropriate. Or Cottonwood Hills Lighting and open space maintenance district.

Table 6.3 (continued)

COMMUNITY OPEN SPACE PRESERVATION AND MAINTENANCE

Figure Reference	Type of Open Space	Preservation/Development Mechanism(s)	Maintenance Mechanism(s)
Figure 7.5 to Figure 7.9	<p>MAJOR COMMUNITY ROADS</p> <p>(a) Right-of-way, including medians, parkways, and temporary parkways.</p> <p>(b) Expanded parkways, including slopes, project entries.</p>	<p>(a) Developed by developer. Dedicated to City.</p> <p>(b) Developed by developer. Private ownership or common area ownership by private project. Open space easement to City as needed.</p>	<p>(a) City - maintained; or City-wide Lighting and Landscaping district.</p> <p>(b) Individual private owner, or project property owners association, as appropriate. Or Cottonwood Hills Lighting and open space maintenance district.</p>
-	<p>DEVELOPED PROJECT OPEN SPACE</p> <p>Residential, commercial, and institutional projects.</p>	<p>Developed by developer. Common area ownership by private project, or private ownership.</p>	<p>Project property owners association, or individual private property owner.</p>

6.4 Grading

This section addresses the objectives, overall concept, and guidelines for grading in Cottonwood Hills.

6.4a Grading Objectives

Grading for the community is based on the following objectives:

- o Protect designated wetlands habitat areas in their natural state, while channelizing and enhancing other creek areas as habitat and visual resources.
- o Preserve steep slopes and prominent topographic features as natural open space.
- o For manufactured slopes in highly visible areas immediately adjacent to natural open space, contour grade to produce a natural appearance.
- o Maximize view opportunities in the buildable portions of the site.
- o Utilize a 30-foot height maximum on manufactured slopes to avoid major scarring.
- o Provide adequate gradients for City streets and sight distances at street intersections as required by City design standards.
- o Balance cut and fill within proposed phases of community build-out to the extent possible.
- o Utilize grading as an element of buffering between land uses; between the community and off-site areas; and between residential areas and major roads.

The sand and gravel extraction areas are discussed in Section 6.2e.

6.4b Community Grading Concept

Figure 6.4 illustrates the grading concept for the Cottonwood Hills community. Under this concept, the bulk of development is confined to buildable areas of less than 25 percent slope.

For the valleys and gently sloping areas, a combination of cut and fill is proposed as shown in Figure 6.5. Generally, valley floors are filled; valley sides are terraced in cut-and-fill; and lower valley walls or perimeter valley areas are cut. This grading approach helps produce a balance of cut-and-fill while permitting maximization of views for residential units.

The San Jacinto River and lower portion of Cottonwood Creek (downstream and northwesterly of the existing Cottonwood Canyon Road - Holland Road intersection) are preserved as wetlands areas with existing riparian habitat. In these areas, grading should daylight above the preservation area where possible. This approach creates buildable pads and roadways separated from the natural open space to be preserved, without spillovers, as shown in Figure 6.5. Where the topography does not permit this approach, fill slopes may be utilized to create a grade separation; the slope height should be as minimal as possible, up to a maximum of 30 feet. For the upper (upstream and southwesterly) portion of the Cottonwood Creek within the proposed Community Park and along Cottonwood Canyon Road, a grass-lined channel is proposed to contain flooding over nearby flat lands. The creek property in these areas should be regraded consistent with functional requirements, including a possible retention basin area; contoured for a natural creek appearance; and planted with naturalized vegetation.

As depicted in Figure 6.5, prominent topographic features including hills and mounds are to be preserved in natural open space. These features act as landmarks for the community and focal points for adjacent development projects.

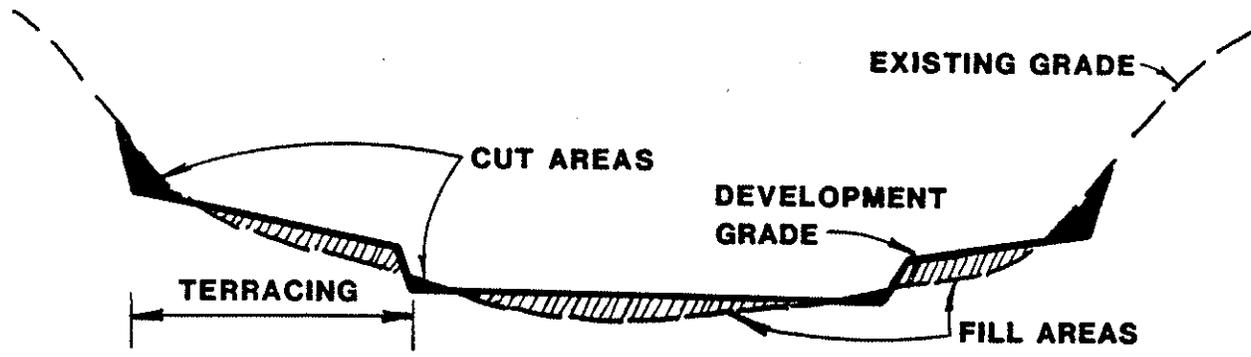
For highly visible manufactured slopes next to major roads, grading should consider visual impacts to motorists. Slopes adjacent to natural open space should be graded to produce a natural appearance.

6.4c Grading Guidelines

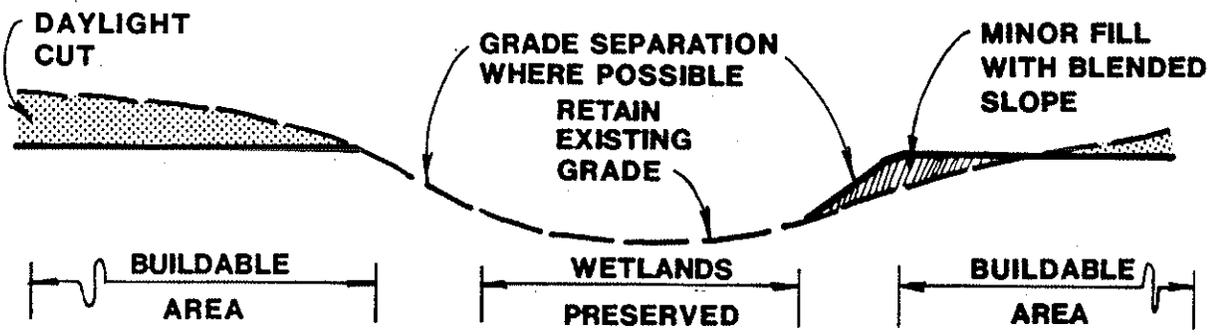
The following guidelines apply to the design and implementation of grading within the Cottonwood Hills community. Additional guidelines for phasing of grading are provided in Section 9.3.

Manufactured Slopes

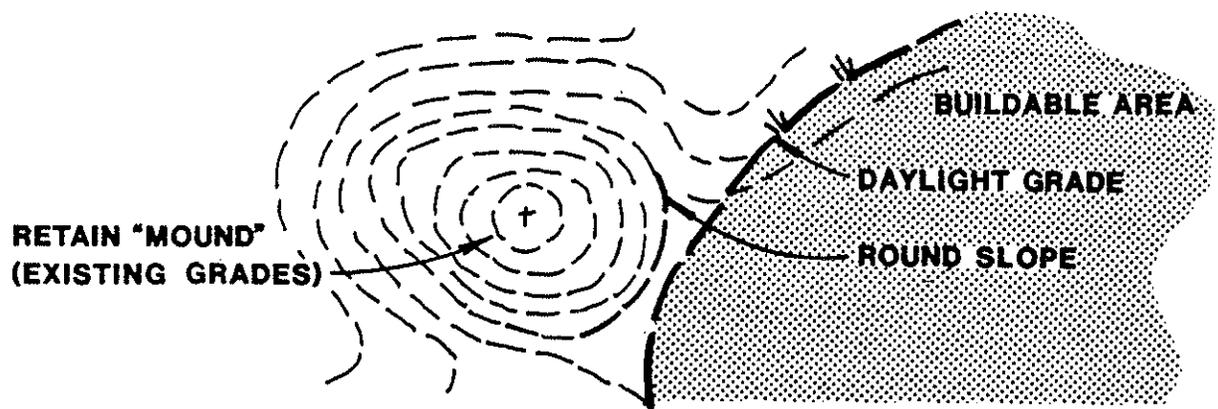
- o Limit manufactured slopes to a 30-foot maximum height within the community. This guideline should apply except as noted below:
 - short slope lengths at interfaces with natural open space and at the perimeter of projects in spot locations, where additional height is required to achieve a naturalized appearance and/or to provide a needed circulation linkage designed to City street standards.
 - at the rear of Planning Unit 4, to achieve a reasonable pad configuration and elevation in relation adjacent streets without undue encroachment into the Cottonwood Creek riparian area.
 - at the interface of Planning Unit 29 with Open Space "M1", to produce a daylight condition where possible next to the open space peak.
 - in Planning Unit 1, where an existing drainage course meets the San Jacinto River, to produce a daylight condition where possible in the transition buffer adjacent to the river.



GRADING IN VALLEY AREAS

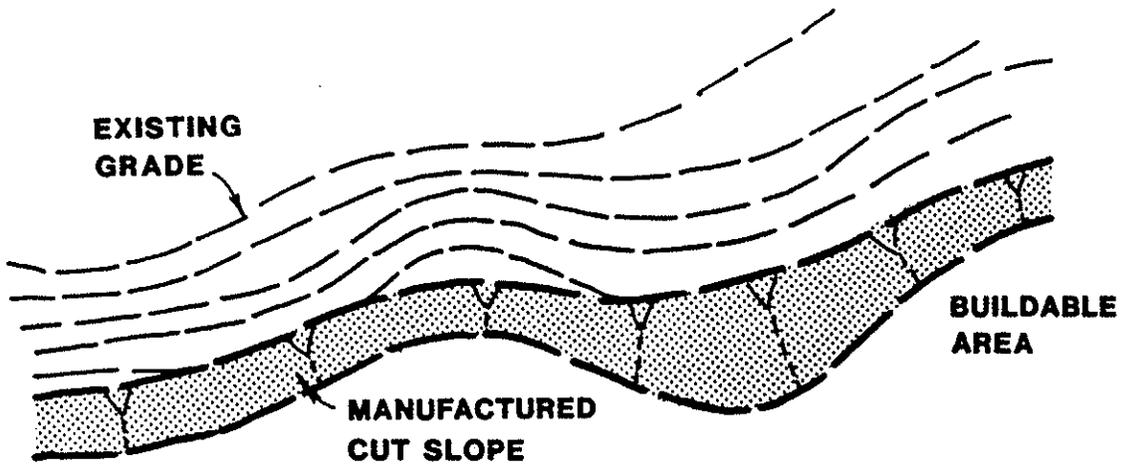


GRADING ADJACENT TO WETLANDS

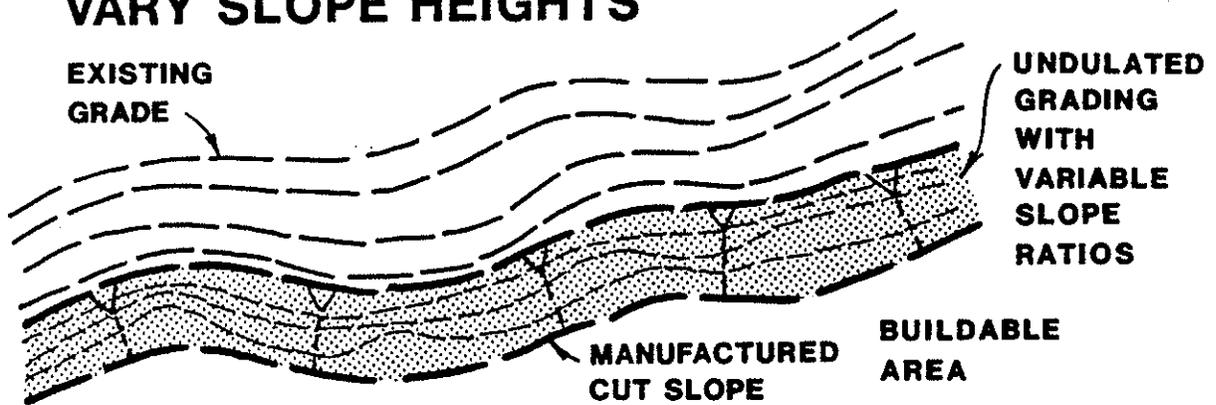


GRADING TO ENHANCE KEY FEATURES

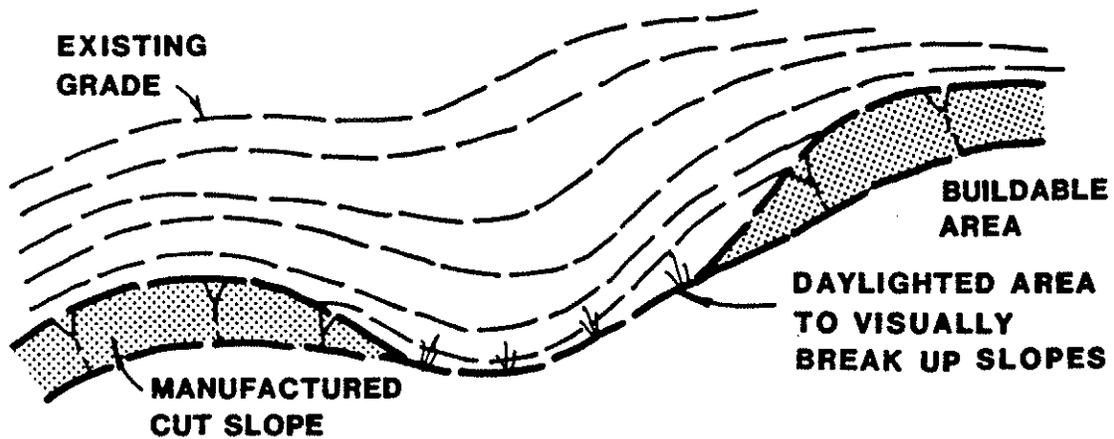
Cottonwood
Hills



VARY SLOPE HEIGHTS



CONTOUR GRADE SLOPES



BREAK UP SLOPES

Cottonwood
Hills

- o Utilize a maximum slope gradient of 2:1.
- o Permit flexible design for manufactured slopes along Railroad Canyon Road. The 30-foot slope height may be exceeded, or a 3/4:1 slope gradient may be utilized in geologically stable areas. This exception is due to the street standards and utilities engineering requirements of the roadway in relation to the terrain and soils.
- o Consider visual appearance in the grading of cut slopes. Under the grading concept outlined in Section 6.4b above, a number of potentially long cut slopes may be created adjacent to natural open spaces. Figure 6.6 illustrates several grading variations to enhance these slopes, including varying slope heights, contour grading, and breaking up long slopes. Other visual techniques should also screen or aesthetically improve long cut slopes, including placement of buildings in front of slopes and well-located landscaping.

Roadways

- o Along the major road system, specially treat manufactured slopes located adjacent to natural open space to produce a natural appearance. As shown in Figure 6.6, grading techniques include varying slope heights; contour grading slopes; and breaking up long lengths of slopes. The maximum unbroken horizontal length of slope at the same height should be 400 feet; in combination with slope height variations and/or contour grading, longer distances may be considered, pending Site Plan Review. The tops and toes of slopes should be rounded, utilizing a radius of twenty (20) feet. Naturalized landscaping should also be installed to visually blend the graded areas into the natural terrain. See Table 7.1b and Figure 6.3.
- o For lengthy manufactured slopes within the expanded parkways of major roads (abutting development projects), vary the slope faces in coordination with the landscaping to avoid a monotonous appearance. See Table 7.1c.
- o Flatten and round slope banks at community entrances and project entries to create attractive entrance points and provide sight distances for motorists.
- o Utilize City of Lake Elsinore and Riverside County standards, as appropriate, for the engineering of street grades and sight distances.

Residential Development

- o For residential areas, utilize terracing where appropriate to work with the natural grade and to maximize view opportunities, as shown in Figure 6.7.

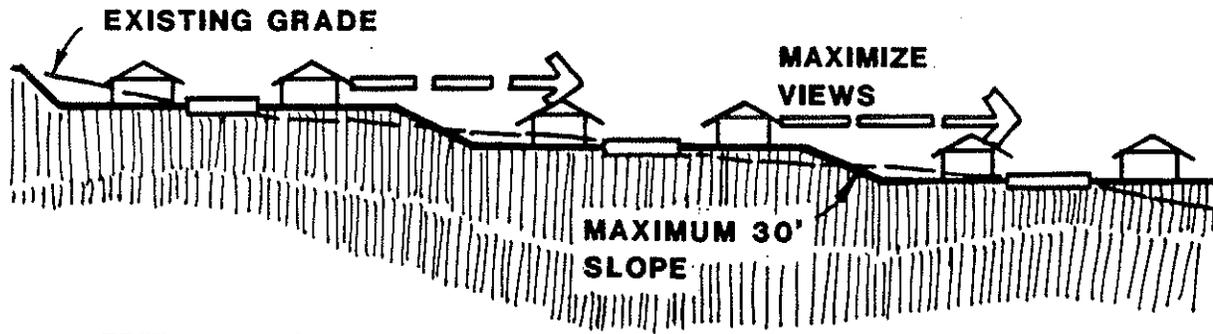
- o For interfaces with off-site rural residential areas, use elevation differentials where possible to provide a buffer. An example depicting a slope downward into the community as is illustrated in Figure 6.7. Section 3.2 addresses the interfaces of residential projects with off-site uses.
- o For interfaces between residential areas and watercourses, limit grading into riparian habitat areas. Section 3.2 provides design guidelines for riparian interfaces with residential areas.
- o Where possible, buffer residential areas from highly trafficked major roads using slope banks or berms. Figure 6.7 demonstrates the residential pad above the street, with a slope as a buffer.

Non-Residential Development

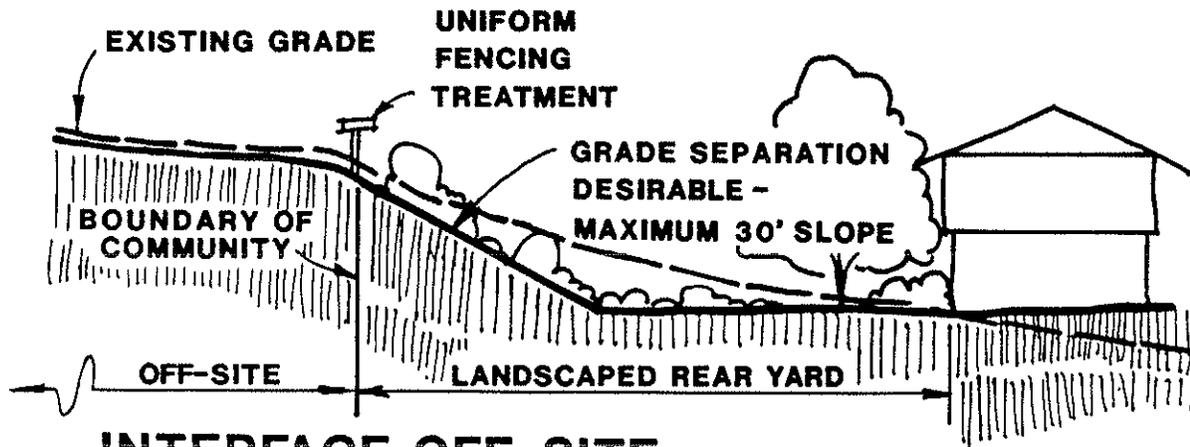
- o For commercial areas adjacent to major roads, keep the pads as close to the street elevations as possible to facilitate access and project visibility. At the same time, the use of minor slopes and berms as well as landscaping and siting techniques is encouraged to screen large expanses of parking from passersby.
- o Utilize slope banks where feasible to buffer residential areas from immediately adjacent non-residential uses, such as commercial development and school sites.

Grading Operations

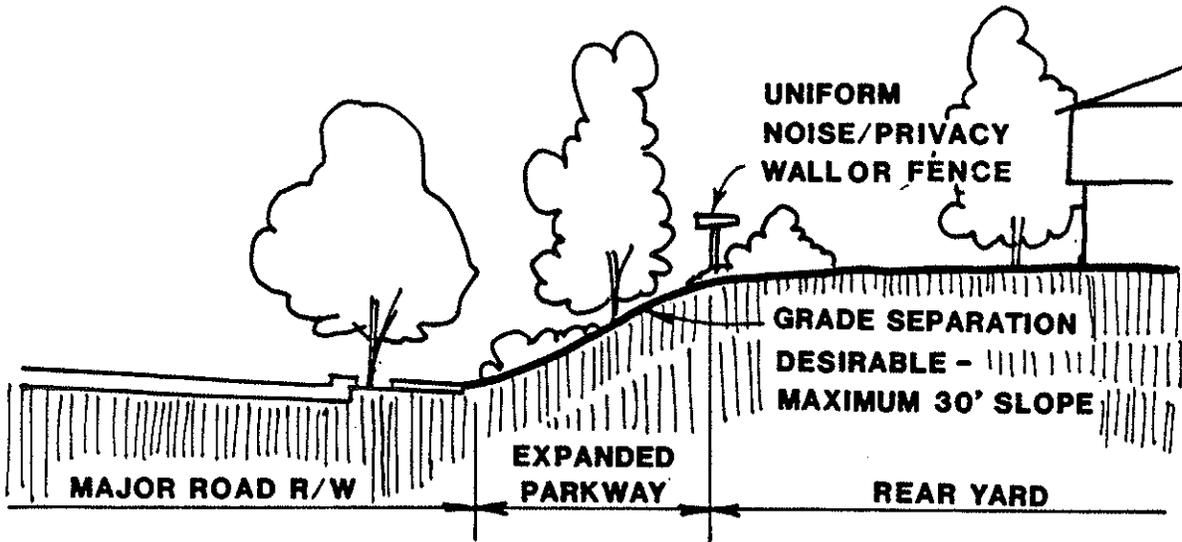
- o Consider the potential for erosion and settling in all grading operations. To the extent feasible, earth moving should be accomplished in phases, to avoid clearing of ground far in advance of grading.
- o Control grading operations, to avoid spillovers into natural open space areas and trampling native vegetation to be preserved.
- o Recognizing that brush or native vegetation must be cleared or trimmed to protect built structures, limit clearing within designated natural open spaces to the minimum necessary to comply with fire codes and the fire management program, and preserve the natural landform. Steep slopes in natural open space areas should retain their native root stock or be replanted with native or naturalized fire retardant vegetation.
- o Protect natural features, such as significant rock outcrops and specimen trees within designated natural open space areas, to the extent feasible during both grading design and operations.



TERRACING



INTERFACE OFF-SITE



INTERFACE WITH MAJOR ROADS

Cottonwood
Hills

The Grading Ordinance (Ordinance 801) of the City of Lake Elsinore should be applied to individual development projects within Cottonwood Hills as each is designed, approved, and implemented. Where the ordinance and these guidelines disagree, the Specific Plan guidelines should be utilized.

6.4d Geology and Soils

Cottonwood Hills is located within an alluvial basin bounded by the Elsinore fault zone to the west and the San Jacinto fault zone on the northeast. The site is generally underlain by dense granitic and metamorphic bedrock which would not experience any settlement during a seismic event. However, Cottonwood Canyon itself is underlain by older alluvial fan deposits. Under seismic loading conditions, settlement potential for these on-site alluvial materials is low, but soils below the water table have a high liquefaction potential. With construction, settlement of foundations in bedrock areas will be negligible; and in fill areas, within tolerable limits once alluvial soils are removed.

Engineering and construction for the specific plan area should be conducted in accordance with the recommendations of a registered engineer, to ensure mitigation of geologic and seismic impacts on the property. Detailed grading plans will be reviewed by the City to ensure appropriate treatment of cut and fill slopes, alluvial removal, and handling of drainage. During construction, continuous observation and testing under the direction of the soils engineer and/or engineering geologist is required to verify conformance with the recommendations and to confirm that the conditions found are consistent with the geotechnical investigation.

6.5 Drainage and Hydrology

This section describes the objectives and concepts for drainage in the Cottonwood Hills community.

6.5a Drainage Objectives

The following objectives provide the basis for the community drainage concept:

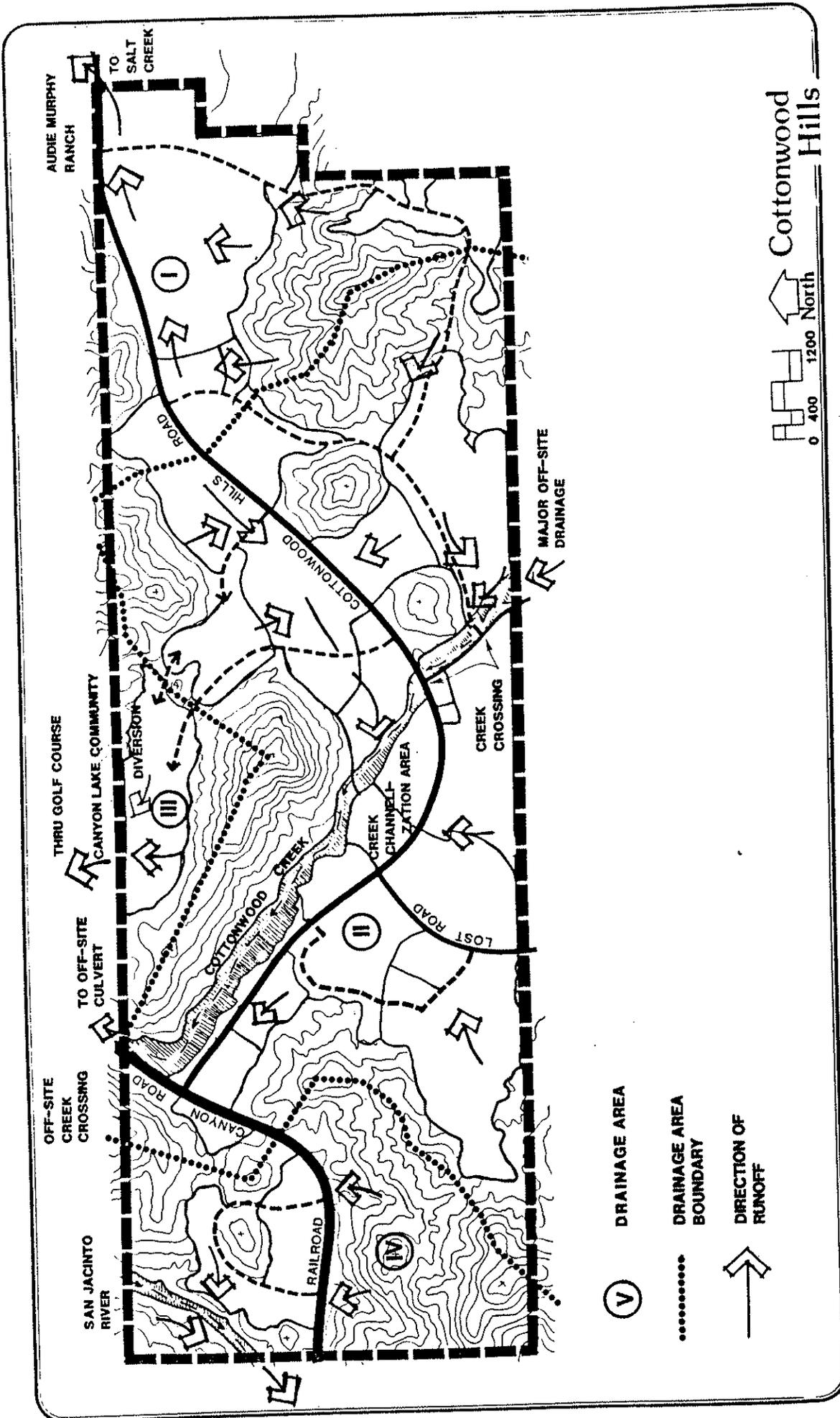
- o Maintain the basic drainage pattern from the existing to developed condition. In general, avoid diversion of flows from existing drainage courses.
- o Control soil erosion of and sedimentation from steep natural slopes and manufactured slope banks.
- o Retain the San Jacinto River in its existing state.
- o For Cottonwood Creek, provide improvements as necessary to control flooding and permit adequate community circulation.
- o Consider off-site areas in the design of community drainage facilities.

6.5b Drainage Concepts

Figure 6.8 illustrates the four major drainage areas within the specific plan area. The drainage concept for each area is outlined in Table 6.4. Overall, the anticipated runoff from the community is estimated at 5 to 7 percent above existing levels. It is anticipated that this drainage can be accommodated by existing facilities off-site for areas II, III, and IV, and proposed new facilities associated with Audie Murphy Ranch in drainage Area I. The increase in run-off due to Cottonwood Hills will constitute less than 1% of the San Jacinto River flows.

All drainage and flood control measures shall be provided in accordance with the requirements of the Riverside Flood Control and Water Conservation District. The tentative map for any parcel or tract shall include information concerning on-site and off-site drainage and drainage facilities, per City requirements. A preliminary hydrology study for major flow areas and a preliminary hydraulic design for the storm drain system and structures shall be submitted with the Site Plan Review application accompanying tentative maps. On-site facilities will be provided by the developer(s) in conjunction with development. Maintenance of storm drains and street culverts will be the responsibility of the City.

Section 9.3d addresses grading and drainage during construction.



Drainage Concept 6.8

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Table 6.4

DRAINAGE CONCEPTS

Area I

- o Route development run-off off-site via a storm drain system in Street "C" and Cottonwood Hills Road/ Holland Road.
- o Coordinate with Audie Murphy Ranch concerning routing community run-off through the proposed Salt Creek flood control/water quality improvements, and with regard to the phasing of these improvements.

Area II

- o Channelize upper Cottonwood Creek (from the southern boundary of the community to the heavy riparian area in the Community Park), to prevent potential flooding over adjacent properties. Consider off-site upstream properties and possible on-site retention in the design of this channel. See Section 6.4b.
- o Install box culverts for street crossings to accommodate Cottonwood Hills Road and Street "B".
- o As required, increase the capacity of the existing culvert under Railroad Canyon Road and downstream facilities to accommodate Cottonwood Creek drainage, as it moves on ultimately to the San Jacinto River.
- o Limit incursion into lower Cottonwood Creek (from the heavy riparian area in the Community Park to the northern community boundary), retaining the existing drainage pattern.
- o Route run-off via a storm drain system into Cottonwood Creek, and provide erosion and siltation control measures as needed.
- o Obtain any special permits or approvals required for creek-related improvements.

Area III

- o Route run-off via a storm drain system, for subsequent drainage off-site.
- o Divert increased run-off from the easterly portion of the drainage area from flowing directly into Canyon Lake.
- o Direct off-site flows via the existing drainage course, ultimately to the San Jacinto River. Coordinate with the Canyon Lake community.
- o Utilize erosion and siltation control measures as required.

Area IV

- o Route run-off into the San Jacinto River via a storm drain system including Street "A" and Railroad Canyon Road.
- o Provide erosion and siltation controls as necessary.
- o Limit incursion into the San Jacinto River, retaining the existing drainage pattern.

6.6 Energy Conservation

This section addresses energy conservation for the Cottonwood Hills community.

6.6a Energy Conservation Measures

Energy conservation measures which will be incorporated into the community are described below:

- o Title 24 of the California Administrative Code requires that new buildings be designed to meet energy performance standards for space conditioning and water heating. Compliance can be achieved through a combination of measures, including systems design and equipment for space conditioning and water heating; appliance selection; roof, wall, and infiltration insulation; lighting; and swimming pool heating.
- o The circulation system and land use plan for the community contributes to energy conservation. Use of alternatives to the private automobile is encouraged by convenient bicycle and pedestrian routes and by accommodating public transit. Support commercial and public facilities are provided within the community for residents.
- o The community will be supplied gas service in addition to electric power, as discussed in Section 5.4c.
- o The parkways for the major streets and minor roadways should utilize street trees to shade sidewalks and any on-street parking, per Chapter 7.
- o All swimming pools in multi-family attached (MF1 and MF2) projects should be heated by solar collectors.
- o Overhangs for shade in single-family detached residential (SF1, SF2, SF3) planning units should project a minimum of 18 inches on front elevations and 12 inches on rear and side elevations.
- o In multi-family attached (MF1 and MF2) projects, parking should be shaded using garages, carports, canopy trees, and/or shaded locations.
- o In commercial and institutional parking and pedestrian areas, shading devices should be utilized extensively, such as canopy trees, arcades, decorative awnings, and porticos.

6.6b Subdivision Design

To the extent practical, energy conservation concerns should be considered in project layout and building design. However, it is recognized that energy concerns must be realistically balanced with recreation, privacy, aesthetic, grading, market, and other environmental considerations. Under Section 66473.1 of the State Subdivision Map Act, all proposed projects

should address passive or natural heating and cooling opportunities in subdivision design. This requirement should be met as part of the Tentative Map/Site Plan Review package, as described in Chapter 9.

As a first priority, project design should consider summer cooling opportunities. Possible measures include

- o Orienting buildings and ventilation to take advantage of south-southwest prevailing summer breezes.
- o Utilizing overhangs and other shading devices to shadow south and west building exposures during the summer.
- o Using deciduous trees to shade southern building exposures, parking areas, sidewalks, and recreation areas during the summer, yet allow solar access during the winter.
- o Providing year around shade where possible to western building exposures, parking areas, and pedestrian areas.
- o Considering summer glare and reflection in the selection of hardscape materials, particularly in public projects.

As a second priority, project design should consider year around direct access for solar collectors. Possible measures include

- o Maximizing the length or area of south-facing walls and/or roofs in houses and residential buildings.
- o Optimizing active access for solar collectors for pool areas in attached residential projects.
- o Limiting shade from vegetation and building overhangs on the southern building exposure, but considering summer cooling opportunities.

As a third priority, project design should take into account winter passive solar heating opportunities. Potential measures include

- o Utilizing a north-south lotting pattern on east-west streets, which is expected to be the predominant pattern for terraced single-family detached projects in the specific plan area. This maximizes the southern exposure of glazing facing the front and rear yards.
- o Considering shadow patterns and passive solar access in laying out attached residential projects.
- o Incorporating summer shading devices which can permit winter passive heating of buildings on the southern exposure, such as roof overhangs; deciduous street and yard trees; and rear yard patio covers.

6.7 Cultural Resources

This section discusses the mitigation of impacts by development on cultural resources within the specific plan area.

Three prehistoric sites are considered important archaeological resources on the site, because they may help determine when the area was settled and from where the prehistoric inhabitants came. Development is proposed for these sites, and it is not feasible to preserve them in open space. To mitigate this significant impact, a program to recover information from the sites prior to grading is required. This program should include a test phase to identify important cultural deposits located at each site and define site boundaries. If important cultural deposits are found, a subsequent data recovery phase should be undertaken. The mitigation program should be completed prior to issuance of land development permits.

In addition, there is a historic family cemetery on-site, as well as a nearby homesite noted on the 1898 U.S.G.S. map. The cemetery is located in Open Space "K," a designated public open space area, and should be preserved. To prevent vandalism, the grave markers should be removed and stored. At the Manker family homesite, the initial clearing operations for construction should be monitored by an historic site archaeologist to determine if there are any structures or historic artifacts which can be recovered. If any significant historic material is discovered, it should be removed before grading occurs. Initially, limited grading should be undertaken to discover any buried historic resources. If any are found, a data recovery phase should be accomplished prior to completion of grading operations.

Chapter 10 notes which planning units must include cultural resources mitigation during design and construction.

6.8 Biological Resources

This section addresses the mitigation of impacts on biological resources within the specific plan area.

6.8a Vegetation

Under the Specific Plan, approximately 1,033 acres of Cottonwood Hills will be developed or placed in naturalized open space, and approximately 936 acres will be preserved in natural open space. Much of the proposed development is sited in disturbed areas or in non-native grasslands. Most of the natural open space constitutes large, relatively undisturbed tracts of native vegetation.

This approach results in the permanent preservation of substantial portions of the following plant communities on-site:

- Diegan coastal sage scrub
- Chamise chaparral
- Southern willow scrub
- Southern cottonwood-willow riparian forest
- Southern coast live oak riparian forest

The direct impacts on general vegetation resources resulting from community development remain below a threshold level of significance. Impacts to scattered areas of southern coast live oak riparian forest and to southern willow scrub areas in upper Cottonwood Creek will be mitigated by implementation of a riparian enhancement program in Open Spaces "E2", "C", "D", and the Parkway Park. This program is described in Section 6.2b.

There are no known significant impacts to sensitive plant species, including listed rare or endangered species. The preservation of relatively undisturbed tracts of natural habitat within open space areas provides adequate mitigations of potential impacts to sensitive plant populations.

6.8b Wildlife

Much of the most significant habitats for wildlife are preserved in natural open space. These encompass wetlands, woodlands, coastal sage scrub, and rock outcroppings.

The Plan contains a series of measures to preserve the San Jacinto River as wetlands habitat. This includes preservation of natural open space (Open Space A), as well as sensitive design of adjacent development in Planning Unit 1 (see Section 3.2). If they do use the river habitat, the least Bell's vireo, a listed endangered species, should be adequately protected.

All of the identified and potential habitat of the Stephen's kangaroo rat, an endangered species, will be eliminated by proposed development. This significant adverse impact will be mitigated by participation in the regional management program established by the County under a Habitat Conservation Plan (HCP); or in an interim City program consistent with the goals of the HCP and approved by the U.S. Fish and Wildlife Service. Because the area of habitat used by the Stephen's kangaroo rat within the specific plan area appears to be much less than the minimum area necessary for long-term conservation of populations of the species, avoiding the adverse impact by preservation on-site is not expected to attain the desired result of preservation of this species on the property.

Significant impacts by development are anticipated to the black-tailed gnatcatcher and orange-throated whiptail, two animals listed in Category 2 of listed species under consideration for inclusion on the Endangered Species List of the U.S. Fish and Wildlife Service. Conversion of substantial portions of coastal sage scrub and chamise chaparral from a status of no protection from grazing, agriculture and recreation uses, to large parcels of protected natural open space adequately mitigates these impacts. Also, this conversion adequately mitigates impacts on other sensitive animal species identified on-site.

6.9 Water Conservation

Water conservation should be considered in the selection of mechanical equipment and plumbing fixtures in Cottonwood Hills projects. Emphasis should be placed on devices and design characterized by low water requirements and efficient utilization of water.

Low flush toilets and urinals are required under the California Health and Safety Code Section 17921.3, and efficiency standards for maximum flow rates for faucets and showerheads are specified by Title 20 of the California Administrative Code.

In addition, landscape design should recognize water conservation concerns. Choice of plant materials should emphasize low water requirements and should minimize water run-off. Landscape watering systems should supply water efficiently, minimizing waste. In landscape design, water conservation concerns should be balanced with two other factors: (1) the need for canopy trees and shade, and (2) the high moisture content/low fuel volume requirements of landscaping in fire management zones.

In single-family detached planning units with four or more model home plans, one of the model homes shall incorporate water conserving landscape design principles. This model home should demonstrate exclusively low water use plant materials and low flow irrigation systems, with appropriate signage. Home buyers should be referred to the City or water district for literature concerning water conservation in landscaping.

Circulation Element

Chapter 7

Chapter 7 - CIRCULATION ELEMENT

7.1 Street System

This section describes the street system for the Cottonwood Hills specific plan area, including ties to surrounding roadways. Figure 7.1 illustrates the larger area-wide circulation network, while Figure 7.2 depicts the street system within the community.

7.1a Railroad Canyon Road

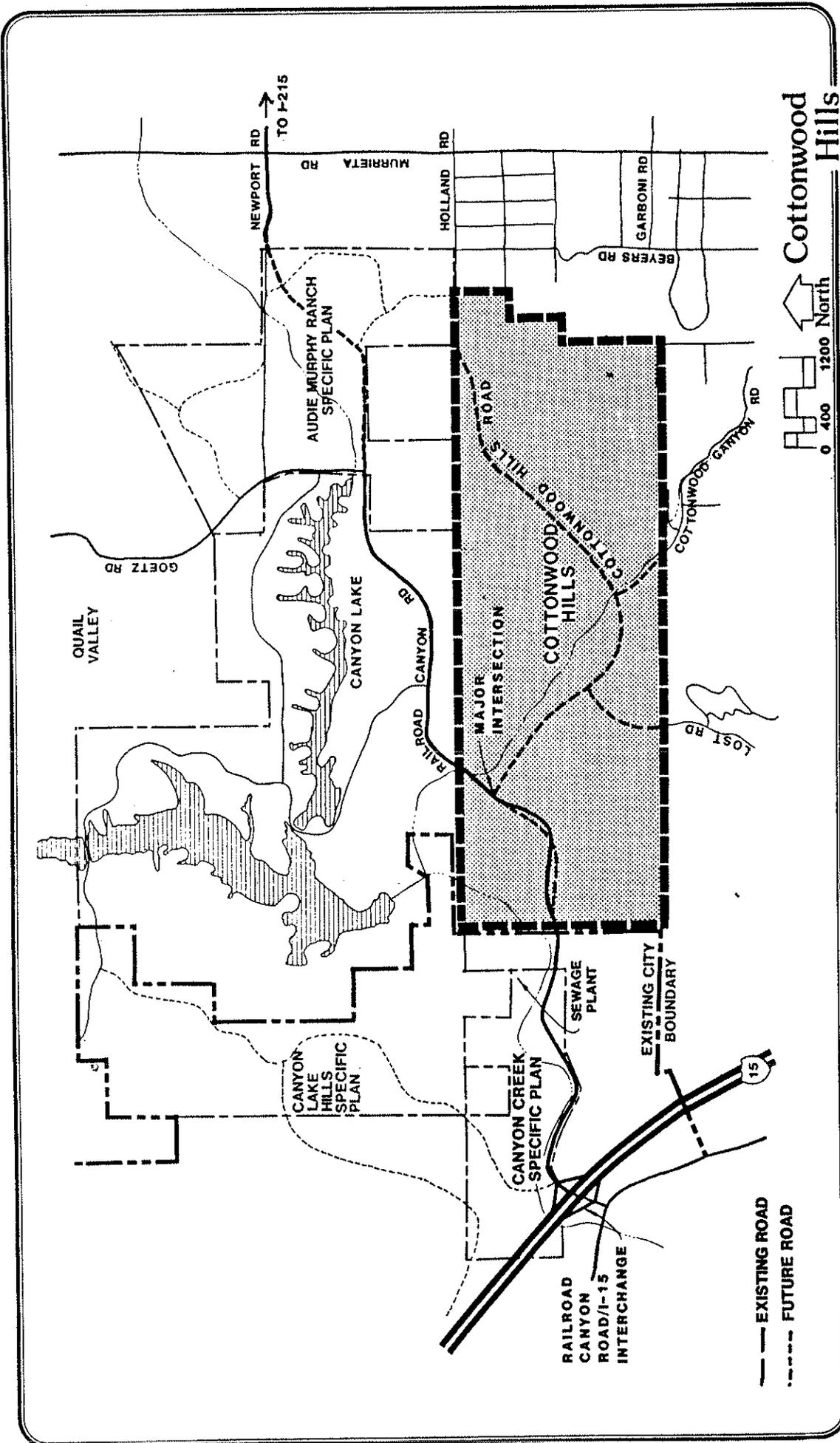
At present Railroad Canyon Road is a curbless, two-lane paved roadway running through the northwest corner of Cottonwood Hills and then westward to an interchange with Interstate 15. The existing Average Daily Traffic (ADT) volume is 10,500 vehicles within the specific plan area.

As shown on Figure 7.1, Railroad Canyon Road is expected to ultimately connect to Newport Road within the Audie Murphy Ranch Specific Plan area. This will provide a continuous east-west linkage between Interstates 215 and 15. Railroad Canyon Road is classified as a 110-foot Arterial Highway in the Riverside County General Plan, with a four-lane divided highway configuration. A portion of roadway within the adjacent Canyon Lake community is already built to a four-lane standard.

In determining future requirements for a regional roadway such as Railroad Canyon Road, four types of traffic generators must be assessed:

- o Existing traffic.
- o Traffic generated by general expected growth in the region.
- o Traffic generated by build-out of major developments, such as Audie Murphy Ranch and Summerhill (Canyon Creek).
- o Traffic generated by development of the Cottonwood Hills community.

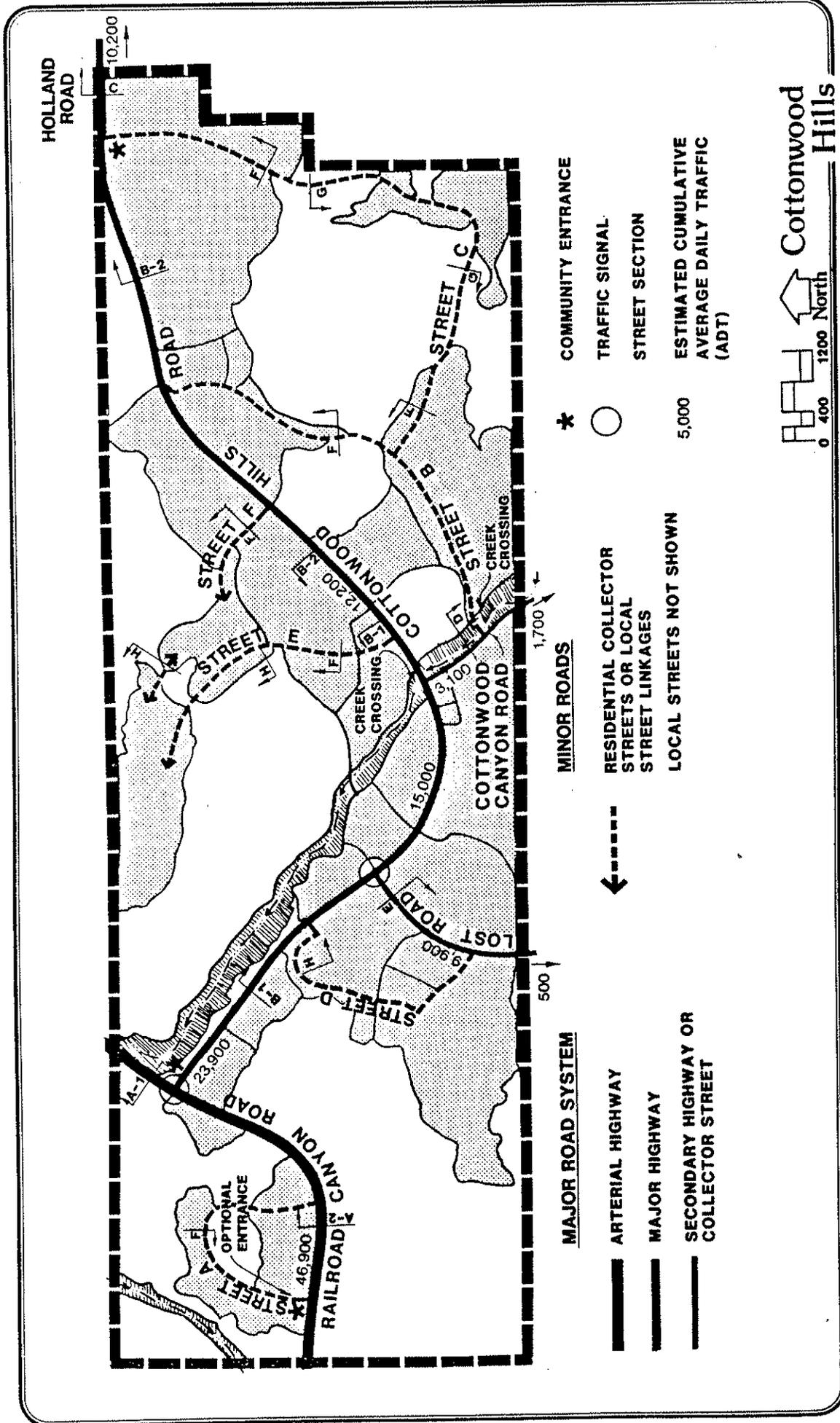
Using these generators, the cumulative future ADT for Railroad Canyon Road southwest of the Cottonwood Hills Road intersection is projected at 46,900. Near Newport Road, it is estimated at 28,200, and near the Interstate 15 interchange at 51,300.



Area-Wide Street Network 7.1

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3-14-89

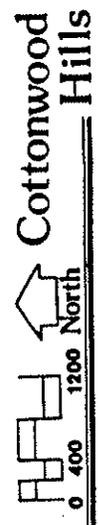


Project Design Consultants

3-14-89



Community Street System 7.2



- MAJOR ROAD SYSTEM**
 - ARTERIAL HIGHWAY
 - MAJOR HIGHWAY
 - SECONDARY HIGHWAY OR COLLECTOR STREET
- MINOR ROADS**
 - RESIDENTIAL COLLECTOR STREETS OR LOCAL STREET LINKAGES
 - LOCAL STREETS NOT SHOWN
- COMMUNITY ENTRANCE
- TRAFFIC SIGNAL
- STREET SECTION
- ESTIMATED CUMULATIVE AVERAGE DAILY TRAFFIC (ADT)

Based on these traffic projections, a four-lane divided highway with a 110-foot right-of-way is proposed north of the Cottonwood Hills Road intersection, as shown in Figure 7.3 (Street Section A-1). This section would connect into the existing four-lane section in Canyon Lake. Southwest of the intersection to the community boundary, a six-lane divided highway with a 122-foot right-of-way is anticipated, as depicted in Figure 7.4 (Street Section A-2). A six-lane roadway with Class II bicycle lanes is expected for the portion of highway between Cottonwood Hills and Interstate 15, although the right-of-way width may vary due to site and design constraints.

Under the engineering design standards for an arterial highway, a new alignment for Railroad Canyon Road will be required. In the Cottonwood Hills plan area, the roadway is proposed for straightening to a single arc. Much of the proposed alignment is located south of the existing roadway, to facilitate use of the present road during construction; and to place the bulk of the developable area on the north side of the roadway, rather than strip development on both sides.

Railroad Canyon Road provides the primary means of access to Cottonwood Hills, either directly to projects adjacent to the highway or indirectly via Cottonwood Hills Road. The Cottonwood Hills Road/Railroad Canyon Road intersection is proposed for special design and signalization. It will function as the major community entrance to the specific plan area. Additional median breaks in Railroad Canyon Road for Street "A" shall be consistent with sight distances and the speed of the highway. The minimum spacing shall be 1,320 feet (1/4 mile).

Interchange improvements are anticipated at the Railroad Canyon Road/Interstate 15 interchange. The current diamond configuration with one-lane ramps will require signalization and two-lane off-ramps.

7.1b Major Road System

The major road system for Cottonwood Hills is represented in Figure 7.2. This system provides the backbone for a hierarchy of community streets, and ties into the area-wide street network. Included are Railroad Canyon Road (discussed in Section 7.1a), Cottonwood Hills Road, Holland Road, Lost Road, and Cottonwood Canyon Road, each described briefly below.

Cottonwood Hills Road is the primary east-west route through the specific plan area, running from Railroad Canyon Road to Holland Road. It roughly follows the alignment of the existing dirt roads on-site: the western portion of Cottonwood Canyon Road and the western portion of Holland Road. In conformance with the Riverside County General Plan, Cottonwood Hills Road is proposed as a major highway with a 100-foot right-of-way dedication. For the portion of roadway west of Street "E", full width improvements are proposed, including four travel lanes, a median, and bicycle lanes as shown in Street Section B-1, Figure 7.5. East of Street "E", projected cumulative traffic volumes will not warrant full improvements at build-out of the specific plan area. For this portion of roadway, two travel lanes, a turn lane, and bicycle lanes are proposed

as pictured in Street Section B-2, Figure 7.6. Temporary parkways occupy the areas between the proposed roadbed and the permanent parkways within the 100-foot right-of-way. If traffic volumes ever warrant an enlarged facility, a four-lane roadway with median may be constructed. A creek crossing is required at Cottonwood Creek.

Holland Road functions as an extension of Cottonwood Hills Road east of Street "C", tying into existing Holland Road east of the community. It provides secondary access to the Cottonwood Hills community. The proposed road alignment generally follows the present roadway along the section line. The road is currently unpaved within the specific plan area and partially paved within the rural residential area to the east. The Riverside County General Plan calls for a major highway with a 100-foot right-of-way for Holland Road. Street Section C, Figure 7.7, portrays the proposed street configuration based on the County-adopted Audie Murphy Ranch Specific Plan. The 100-foot right-of-way is augmented by a 30-foot hiking/riding trail easement on the north side of the road. As for the east portion of Cottonwood Hills Road, roadway improvements include two travel lanes and bike lanes with temporary parkways for future widening should traffic volumes warrant.

Cottonwood Canyon Road provides a linkage between Cottonwood Hills Road and the existing Cottonwood Canyon Road south of the community. The proposed alignment differs from the existing dirt road by remaining on the west side of Cottonwood Creek. This portion of Cottonwood Canyon Road was recently deleted as a secondary highway from the Riverside County General Plan. Accordingly, the road is proposed as a two-lane collector street within a 66-foot right-of-way, tapering to meet the existing roadbed at the community boundary. Street Section D, Figure 7.8, represents the proposed treatment of Cottonwood Canyon Road within the plan area. An expanded parkway is proposed to the east of the street to accommodate Cottonwood Creek and a hiking/riding trail corridor. A stop sign is anticipated at the Cottonwood Hills Road/Cottonwood Canyon Road intersection, with a future signal if traffic volumes warrant.

Lost Road functions as a north-south connection between Cottonwood Hills Road and existing Lost Road south of the specific plan area. The proposed alignment runs in an arc to the east of the present dirt road, but meets the existing road at the section line on the community boundary. Under the Riverside County General Plan, Lost Road is designated a secondary highway with an 88-foot right-of-way, ultimately connecting to Lemon Street north of the Bundy Canyon Road/Interstate 15 interchange. In conformance with the General Plan, the roadway is proposed as a secondary highway within an 88-foot right-of-way dedication within the specific plan area. Street Section E, Figure 7.9, pictures the Lost Road roadway. It is anticipated that the four-lane improvements would taper to a two-lane section south of Street "D", to meet the existing road. A signal will be required at the Cottonwood Hills Road/Lost Road intersection during community build-out.

Median breaks shall be located consistent with sight distances and the speed of the highway. The minimum access point spacing for minor streets intersecting major roads shall be as follows:

Cottonwood Hills Road — 660 feet (1/8 mile)
Lost Road — 330 feet (1/16 mile)
Cottonwood Canyon Road — 250 feet

Detached residential lots shall not receive direct access from major roads, but instead from residential streets.

The cumulative future ADT's for roads off-site are depicted in Figure 7.2. Both Cottonwood Canyon Road and Lost Road are expected to receive little community-generated traffic off-site. These roadways serve rural residential areas, and the requirement and feasibility for substantial off-site upgrading as shown in Figures 7.8 and 7.9 appears low. When built-out, Cottonwood Hills is projected to add approximately 6,960 ADT to Holland Road. Taken together with traffic from the planned Audie Murphy Ranch project and general expected growth in the region, it is anticipated that improvement of Holland Road eastward to Murrietta Road will be required. These off-site improvements could be staged and/or specially designed for consistency with the rural residential character of nearby properties.

7.1c Minor Roads

A hierarchy of residential collector streets and local streets is proposed to feed into the major road system. Figure 7.2 shows the locations of proposed collector streets and other local street linkages serving the community.

Street Section F, Figure 7.10, illustrates the typical residential collector street with two travel lanes in a 66-foot right-of-way. For the special situation of a roadway in the estate residential area, a rural collector is depicted in Street Section G, Figure 7.11. This section would apply to Street "C" between Planning Units 30 and 28/33, where the road serves estate lot (SF1) development. The following guidelines apply to residential collector streets:

- o Detached residential lots are permitted to front directly on residential collector streets. However, it is preferable not to front detached lots across the collector street from elementary school and park sites. Where school district boundaries run along collector streets, it is preferable to face lots inward into projects rather than face each other across a street.
- o On-street parking should be prohibited across the collector street from elementary school and park sites, while parking immediately adjacent to these sites should be permitted.
- o Widening of the roadway may be necessary where the residential collector street intersects a major road, to accommodate turn lanes.

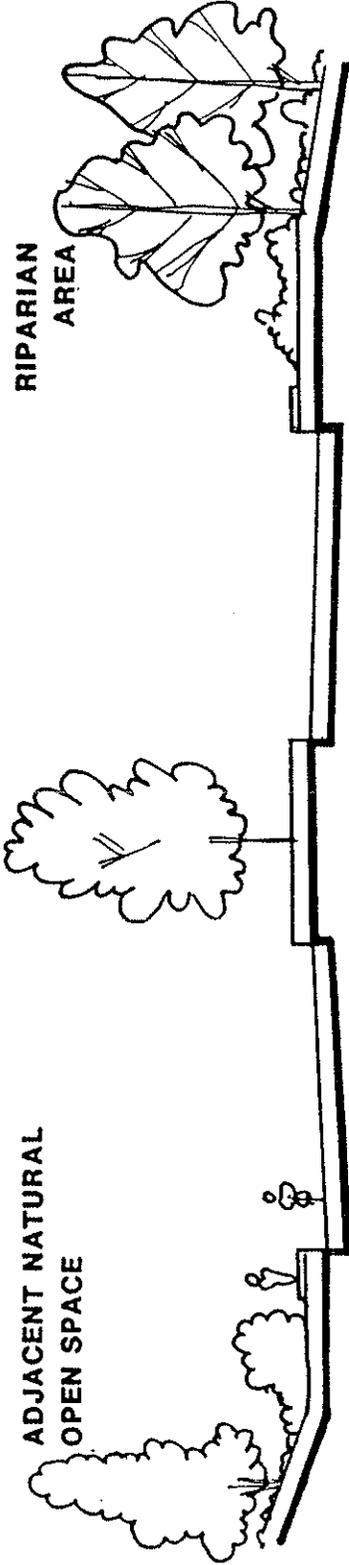
- o The sidewalk design guidelines outlined in Section 7.6a shall apply.

Street sections for public local streets, including local street linkages, are shown in Figure 7.12. These would largely apply to single-family detached residential projects. In general, subdivision street design should conform to the Subdivision Ordinance of the City of Lake Elsinore. In addition, the following design guidelines are applicable, which in event of conflict, supersede the Subdivision Ordinance:

- o A curvilinear residential street pattern shall be utilized, as illustrated in Figure 7.13.
- o For Street "D", the design should discourage thru-movement through Planning Units 6 and 9 from Cottonwood Hills Road. The residential collector street guidelines outlined above shall apply.
- o Under normal circumstances, the maximum cul de sac length shall be 600 feet with a 36 foot/56' foot street section (Figure 7.12). Where developable acreage cannot otherwise be utilized, as in Planning Unit 9, a maximum cul de sac length of 1,000 feet may be used with a 40 foot/60 foot street section (Figure 7.12). However, fire management concerns should be addressed; for example, insuring fire equipment can reach structures by an alternate means, such as utility roads or open space pathways; or use of mid-cul de sac turn-a-rounds of an adequate radius.
- o The following access points requirements apply to single-family detached subdivisions (SF2 and SF3):
 - o 0 thru 200 dwelling units -- minimum 2 access points
 - o 201 thru 300 dwelling units -- minimum 3 access points
 - o 301 thru 400 dwelling units -- minimum 4 access points
- o The following intersection spacing guidelines shall apply:
 - Collector street-to-local street centerline offset: 200 feet minimum.
 - Local street-to-local street centerline offset: 250 feet minimum for SF1 and SF2 planning units; and 200 feet minimum for SF3 planning units.
- o The sidewalk design guidelines outlined in Section 7.6a shall apply.

In multi-family attached residential projects (MF1 and MF2), either public or private streets may be utilized. Private streets may be used with the approval of the City as specified in the Subdivision Ordinance. Roll curbs are permitted for private streets. Driveways for single-family detached dwelling units shall be concrete.

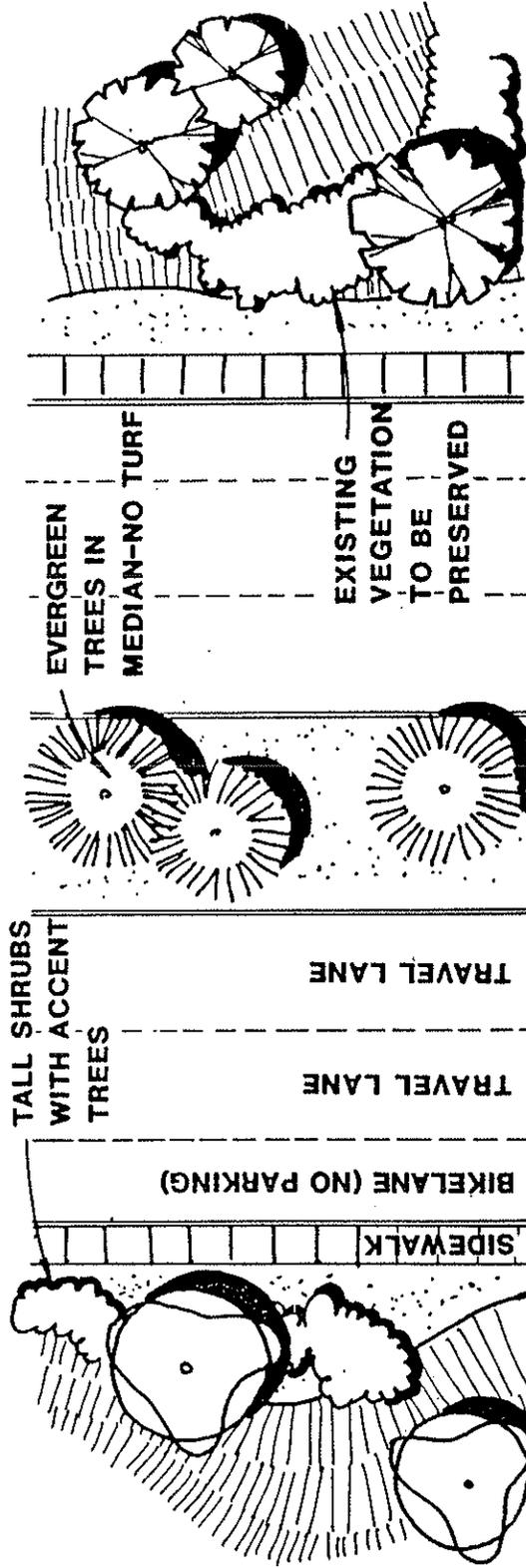
ADJACENT NATURAL
OPEN SPACE



RIPARIAN
AREA

7'	5'	8'	12'	12'	12'	12'	8'	5'	7'	
SW		MEDIAN					SW			
12'		22'					12'			
PKWAY		86' ROADBED					PKWAY			
110' R/W										

ARTERIAL HIGHWAY



RAILROAD CANYON ROAD / NORTHEAST

Cottonwood
Hills

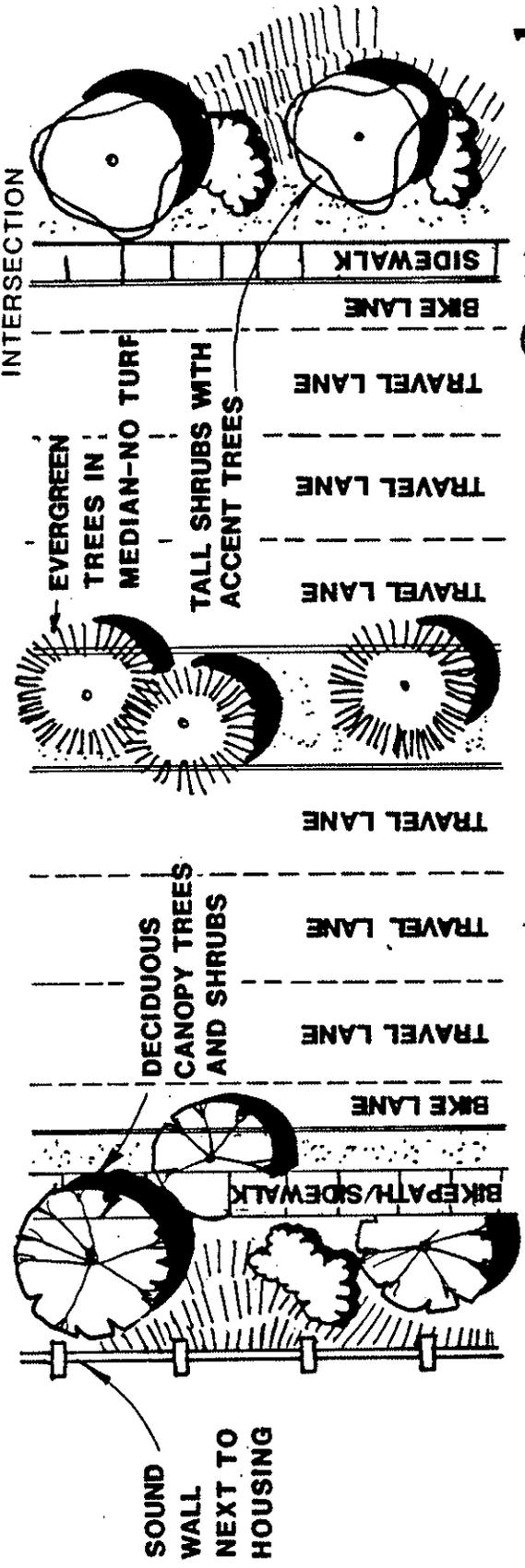
Street Section A-1 7.3

ADJACENT DEVELOPMENT ABOVE OR BELOW ROADBED

ADJACENT NATURAL OPEN SPACE

EXPANDED PARKWAY	SW	5'	6'	12'	12'	12'	12'	12'	12'	6'	5'	7'
		12'	98' ROADBED									12'
122' R/W *												

ARTERIAL HIGHWAY
* MAY BE WIDER AT COTTONWOOD HILLS RD. INTERSECTION

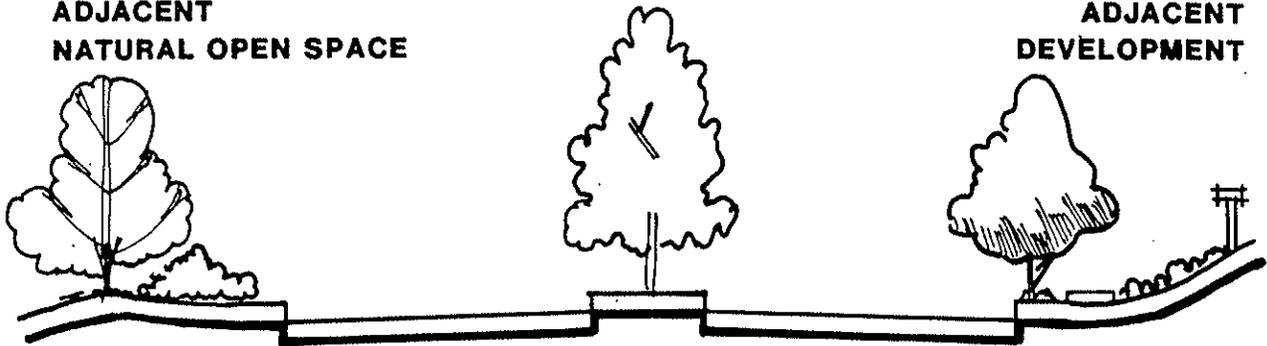


RAILROAD CANYON ROAD/SOUTHWEST Cottonwood Hills

Street Section A-2 7.4

ADJACENT
NATURAL OPEN SPACE

ADJACENT
DEVELOPMENT

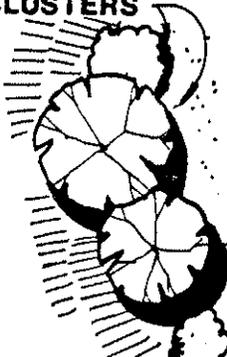


PATHWAY WITHIN OPEN SPACE IN SOME LOCATIONS	NO SIDE WALK	8'	11'	12'	MEDIAN		12'	11'	8'	5'	5'	2'	EXPANDED PARKWAY
	12'	76' ROADBED									12'		
	PKWAY	100' R/W										PKWAY	

MAJOR HIGHWAY

RIPARIAN
NATURAL
OPEN
SPACE

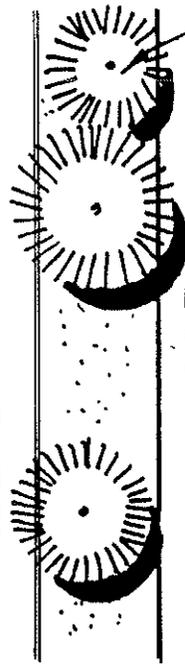
NATURALIZED
SHRUBS AND
ACCENT TREE
CLUSTERS



BICYCLE LANE (NO PARKING)

TRAVEL LANE

TRAVEL LANE

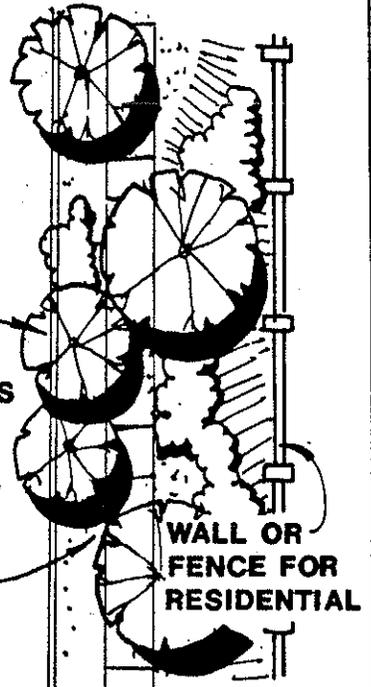


MEDIAN
TREES-
NO TURF

PARKWAY
DECIDUOUS
CANOPY TREES
AND SHRUBS
ADJACENT TO
DEVELOPMENT

SIDEWALK

WALL OR
FENCE FOR
RESIDENTIAL



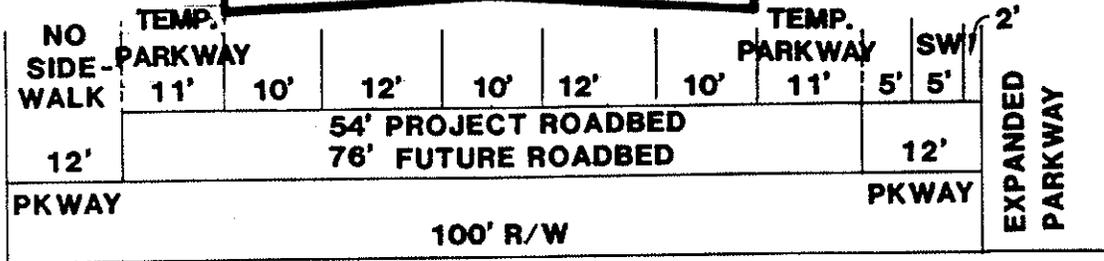
COTTONWOOD HILLS ROAD/WEST

Cottonwood
Hills

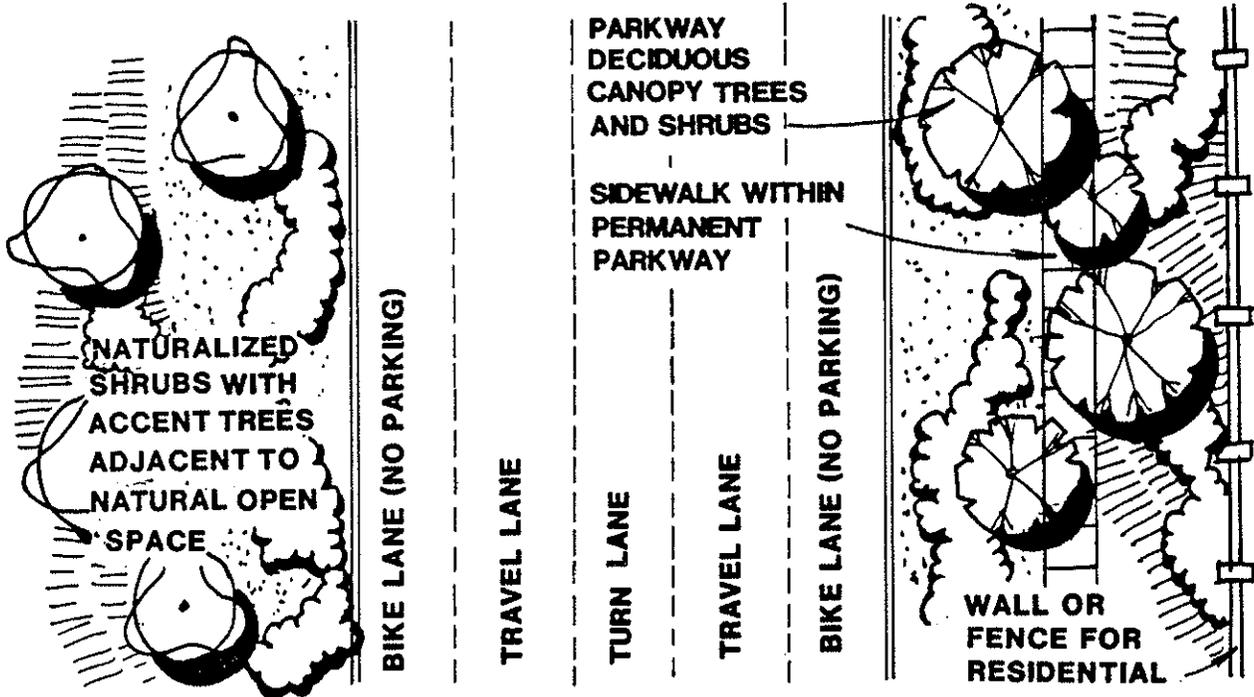
ADJACENT
NATURAL OPEN SPACE

ADJACENT
DEVELOPMENT

NO PERMANENT
LANDSCAPING
IN TEMPORARY
PARKWAY

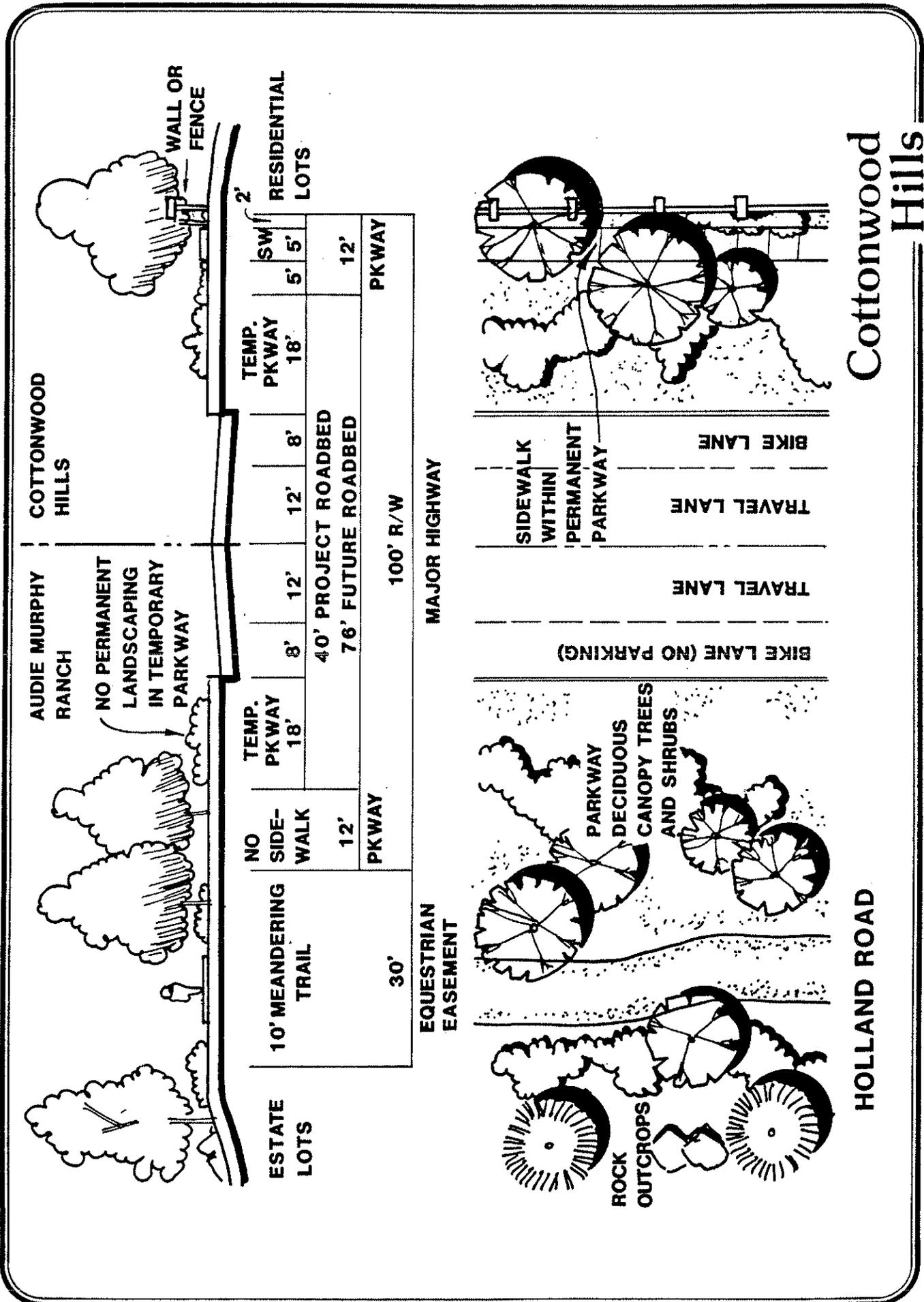


MAJOR HIGHWAY



COTTONWOOD HILLS ROAD / EAST

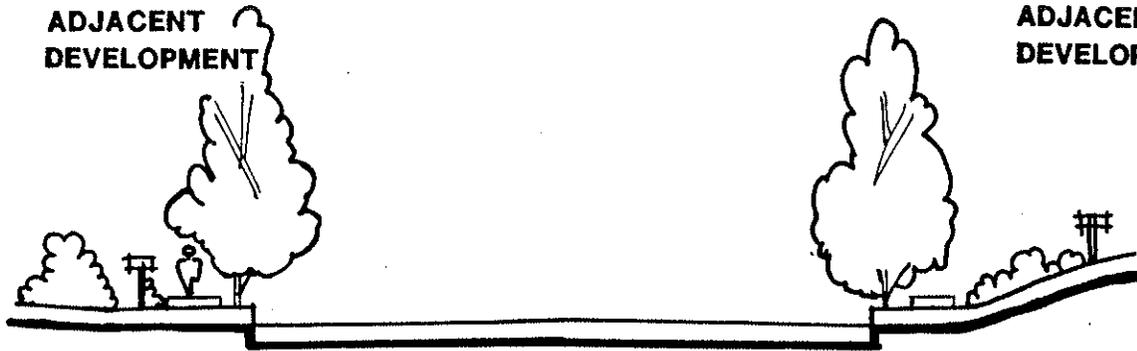
Cottonwood
Hills



Street Section C 7.7

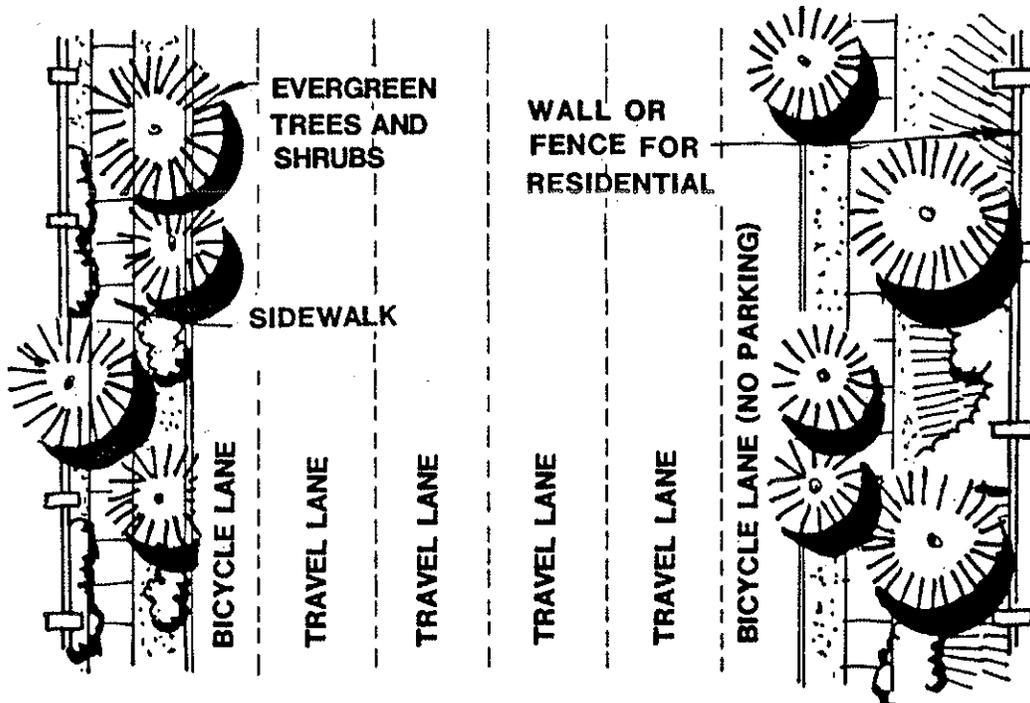
ADJACENT DEVELOPMENT

ADJACENT DEVELOPMENT



2'	SW	5'	5'	8'	12'	12'	12'	12'	8'	5'	5'	SW	2'
12'		64' ROADBED TAPERED TO 44' ROADBED AT BOUNDARY								12'		EXPANDED PARKWAY	
PKWAY		88' R/W								PKWAY			

SECONDARY HIGHWAY



LOST ROAD

Cottonwood Hills

Street Section E 7.9

ADJACENT NATURAL
OPEN SPACE

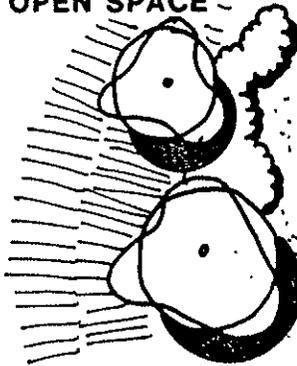
ADJACENT
DEVELOPMENT



6'	SW 5'	10'	12'	12'	10'	6'	SW 5'
11'		44' ROADBED				11'	
PKWAY		66' R/W				PKWAY	

RESIDENTIAL COLLECTOR STREET

NATURALIZED
SHRUBS
WITH ACCENT
TREES
ADJACENT
TO NATURAL
OPEN SPACE



SIDEWALK

SHOULDER-NO PARKING

TRAVEL LANE

TRAVEL LANE

SHOULDER-PARKING PERMITTED

DECIDUOUS
CANOPY
TREES



SIDEWALK

PUBLIC FACILITY
OR PRIVATE
YARD
LANDSCAPING

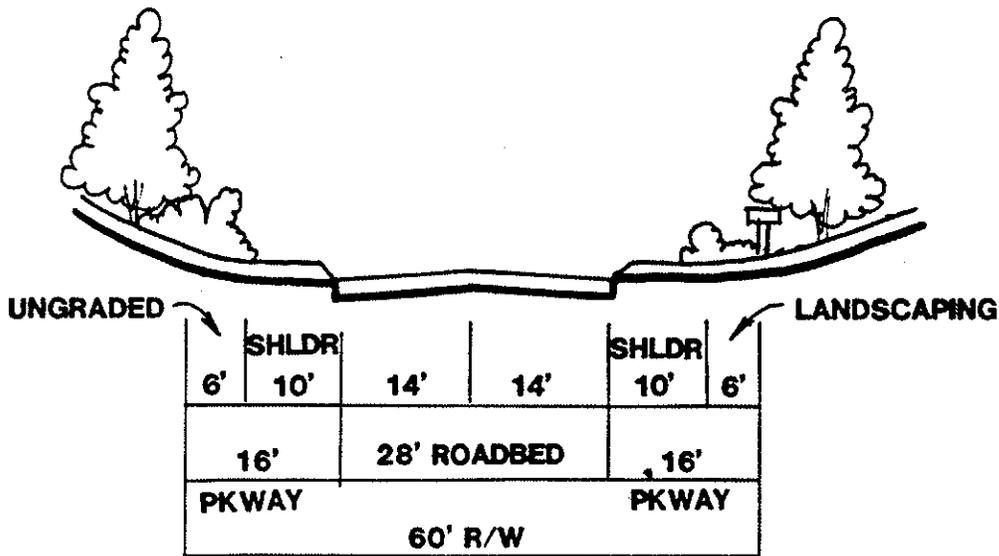
R/W LINE

TYPICAL RESIDENTIAL COLLECTOR STREET

Cottonwood
Hills

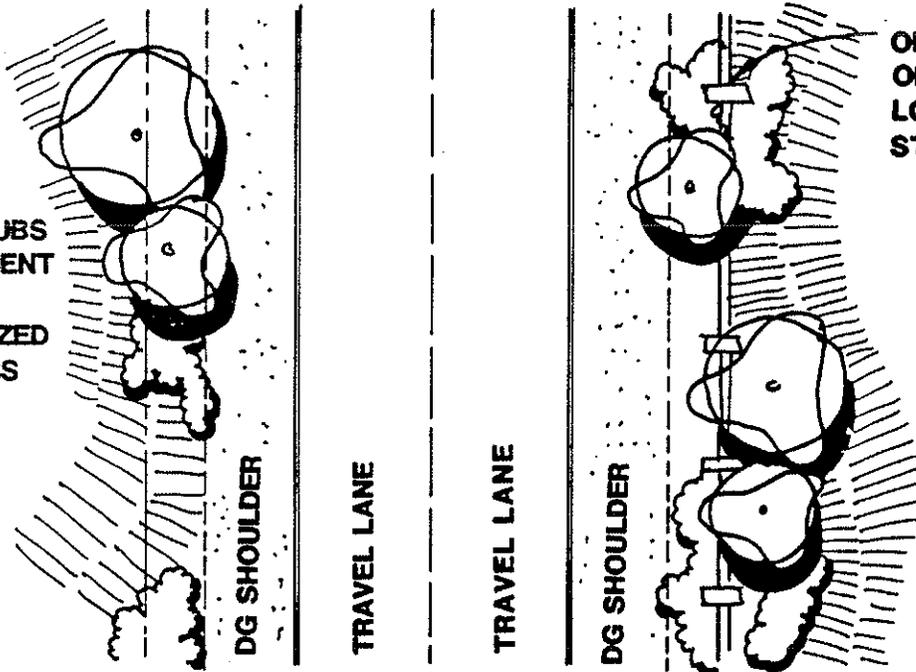
ADJACENT NATURAL
OPEN SPACE

ADJACENT
ESTATE LOTS



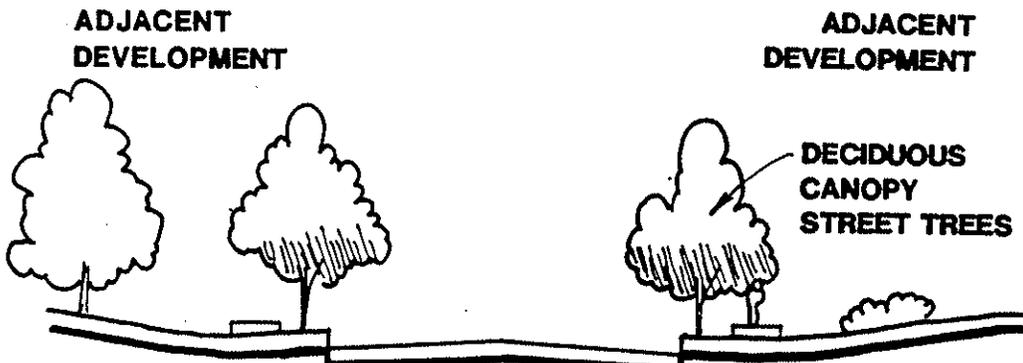
RURAL COLLECTOR STREET

TALL SHRUBS
WITH ACCENT
TREES-
NATURALIZED
PLANTINGS



STREET "C"-ESTATE PORTION

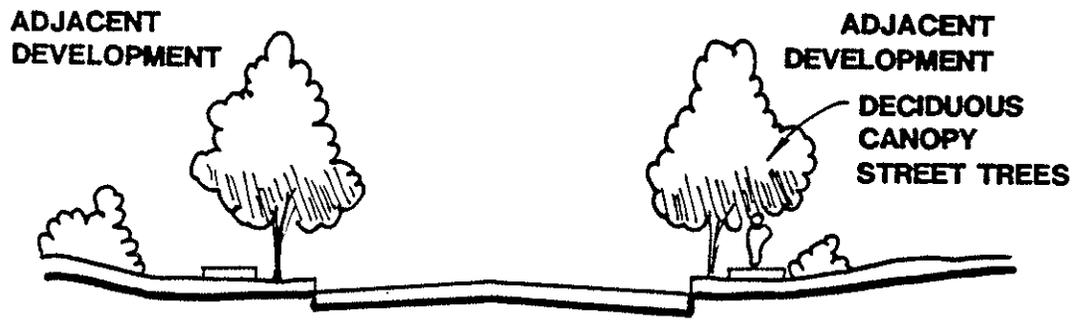
Cottonwood
Hills



SW	5'	5'	PKG	7'	11'	11'	PKG	7'	5'	5'	SW
10'		36' ROADBED						10'			
PKWAY		56' R/W						PKWAY			

CUL DE SAC STREET

SECTION I



SW	5'	5'	PKG	8'	12'	12'	PKG	8'	5'	5'	SW
10'		40' ROADBED						10'			
PKWAY		60' R/W						PKWAY			

THRU LOCAL STREET

SECTION H

TYPICAL LOCAL STREETS

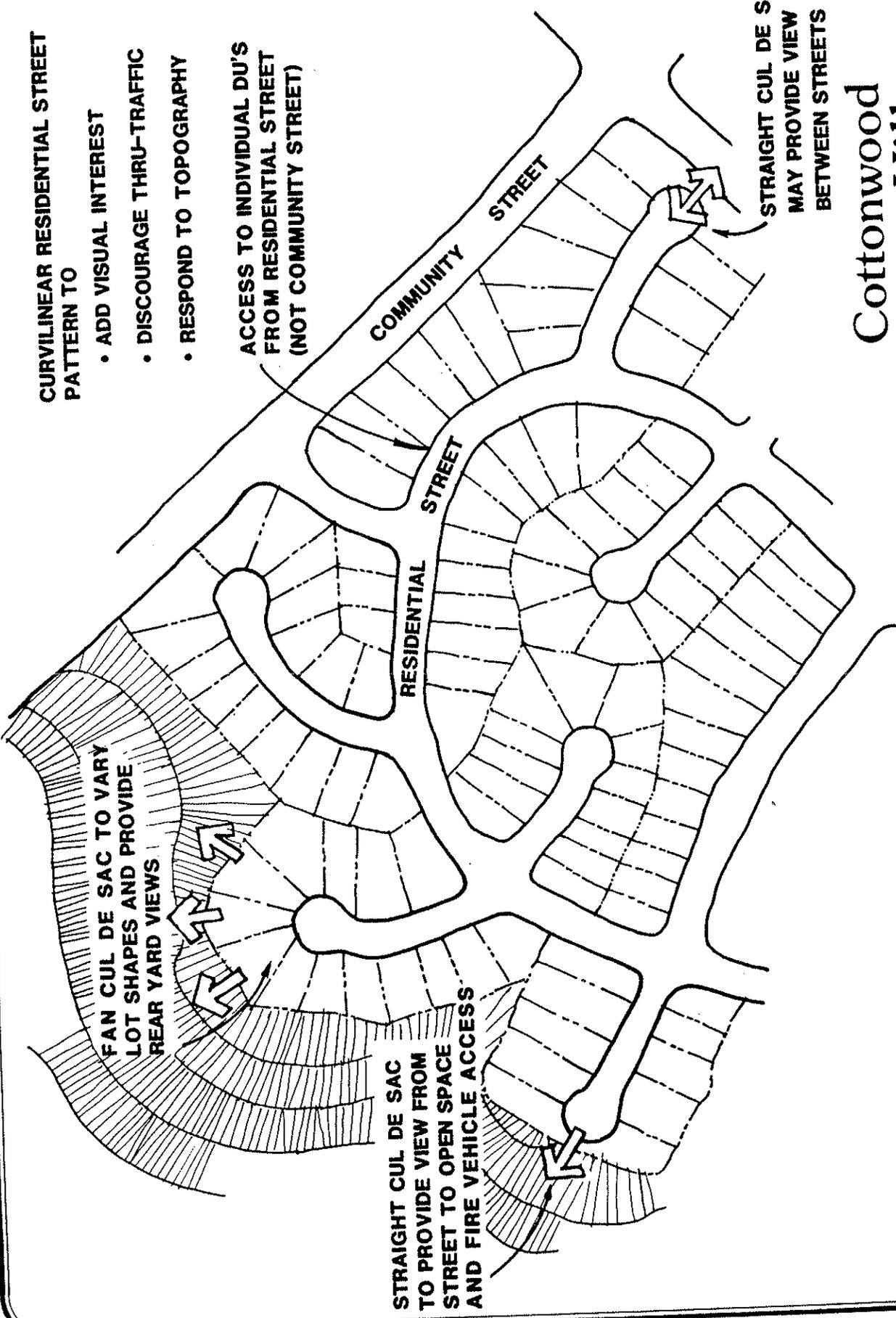
Cottonwood
Hills

**CURVILINEAR RESIDENTIAL STREET
PATTERN TO**

- ADD VISUAL INTEREST
- DISCOURAGE THRU-TRAFFIC
- RESPOND TO TOPOGRAPHY

**ACCESS TO INDIVIDUAL DU'S
FROM RESIDENTIAL STREET
(NOT COMMUNITY STREET)**

**STRAIGHT CUL DE SAC
MAY PROVIDE VIEW
BETWEEN STREETS**



**FAN CUL DE SAC TO VARY
LOT SHAPES AND PROVIDE
REAR YARD VIEWS**

**STRAIGHT CUL DE SAC
TO PROVIDE VIEW FROM
STREET TO OPEN SPACE
AND FIRE VEHICLE ACCESS**

**Cottonwood
Hills**

Residential Street Pattern 7.13

7.2 Streetscape Design

This section addresses the design of the Cottonwood Hills streetscape, focusing on the major road system with community-level and area-wide traffic. Streetscapes within residential planning units are addressed in Section 3.2. The design objectives and concept for streetscapes reflect the goal of creating an attractive, aesthetically coherent community, while the design guidelines are formulated to provide guidance for the detailed design of streets and parkways.

7.2a Streetscape Design Objectives

The design of streetscapes along the major road system within Cottonwood Hills should be based on the following objectives:

- o Contribute to the overall identity of the community by providing a visually continuous and cohesive street design treatment for all major roads.
- o Recognize that roadways with parallel pathways provide a linked network of open space parkways within the community.
- o Encourage bicycle and pedestrian travel within street parkways by providing safe, efficient pathways with visual attraction.
- o Complement adjacent development projects and facilities both functionally and aesthetically, providing visual and noise buffers as required.
- o Provide naturalized transitions between natural open space and major roadways, using appropriate grading and landscaping.
- o "Soften" the effect of hardscape, slopes, and walls through landscaping and grading techniques.
- o Design streetscape improvements to be cost-effective to install and practical to maintain.

7.2b Streetscape Concept

In general, a parkway effect is desired along major roadways combining natural open space vistas and attractive developed areas. The immediate street right-of-way can, in effect, be visually expanded through compatible design treatment of adjacent open space and projects. The design guidelines should be applied to insure aesthetic continuity for streetscapes within the community.

The components of the streetscape concept include the following:

- o Visual continuity community-wide in median landscaping, street furniture selection, street lighting design, and community

entrance design, with particular emphasis on parkways within street rights-of-way.

- o Incorporation of alternative circulation modes in streetscape design, including pedestrian, bicycle, and transit facilities.
- o Sensitive treatment of natural open spaces abutting street parkways, including naturalized slope transitions as required.
- o Compatible design of expanded parkways within development projects adjacent to streets, including landscaping, fencing, grading, and project entries.

A critical component of the streetscape concept is the repetitive use of selected design elements under the same or similar conditions. These elements include:

- o Fencing treatments, including a solid wall treatment such as masonry where noise and privacy control are required, and an open fencing treatment such as tubular steel to capture the view opportunities.
- o Street lighting fixtures.
- o Sidewalk treatments, adjusted for location and function as discussed in Section 7.6a.
- o Transit stops.
- o Landscaping treatments, including a riparian-oriented treatment, an open-valley treatment, and a Railroad Canyon Road treatment.

In addition, signage should contribute to the parkway effect. All community entrance monumentation and landscaping should utilize a similar design motif in style, color, and texture. Project (planning unit) entries should be designed to be aesthetically compatible with the community entrances. The signage program for marketing should complement the permanent signage.

All streetscape design should be harmonious with the architectural character of the community. This includes compatibility in architectural style and detailing, materials, colors, and textures with buildings constructed in the plan area. Motifs and materials suggested by the nature of the site, such as rock outcrops and tree clumps and hedgerows, may also be incorporated into streetscape design.

7.2c Streetscape Design Guidelines

Tables 7.1a, 7.1b, and 7.1c outline guidelines for the design of major road streetscapes for the overall community, adjacent natural open space, and adjacent development projects, respectively.

The street sections in Figures 7.3 through 7.9 illustrate the design guidelines for major road streetscapes.

For streetscape design within planning units, see the Design Guidelines of the appropriate land uses.

Section 9.2a calls for development and implementation of a community signage program including both permanent and temporary signage. This program shall comply with the standards prescribed in Section 8.9, Community Signage. All streets shall be identified by street name signs, indicating the street name and location within the City of Lake Elsinore.

7.2d Traffic Noise Mitigation

Development in Cottonwood Hills should conform to the City's noise control ordinance (Chapter 17.78 of the Zoning Code), as well as State of California noise standards for multi-family residential. Under these regulations, specific noise studies must be performed prior to issuance of building permits for attached units located in areas where exterior noise levels reach 60 dBA, to insure interior noise levels do not exceed 45 dBA. Single-family residential may be located in areas with a CNEL of up to 65 dBA.

A general noise analysis prepared for the community indicates that the portions of residential projects next to Railroad Canyon Road and Cottonwood Hills Road/West and possibly Cottonwood Hills Road/East will exceed acceptable noise levels without mitigation. Accordingly, at the Site Plan Review stage a more detailed noise analysis should be performed to determine the adequacy of traffic noise mitigation. The following mitigation measures may be utilized singly or in combination:

- o Site the development above or below street level.
- o Provide a berm, a solid wall, or a combination berm and wall along the tops of slopes.
- o Where development lies below the street level, build only one-story structures next to the street or structurally insulate the upper floors of two- and three-story structures adjacent to the street.
- o To attenuate noise, designate a buffer zone between the street and any structures, which may include the parkway and any expanded parkway.
- o Use smaller or fewer windows, or thicker wall sheathing, to reduce interior noise levels.

Tentative maps should be conditioned to mitigate noise at the building permit stage if architectural/structural mitigation measures are utilized.

Table 7.1a

STREETSCAPE DESIGN GUIDELINES / OVERALL COMMUNITY

Landscaping

- o For the street medians in Railroad Canyon Road, utilize a single selected plant palette for the length of improvements from Interstate 15 to Canyon Lake.
- o For the street medians and the parkway treatment for Cottonwood Hills Road between Railroad Canyon Road and Street "E", and for Cottonwood Canyon Road, employ a palette of trees, shrubs, and groundcovers which reinforces the natural palette of the Cottonwood Creek riparian area and which carries the character of the creek area across the roadways.
- o For the remainder of community roads, including Cottonwood Hills Road east of Street "E", Lost Road, and Holland Road, develop a landscaped parkway theme emphasizing non-regimented clusters of vertically-oriented trees responding to and structuring open valley views.
- o Select median plant materials for their low maintenance and water requirements, as well as year-round form, color, and texture.
- o Coordinate median design with landscaping of parkways and expanded parkways.
- o Plant temporary parkways with shrubs and groundcovers coordinated with adjacent parkway landscaping, but avoid utilizing trees.
- o For a discussion of the community landscape concept and City Landscape Guidelines, see Section 6.2a.

Street Lighting

- o Use a repeated street light fixture throughout the community, complementing the other street furniture.
- o Increase light intensity at areas of high vehicular and pedestrian activity, such as important street intersections and school/park sites.
- o Utilize as low intensity lighting as possible next to natural open space areas, without sacrificing safety and utility.
- o Gradually modulate lighting between the high and low intensity areas.
- o Select lighting fixtures which minimize impacts to astronomy observatories to the extent feasible.

Street Furniture

- o Coordinate benches, transit stops, lighting fixtures, and other street furniture to create an identifiable community appearance together with the landscaping.
- o Repeat materials, colors, and motifs or styles to produce a sense of visual continuity and architectural cohesion.

Table 7.1a (continued)

STREETSCAPE DESIGN GUIDELINES / OVERALL COMMUNITY

Community Entrances

- o Create identifiable community entrances at the locations indicated in Figure 7.2.
- o Design the entrances as part of an aesthetic transition for those entering and leaving the community.
- o Utilize entrance monuments, accent plantings, and grading to produce distinctive entrances which are compatible with the overall community streetscape design and architectural character of the community.
- o Consider sight distances in entrance design.
- o Emphasize the entrances through illumination.

Bicycle Lanes

- o For the major road system, provide striped bicycle lanes in the roadway next to the curb, with no parking permitted.

Utilities

- o Underground all utilities lines.
- o Screen utility vaults as described in Section 5.4.

Table 7.1b

STREETSCAPE DESIGN GUIDELINES / ADJACENT NATURAL OPEN SPACE

Landscaping

- o For the parkway within the street right-of-way and adjacent relatively flat areas, utilize clusters of shrubs and accent trees, supplemented by groundcover where needed for erosion control or fire management.
- o For manufactured slopes abutting the right-of-way, use planted groundcover or hydroseed mix for erosion control, supplemented by clusters of trees and shrubs. See Section 6.2c and Figure 6.3.
- o Select plant materials of native or naturalized species which are hardy, require little or no irrigation once established, need little maintenance, and are appropriate to local soils and microclimatic conditions. Where appropriate for fire management purposes, utilize plantings with potentially high moisture content and low fuel volume, and provide irrigation as necessary.
- o Recognize that the landscaping provides a visual and botanical transition from the roadway to the natural open space.
- o Consider vistas from the roadway in placing trees and tall shrubs, so views are maintained and "framed" or punctuated.
- o At touchdown points where roadway grading daylight natural open space, utilize plantings to curb casual access by pedestrians and off-road vehicles into natural open space.
- o For the Upper Cottonwood Creek parkway (Figure 7.8) and Lower Cottonwood Creek parkway (Figure 7.5), see Section 6.2b concerning landscaping.

Grading

- o For grading next to the creek areas, see Section 6.4.
- o For cut slopes next to major roadways, utilize the grading techniques illustrated in Figure 6.6 to introduce variation.
- o To the extent practical, daylight grade roadways next to natural open spaces, to reduce grading into natural areas designated for preservation, and to create pockets for clusters of trees and shrubs.

Pedestrian Facilities

- o To retain a natural appearance, place sidewalks next to natural open space areas only where necessary to provide safe circulation to public schools and parks. If a sidewalk is needed, place it directly abutting the street curb to minimize intrusion into the open space, as shown in Figure 7.10.
- o Provide pathways where possible within the Cottonwood Creek corridor rather than immediately adjacent to the roadway, as pictured in Figures 7.5 and 7.8.

Table 7.1c

STREETSCAPE DESIGN GUIDELINES / ADJACENT DEVELOPMENT PROJECTS

Landscaping (See also Table 7.1a)

- o For parkways within street rights-of-way and adjacent relatively flat areas, utilize deciduous canopy trees to provide summer shade and winter sun for pedestrians. Supplement trees with shrubs and groundcovers, or with turf in selected locations. Place plantings informally, singly and in clusters.
- o For the expanded parkway between the street right-of-way and a project wall, screen plantings, or parking area—employ evergreen trees, shrubs, and groundcovers, supplemented by parkway plant materials.
- o Recognize the expanded parkway as a transition between the parkway and individual development projects through plant selection, placement, and function.
- o Select plant materials appropriate to local soils and microclimatic conditions, with design qualities (color, texture, massing) contributing to the overall streetscape character, and with moderate maintenance and irrigation requirements.
- o Utilize plantings to frame or mask views from the roadway, as appropriate.

Grading

- o For expanded parkways next to commercial and institutional projects, preferably create flat areas or limit slope heights to provide site visibility from roadways.
- o For expanded parkways adjacent to residential projects, utilize an elevation differential for noise and privacy control where possible, such as shown in Figure 6.7.
- o For expanded parkways abutting parks and schools, preferably grade flat areas or slopes downward from the street into the site, to optimize site access and visibility for security reasons from the roadway. While this guideline is especially applicable to residential collector or local streets next to school/park sites, it may be considered for major roads as well.
- o Vary long expanses of slopes through grading, utilizing contouring and height changes.

Fencing

- o Employ fencing to separate residential projects from major road parkways or expanded parkways. Utilize a solid wall treatment such as masonry where noise and privacy control are required, and an open fencing treatment such as tubular steel to capture view opportunities. Design and utilize these treatments as a repeated feature throughout the community.
- o Visually vary lengthy walls and fences with accent plantings and/or subtle color or material changes.

Table 7.1c (continued)

STREETSCAPE DESIGN GUIDELINES / ADJACENT DEVELOPMENT PROJECTS

- o Select fencing materials to coordinate with the overall parkway treatment in terms of color, texture, and style.

Project (Planning Unit) Entries

- o Provide identifiable project entries along major roads into commercial, institutional, and residential projects.
- o Utilize entrance signage on property walls or stand alone monument walls, emphasized by grading, accent plantings, and night illumination.
- o Coordinate project entry design with the community entrances and overall community streetscape design.
- o Apply sight distance considerations to project entry design.

Pedestrian and Transit Facilities

- o Place sidewalks next to all public and private development projects. Adjacent to residential projects, provide a landscaped strip between the curb and sidewalk, as shown in Figures 7.4, 7.5, 7.6, 7.7, 7.8, and 7.9.
- o Integrate transit stop design into the pedestrian walks at key street intersections, and incorporate attractive seating, shading devices, signing, and lighting. See Section 7.4.
- o See Section 7.6a for walkway widths.

7.3 Auto Parking

Adequate parking should be provided for each residential, commercial, institutional, and public use project as it is developed. Because of the use of shoulders for emergency parking, stopped transit vehicles, and bicycle lanes, no street parking is permitted along the major road system. For the residential collector streets, parking is permitted next to development, but not adjacent to natural open space in order to protect views. Parking should be prohibited across the collector or local street from school and park sites.

Standards for off-street parking are incorporated into the development standards for each land use type. Utilization of shared parking should be considered in the neighborhood park - elementary school complexes and the community park - school complex. Any shared parking program should consider the peak use periods for each land use/activity to determine overall peak parking needs for the complex. Use of a mix of compact and standard parking spaces is encouraged, provided City standards and mix ratios are met.

Parking areas should be integrated into the overall design of the projects they serve. Flow patterns for motorists, bicyclists, and pedestrians should be considered. The visual impacts of parking lots should be minimized through careful design. Examples include use of small lots (as opposed to large, unbroken lot expanses); perimeter screening, such as berms, walls, elevation differentials, and landscaping; and interior lot islands and landscaping. All parking areas should be landscaped. Canopy trees are preferred, to provide shade and reduce emissions. A planting theme for parking areas should be utilized throughout each project (planning unit).

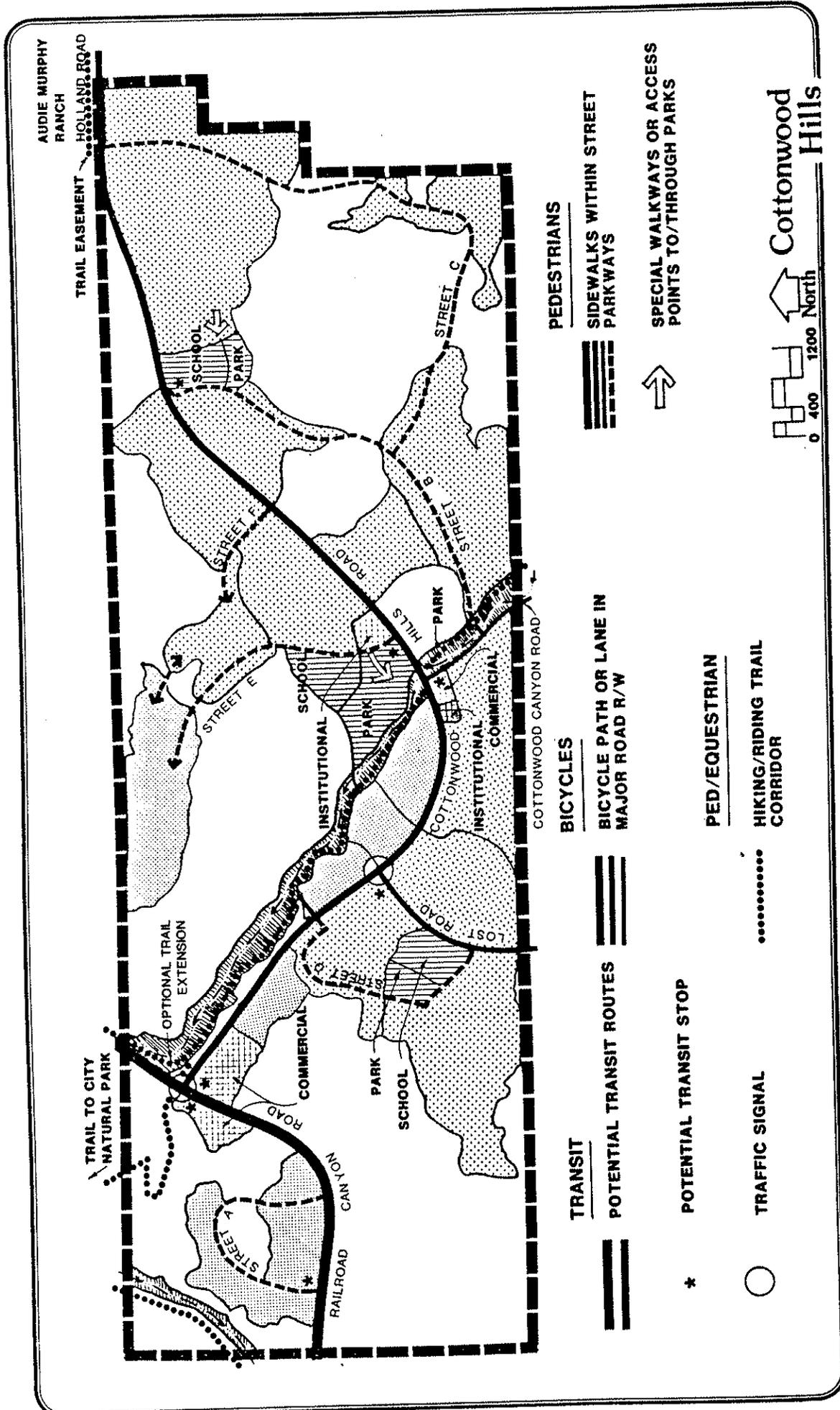
7.4 Public Transportation

Transit includes a number of travel alternatives such as intercity and intracity bus service, and para-transit and dial-a-ride. The Railroad Canyon Road - Newport Road corridor is a likely future transit corridor, with the potential to link Cottonwood Hills to central Lake Elsinore and Interstate 15. Local transit is also a possibility along Cottonwood Hills Road within the specific plan area. Future bus service to the community could be provided by the Lake Elsinore Transit System (LETS) and/or the Riverside Transit Authority (RTA). The nature and timing of this service will depend of the demand created by the community and other nearby projects.

The Cottonwood Hills major road system can accommodate buses, vanpools, and other transit vehicles within the proposed street roadways. Road shoulders are adequate for transit vehicles sitting at transit stops, since no parking is permitted.

Transit stops with benches and shelters are logically located along bus routes at signalized street sections. As shown in Figure 7.14, this could include the Railroad Canyon Road/Cottonwood Hills Road intersection and the Cottonwood Hills Road/Lost Road intersection. Other potential stops include the Cottonwood Hills Road/Cottonwood Canyon Road intersection, the Cottonwood Hills Road/Street "E" intersection; the Cottonwood Hills Road/Street "B" intersection; and the westernmost Street "A" intersection with Railroad Canyon Road.

During the Design Review stage, detailed site plans for commercial areas (Planning Units 3, 4, and 16) should be reviewed by LETS for bus stop accommodation, if any. Any transit stops at the commercial centers should be coordinated to provide access to nearby hiking trail corridors as described in Section 7.6. Transit stops should provide shade, such as through canopy trees or shelters.



Non-Auto Circulation 7.14

Project Design Consultants

3-14-89

7-29



7.5 Bicycle Circulation

A community bikeway system for Cottonwood Hills is depicted in Figure 7.14. This system includes the following:

- o Striped and signed bicycle lanes (Class II Bikeways) within the rights-of-way of Railroad Canyon Road, to serve area-wide bicycling needs. This is consistent with the City and County General Plans.
- o Marked bicycle lanes within the major road system to predominantly serve community needs, including Cottonwood Hills Road, Cottonwood Canyon Road, Lost Road, and Holland Road.
- o Bicycle crossings at signalized intersections and signed intersections.
- o Informal bicycle movement along public residential collector and local streets and private project streets.

Parking for bicycles shall be provided in activity centers, as part of project development. This includes schools, parks, and commercial uses. Bicycle parking may also be desirable for institutional uses and MF2 attached residential projects, depending on the projected users. Bicycle parking needs should be addressed during Design Review.

7.6 Pedestrian/Equestrian Circulation

Figure 7.14 illustrates the pedestrian circulation system for the Cottonwood Hills community. This network of pathways links the various residential projects and community facilities such as commercial centers, parks, and schools. In addition, recreational trails run through scenic park and open space areas.

7.6a Pedestrian System

The pedestrian path network for Cottonwood Hills incorporates the following elements:

- o Sidewalks within the parkways of the major road system to serve largely community needs (Railroad Canyon Road, Cottonwood Hills Road, Cottonwood Canyon Road, Lost Road, Holland Road).
- o Special walkways linking residential developments to nearby public parks, which may be designed to accommodate bicycles as well.
- o Sidewalks along public residential collector streets, where indicated in Figure 7.10, to serve local pedestrian needs.
- o Sidewalks along public local streets, as shown in Figure 7.12.
- o Path crossings at signalized intersections or signed intersections in the form of delineated crosswalks, sometimes in conjunction with transit stops or project entries.
- o Sidewalks and pathways within projects and along private streets.

The following minimum widths should be utilized for public pathways:

- o Eight (8) feet for special walkways within easements providing park access; ten (10) feet if pathway accommodates both foot and bicycle travel.
- o Eight (8) feet contiguous to the street curb for sidewalks within public street rights-of-way fronting on commercial centers and institutional uses.
- o Six (6) feet for sidewalks within public street rights-of-way along schools and parks; and abutting driveway access points for schools and parks.
- o Five (5) feet for the remainder of sidewalks within public street rights-of-way.

All sidewalks should be concrete and/or pavers to accommodate intensive foot travel. Sidewalks next to non-commercial development should be setback from street curbs a minimum of five (5) feet to accommodate street trees. Sidewalks adjacent to open space areas should abut the curb, to minimize incursion into open space.

Access by physically disabled persons shall be provided in commercial centers, institutional uses, parks, and schools. Curb cuts should be provided where streets intersect sidewalks.

7.6b Hiking/Riding Trails

As shown in Figure 7.14, primary trail corridors are proposed within open space areas which may serve both hikers from the community and surrounding areas, and equestrians from off-site. These corridors include:

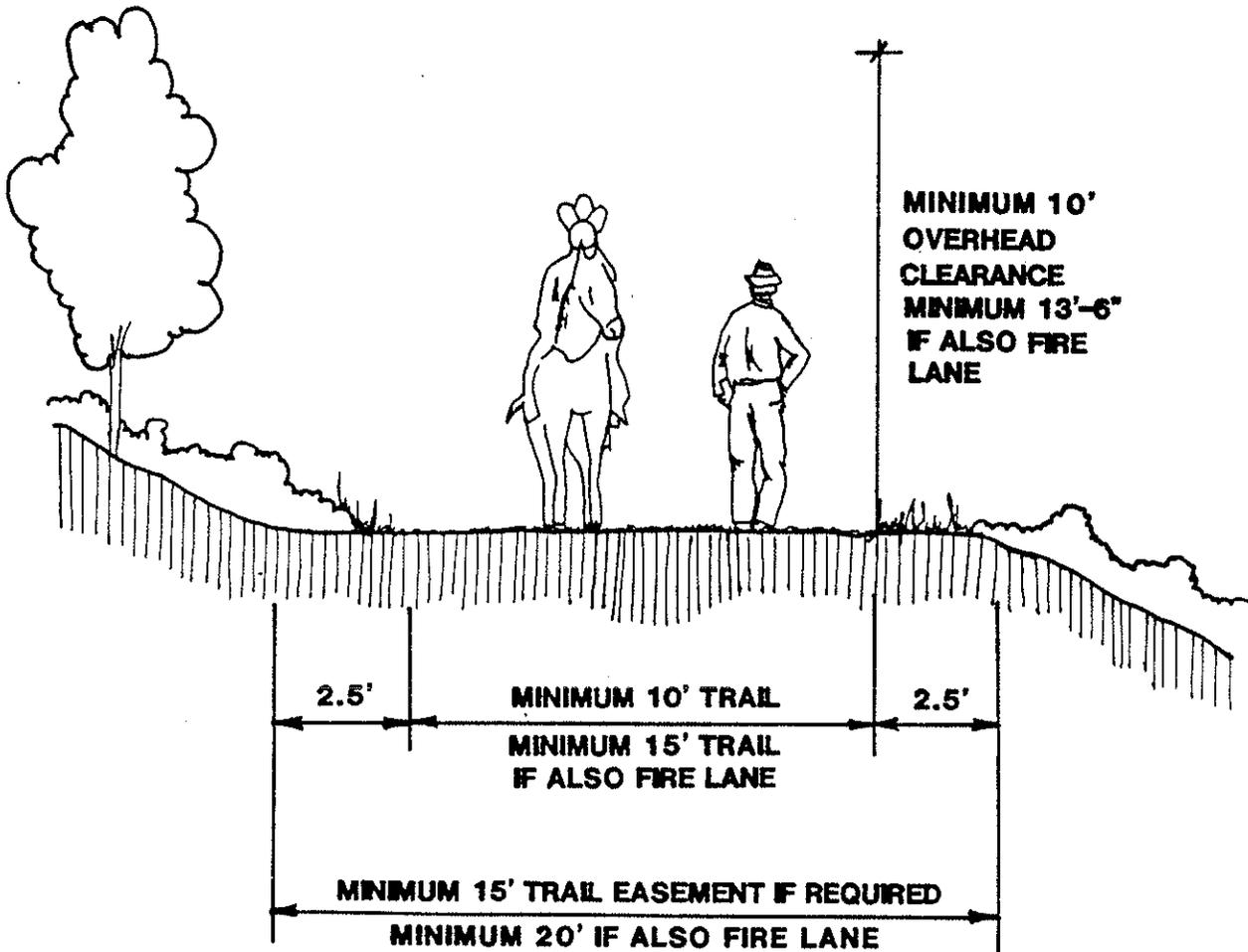
- o The Cottonwood Creek corridor, located in Open Spaces "B1" and "B2"; Open Space "C" within the Community Park; the Parkway Park (Planning Unit 17); and Open Space "D" within the expanded parkway of Cottonwood Canyon Road.
- o An optional connection of the Cottonwood Creek corridor from the Railroad Canyon Road/Cottonwood Hills Road intersection northeast to the creek culvert at the Canyon Lake boundary.
- o The San Jacinto River corridor (Open Space "A"); and
- o The Open Space "E2" trail corridor providing access to the City natural park in Canyon Lake Hills, potentially linking to the San Jacinto River corridor off-site.

An additional east-west corridor linking Holland Road and Cottonwood Creek is under study. The Cottonwood Creek and San Jacinto River corridors correspond to "secondary riding and hiking trails" proposed in the Riverside County General Plan. Provision of a trail corridor does not imply construction or right to use until such time as a trail program is implemented.

Any pathways should utilize existing dirt roads where possible, to minimize disturbance to natural open space. Trails and pathways should be dirt, decomposed granite, or similar materials appropriate to natural open space areas. Access control should be provided at trailheads and intersections with major community roads to limit incursion by off-road vehicles. Trailheads and intersections may be signed in accordance with the standards provided in Section 8.9.

Figure 7.15 illustrates development standards for the hiking/riding trails within the community. Trail and easement widths are shown for trails and trails doubling as fire lanes; trails utilizing existing dirt roads maybe wider. Vertical clearance standards are provided for equestrians and emergency vehicles; these may require selective pruning of trees.

Parking for hikers using the trail corridors should be provided in the Community Park. Hiker parking shall not be permitted in the commercial centers (Planning Units 3, 4 and 16). However, potential transit stops at the commercial centers should be accessible to the trail corridors. No equestrian centers are planned within the community.



NOTES: TRAIL MAY BE WIDER WHERE EXISTING DIRT ROADS ARE UTILIZED.

MAXIMUM DESIRABLE GRADE: 15%

PROVISION OF A TRAIL CORRIDOR DOES NOT IMPLY CONSTRUCTION OR RIGHT TO USE UNTIL SUCH TIME AS A TRAIL PROGRAM IS IMPLEMENTED.

**PRIMARY
HIKING/RIDING TRAIL
CORRIDOR**

**Cottonwood
Hills**

7.7 Circulation Implementation

Implementation mechanisms for community circulation proposals are outlined in Table 7.2. This includes land, improvements, and operations and maintenance options. Selection of an option, where there are choices, will occur with approval of a development agreement or final map, as appropriate.

The potential off-site improvement of Holland Road could be funded through a variety of mechanisms, including planned improvements in conjunction with Audie Murphy Ranch; amendment to the Menifee Valley Road and Bridge Benefit District; a new Assessment District including nearby properties; or the County Capital Improvement Program.

Implementation of open space related to circulation improvements is addressed in Section 6.3.

Table 7.2

CIRCULATION IMPLEMENTATION

On-Site Public Facility	Land Options	Improvement Options	Operations & Maintenance Options
RAILROAD CANYON ROAD	<ul style="list-style-type: none"> o Acquired thru Menifee Valley Road and Bridge Benefit District. Or City Benefit District. o Dedicated to City by developer(s). 	<ul style="list-style-type: none"> o Funded thru County Benefit District or City Benefit District. Improved by City. 	<ul style="list-style-type: none"> o City-maintained.
MAJOR COMMUNITY ROADS o Cottonwood Hills Road. o Holland Road. o Lost Road. o Cottonwood Canyon Road. Includes signals.	<ul style="list-style-type: none"> o Dedicated to City by developer(s). 	<ul style="list-style-type: none"> o Improved and funded by developer thru subdivision process. o Improved by developer, funded thru Mello-Roos. 	<ul style="list-style-type: none"> o City-maintained.
PUBLIC STREETS o Residential collector streets. o Local streets and cul de sacs.	<ul style="list-style-type: none"> o Dedicated to City by developer(s). 	<ul style="list-style-type: none"> o Improved and funded by developer thru subdivision process. 	<ul style="list-style-type: none"> o City-maintained.
PRIVATE STREETS o Private streets and drives.	<ul style="list-style-type: none"> o Deeded to project property owners association. 	<ul style="list-style-type: none"> o Improved and funded by developer thru subdivision process. 	<ul style="list-style-type: none"> o Project property owners association.

Table . . . (continued)

CIRCULATION IMPLEMENTATION

On-Site Public Facility	Land Options	Improvement Options	Operations & Maintenance Options
RIGHT-OF-WAY IMPROVEMENTS	(a) (b) (c) Dedicated public street right-of-way.	(a) (b) Per roadway in which located.	(a) (b) City-Maintained.
(a) Bike lanes.			
(b) Sidewalks.			
(c) Transit stops.		(c) Transit agency. Or adjacent project developer.	(c) Transit agency.
SPECIAL WALKWAYS			
(a) Linkage to East Neighborhood Park.	(a) Deeded to project property owners association as common area; possible City easement. Or deeded to City as part of neighborhood park.	(a) Improved and funded by developer thru subdivision process.	(a) Project property owners association. Or City-maintained as extension of neighborhood park.
(b) Community Park linkage.	(b) Part of school site and community park site.	(b) Part of school and community park improvements.	(b) Part of school and community park maintenance.
HIKING/RIDING TRAILS			
(a) Holland Road (off-site).	(a) Easement to County.	(a) Improved and funded as part of Audie Murphy Ranch development.	(a) County-maintained. Maintenance by Audie Murphy Ranch development.

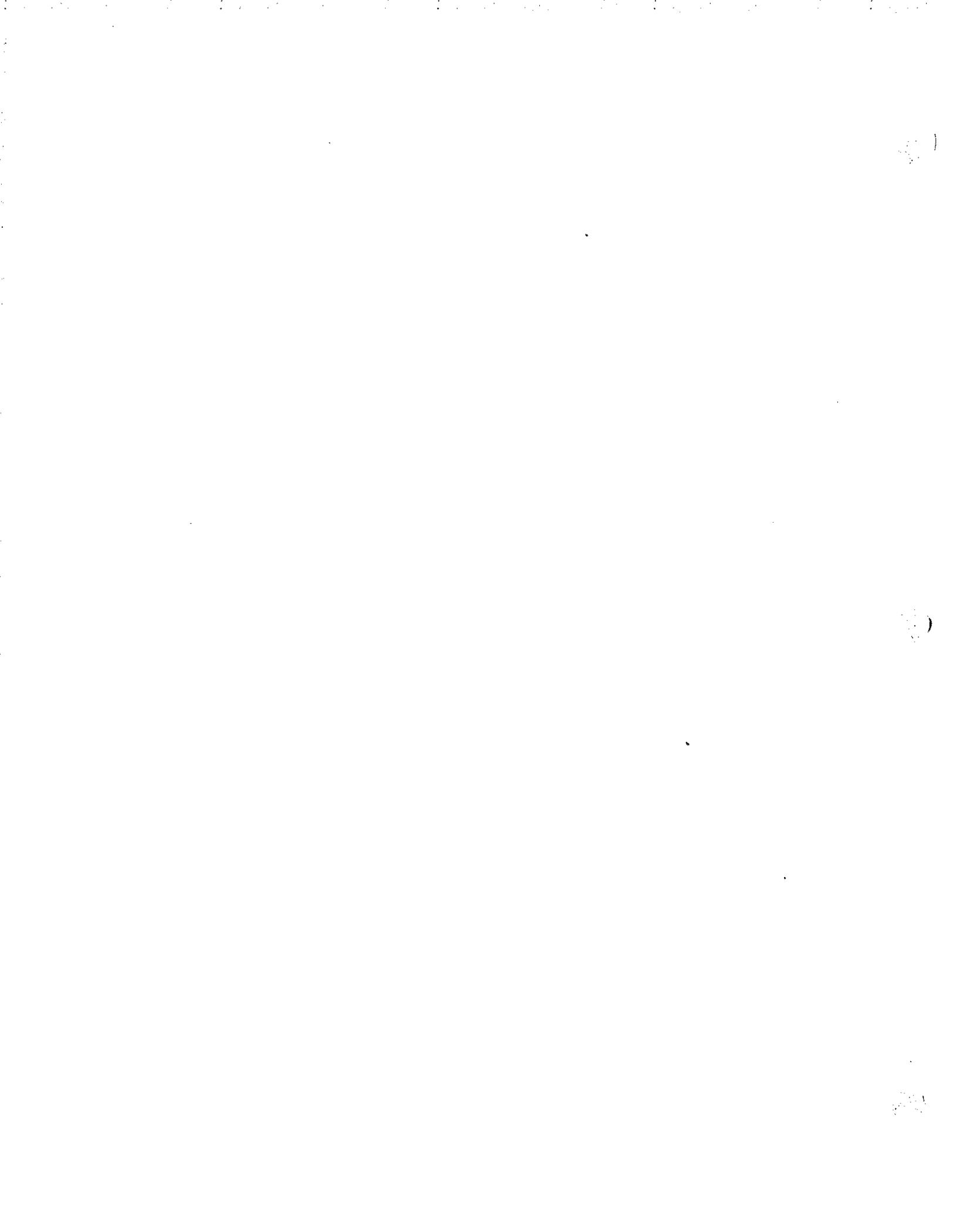
Table 7.2 (continued)

CIRCULATION IMPLEMENTATION

On-Site Public Facility	Land Options	Improvement Options	Operations & Maintenance Options
HIKING/RIDING TRAILS (continued)			
(b) Cottonwood Creek Corridor. Optional connection to Canyon Lake culvert.	(b) Open space or park-land dedeed in fee or dedicated to City. Expanded street right-of-way dedicated to City. Possible County easement.	(b) Existing dirt roads and trails. Improvement as part of parallel channel improvements in upper Cottonwood Creek. Improvement as part of parallel street improvements by developer. Improvement of Lower Cottonwood Creek trail by developer. Future improvement by County within existing open space corridor. Improvement as part of Community Park and Parkway Park.	(b) Cottonwood Hills Lighting and open space maintenance district, or equivalent. Part of Community Park. Part of Parkway Park. Trail user fees collected by City and County. City or County community-contributed service program.
(c) San Jacinto River Corridor.	(c) Open space dedeed in fee to City. Possible County easement.	(c) Existing dirt road.	(c) Cottonwood Hills Lighting and open space maintenance district, or equivalent. Trail user fees collected by City or County. City or County community-contributed service program. Water District.
(d) Open Space "E2" to Canyon Lake Hills Natural Park.	(d) Open Space dedeed in fee to City. Possible County easement.	(d) Improvement by City. Improvement by developer(s) of Cottonwood Hills and/or Canyon Lake Hills.	(d) Cottonwood Hills Lighting and open space maintenance district, or equivalent. Trail user fees collected by City or County. City or County community-contributed service program.

Zoning Element

Chapter 8



Chapter 8 - ZONING ELEMENT

This chapter prescribes the zoning and development standards for the various land uses described in this Plan. In general, the zoning districts and community signage standards are based on the existing City Zoning Ordinance, with modifications to accommodate conditions for the Cottonwood Hills community.

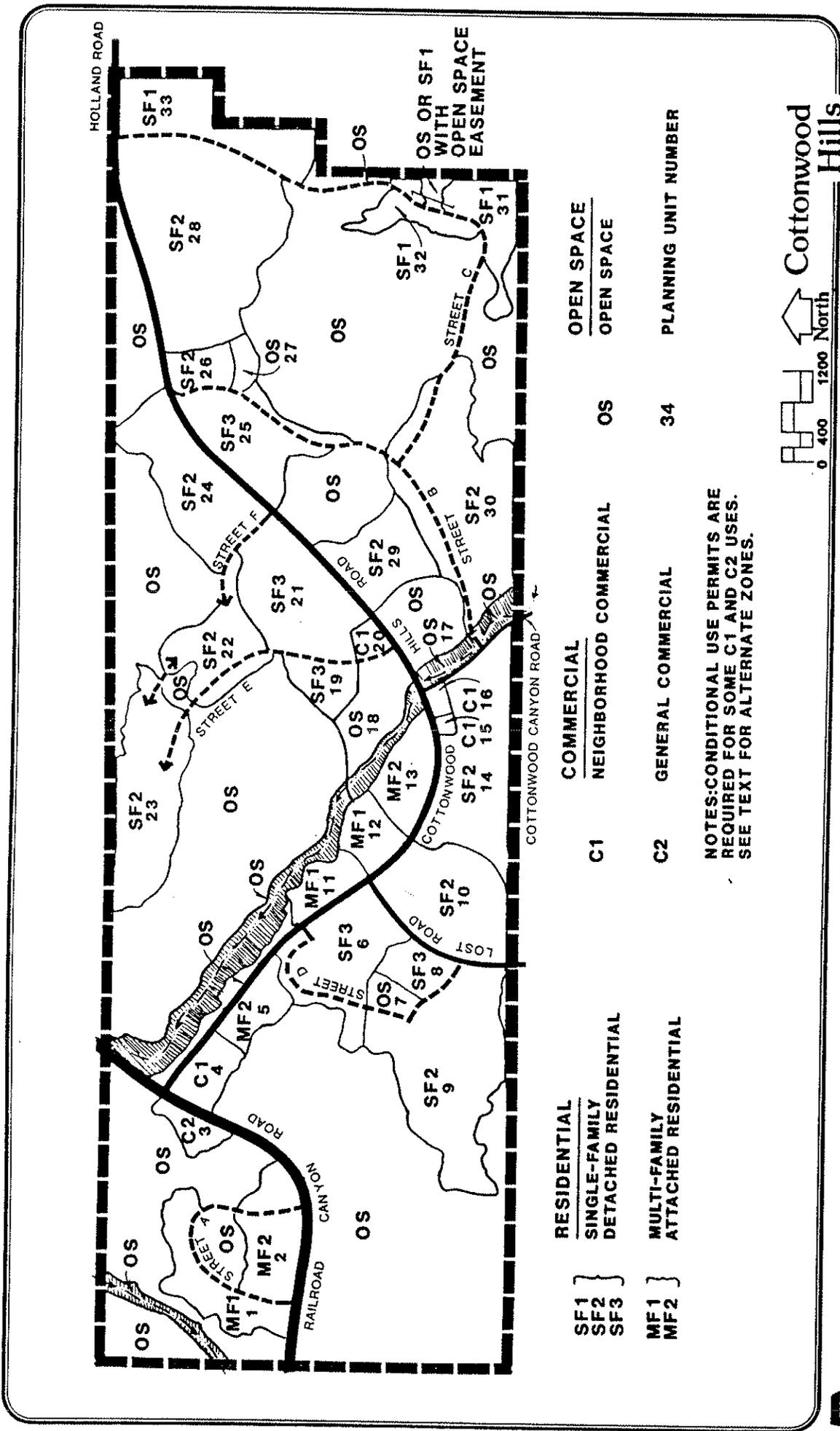
Figure 8.1 depicts the zoning for the specific plan area. The permitted uses, development regulations, and design standards of the designated zone shall apply to the project planning unit. Additional design guidelines for individual planning units are cited in Chapter 10.

References to the City of Lake Elsinore Zoning Ordinance refer to the ordinance in effect at the time of Specific Plan adoption. Where design guidelines or zoning development standards of this Plan do not agree with the Zoning Ordinance, this Plan shall apply.

In the event that alternate residential land uses are utilized as described in Chapter 4, the following zones shall apply:

- Planning Unit 15 - C1
- Planning Unit 20 - SF3

Variances from this Zoning Element are subject to Chapter 17.76 of the City's Zoning Ordinance, Variances. Deviations from the Design Guidelines set out in Sections 3.2, 4.1c, 4.2c, 5.1c, 5.2, 6.2, and 7.2 of this Plan are subject to City review during Site Plan Review (see Section 9.2).



Zoning Map 8.1

8.1 SF1: Single-Family Detached Residential 1 District

8.1a Purpose

The SF1 (single-family residential 1) zoning district is intended to provide for the development of low density, quality single-family residences on estate-sized lots. These estate-sized lots are intended to minimize the impact of development in environmentally sensitive areas which are not appropriate for higher density development. Certain uses permitted in the City's R-A Zone are not permitted, due to the character of the proposed development or provision of these uses elsewhere in the community.

8.1b Permitted Uses.

Uses permitted in this district shall include those uses listed below when developed in compliance with the purpose and intent of this zone. Each use shall be evaluated in terms of its design characteristics and specific site location pursuant to the provisions of Chapter 9 of the Specific Plan, Site Plan Review, and Chapter 17.82 of the Zoning Ordinance, Design Review.

- A. Single-family detached dwelling units; one dwelling unit per lot.
- B. Accessory uses and structures pursuant to Section 8.1d.
- C. Small family day care and residential care facilities pursuant to Chapter 17.16 of the Zoning Ordinance.
- D. Public utility distribution and transmission facilities excluding private radio, television, and paging antenna and towers.
- E. Government buildings and service facilities.
- F. Public parks, playgrounds, community centers, recreation buildings, and elementary, junior high, and high schools.
- G. Structures and installations necessary to the conservation and development of water resources and/or the control of flooding.
- H. Designated open space, public or common area.

8.1c Uses Subject to a Conditional Use Permit.

It is recognized that certain uses while similar in characteristics to Permitted Uses in Section 8.1b may have the potential to impact surrounding properties and therefore require additional approval and consideration. Such uses to be permitted in the SF1 District shall require a Use Permit pursuant to Chapter 17.74 of the Zoning Ordinance and shall include the following:

- A. Large family day care homes in compliance with the provisions of Chapter 17.16 of the Zoning Ordinance.

- B. Second units in compliance with the provisions of Chapter 17.17 of the Zoning Ordinance.
- C. Country clubs, swimming and tennis clubs, golf courses.
- D. Keeping of exotic animals or birds, or more than six (6) weaned dogs and/or cats, other than provided in this section on the same lots as a permitted dwelling for pets only and not for commercial purposes. The Planning Commission shall impose adequate limitations to assure that the residential character of the property and neighborhood are maintained.
- E. Agricultural and horticultural uses, for commercial purposes.

8.1d Accessory Uses and Structures.

The following accessory buildings and uses may be located on the same lot with a permitted dwelling, provided that they are found to be compatible with the residential character of the neighborhood and that any buildings or structures be harmonious with the architectural style of the main building. With the exception of open trellis-type patio covers and detached garages, all accessory buildings and/or structures shall be located only in rear yard areas or the enclosed portion of a side yard.

A. Uses:

- 1. Accessory Uses and Structures. Home occupations; subject to the completion and approval of an application for a home occupation issued by the Planning Division and compliance with the provisions of Chapter 17.15 of the Zoning Ordinance.
- 2. Non-commercial hobbies.
- 3. Keeping of household pets (when no commercial activity is involved). For the purpose of this ordinance, a household pet is an animal clearly considered customary to a residential uses, e.g., dogs, cats, birds, and fish. Said pets shall be limited to a maximum of six (6) weaned dogs and/or cats.

B. Structures:

- 1. Group I.

Sheds, children's playhouses, and similar enclosures of less than 120 square feet provided they do not exceed a maximum height of six and one-half (6-1/2) feet. Such structures may be located on the property line provided the design of the structure complies with the City's Fire and Building Codes.

2. Group II.

Unenclosed structures such as carports, gazebos, and patio covers (both trellis-type and solid) as well as enclosed structures not included in Group I but containing less than 600 square feet of floor area such as sheds, cabanas, children's playhouses, and work shops, provided they do not exceed a maximum height of fifteen (15) feet and are located no closer than five (5) feet to a side property line; ten (10) feet to a rear property line; or twenty (20) feet from public natural open space. Attached, enclosed structures in this category, such as garages and enclosed patios, shall be located no closer to a property line than the setback prescribed for the main dwelling unit.

3. Group III.

Garages, enclosed patios, workshops, cabanas, and similar enclosed structures containing 600 or more square feet of floor area; provided they are located no closer than twenty (20) feet to a public right-of-way or public natural open space; and no closer than ten (10) feet to any other property line.

4. Group IV.

- a. Antennas, satellite dishes, and similar devices; subject to compliance with the provisions of Chapter 17.67 of the Zoning Ordinance.
- b. Swimming pools, jacuzzis, spas, and associated equipment; provided they may be permitted only within rear yard areas or the enclosed portion of a side yard. Pool equipment located within ten (10) feet of a property line shall be separated from the adjacent property by a minimum six (6) foot high masonry wall. Otherwise there shall be no required setback provided the design and installation complies with the City's Building Codes.

Trellis-type patio covers may be located in a front yard area but shall encroach not more than twenty (20) feet into the required setback.

8.1e Lot Area.

- A. The minimum lot area for any new lot created in the SF1 District shall be one-half acre (21,780 square feet).
- B. Flag lots are not permitted.

8.1f Street Frontage Width.

The minimum street frontage width for any new lot created in the SF1 District shall be as follows:

- A. Standard lots: 100 feet.
- B. Knuckle or cul de sac lots: Fifty (50) feet; provided the average width is 100 feet.

8.1g Setbacks.

The following minimum setbacks shall apply to all new construction within the SF1 District:

- A. Front yard: Thirty (30) feet; provided however, that turn-in entry garages may encroach to within twenty (20) feet of the front property line.
- B. Side yard:
 - 1. Main dwelling unit: Adjacent to interior lot lines there shall be a minimum side yard of fifteen (15) feet. Adjacent to a public right-of-way or public natural open space the minimum side yard shall be twenty (20) feet.
 - 2. Accessory structures: In the rear one-half of the lot, as specified in Section 8.1d; otherwise the same as required for the main dwelling unit with the exception that where straight-in entry garage gains access via the side yard, the setback shall be twenty (20) feet.
- C. Rear yard:
 - 1. Main dwelling unit: Fifty (50) feet.
 - 2. Accessory structures: As specified in Section 8.1d with the exception that where straight-in entry garage gains access via the rear yard, the setback shall be twenty (20) feet.

8.1h Lot Coverage.

The maximum lot coverage in the SF1 District shall be twenty-five (25) percent.

8.1i Building Height.

Except as otherwise provided for accessory structures, the maximum building height in the SF1 District shall be thirty (30) feet.

8.1j Minimum Dwelling Unit Size.

The minimum dwelling unit size within SF1 District shall be 1,725 square feet exclusive of garage area.

8.1k Parking.

The provisions of Chapter 17.66 of the Zoning Ordinance shall be used to determine the required parking for development in the SF1 District.

8.1l Signs.

The provisions of Section 8.9, Community Signage, shall be used to determine permitted signs in the SF1 District.

8.1m Design Review.

No building permits shall be issued for the construction of any building or structure in the SF1 District until the applicant has obtained Design Review approval pursuant to the provisions outlined in Chapter 17.82 of the Zoning Ordinance. The following exemptions shall apply:

- A. Additions or alterations to an existing structure which do not change the use from one permitted in the District and which do not increase the floor area by more than fifty (50) percent.
- B. Group I, II and IV accessory structures.
- C. Fences and walls.

Although the above exempted structures, do not need formal Design Review approval, their proposed location and design must still be approved by the Director of Community Development or his designee, and building permits secured as required, prior to construction installation.

8.1n Design Standards.

Chapter 17.14 of the Zoning Ordinance contains residential development standards applicable to all projects within the City regardless of zoning district. It is therefore important that the provisions of Chapter 17.14 be considered together with the regulations contained herein for the SF1 District.

8.2 SF2: Single-Family Detached Residential 2 District

8.2a Purpose.

The SF2 District is intended to accommodate low density projects comprised of quality single-family residences at a density of two up to four dwelling units per gross residential acre (2 up to 4 DU/gross AC). This development is designed for an urban environment with available public services and infrastructure. The district is not intended for hillside development in steep slope areas. Certain uses permitted in the City's Single-Family Residential District are not permitted, due to the character of the proposed development or provision for these uses elsewhere in the community.

8.2b Permitted Uses.

Uses permitted in this district shall include those uses listed below when developed in compliance with the purpose and intent of this zone. Each use shall be evaluated in terms of its design characteristics and specific site location pursuant to the provisions of Chapter 9 of the Specific Plan, Site Plan Review, and Chapter 17.82 of the Zoning Ordinance, Design Review.

- A. Single-family detached dwelling units; one dwelling unit per lot.
- B. Accessory uses and structures pursuant to Section 8.2d.
- C. Small family day care and residential care facilities pursuant to Chapter 17.16 of the Zoning Ordinance.
- D. Public utility distribution and transmission facilities excluding private radio, television, and paging antennas and towers.
- E. Government buildings and service facilities.
- F. Public parks, playgrounds, community centers, recreation buildings, and elementary, junior high and high schools.
- H. Designated open space, public or common area.

8.2c Uses Subject to a Conditional Use Permit.

It is recognized that certain uses while similar in characteristics to Permitted Uses in Section 8.2b may have the potential to impact surrounding properties and therefore require additional approval and consideration. Uses permitted subject to approval of a Use Permit, pursuant to Chapter 17.74 of the Zoning Ordinance, in the SF2 District shall be as follows:

- A. Large family day care homes in compliance with the provisions of Chapter 17.16 of the Zoning Ordinance.

- B. Second units in compliance with the provisions of Chapter 17.17 of the Zoning Ordinance.
- C. Keeping of exotic animals or birds, or more than three (3) dogs and/or cats, on the same lot as a permitted dwelling for pets only and not for commercial purposes. The Planning Commission shall impose adequate limitations to assure that the residential character of the property and neighborhood are maintained.
- D. Churches, located on a site a minimum of one (1) acre in size and located on streets designated as arterial, major highway, or secondary highway streets. Churches shall not otherwise be permitted in the SF2 District.
- E. Horticultural uses, including growing of fruit, nuts, vegetables, and ornamental plants for commercial purposes.

8.2d Accessory Uses and Structures.

The following accessory buildings and uses may be located on the same lot with a permitted dwelling, provided that they are found to be compatible with the residential character of the neighborhood and that any buildings or structures be harmonious with the architectural style of the main building. With the exception of open trellis-type patio covers and detached garages, all accessory buildings and/or structures shall be located only in the rear one-half of the lot.

A. Uses:

1. Home occupations; subject to the completion and approval of an application for a home occupation issued by the Planning Division and compliance with the provisions of Chapter 17.15 of the Zoning Ordinance.
2. Non-commercial hobbies.
3. Keeping of household pets (when no commercial activity is involved). For the purpose of this ordinance, a household pet is an animal clearly considered customary to a residential use, e.g. dogs, cats, birds, and fish. Said pets shall be limited to a maximum of three (3) weaned dogs and/or cats. Birds shall be permitted only inside the main dwelling unit, unless a use permit is obtained.

B. Structures:

1. Group I.

Sheds, children's playhouses, and similar enclosures of less than 120 square feet provided they do not exceed a maximum height of six and one-half (6-1/2) feet. Such structures may be located on property line provided the design of the structure complies with the City's Fire and Building Codes.

2. Group II.

Unenclosed structures such as carports, gazebos, and patio covers (both trellis-type and solid) as well as detached enclosed structures not included in Group I, but containing less than 600 square feet of floor area such as sheds, children's playhouses, and workshops, provided they do not exceed a maximum height of fifteen (15) feet and are located no closer than five (5) feet to a property line, or ten (10) feet to a rear property line. Attached, enclosed structures in this category shall be located no closer to a property line than the setback prescribed for the main dwelling unit.

3. Group III.

Garages, enclosed patios, workshops, cabanas, and similar enclosed structures containing 600 or more square feet of floor area provided they are located no closer to a property line than the setback prescribed for the main dwelling unit, except that straight-in entry garages may be required a greater setback.

4. Group IV.

- a. Antennas; subject to compliance with the provisions of Chapter 17.67 of the Zoning Ordinance.
- b. Swimming pools, jacuzzis, spas, and associated equipment; provided they may be permitted only within rear yard areas or the enclosed portion of a side yard. Pool equipment located within ten (10) feet of a property line shall be separated from the adjacent property by a minimum six (6) foot high masonry wall. Otherwise there shall be no required setback provided the design and installation complies with the City's Building Codes.
- c. Open trellis-type patio covers may be located in a front yard area but shall not encroach more than five (5) feet into the required setback.

8.2e Lot Area.

The minimum lot area for any lot created in the SF2 District shall be as follows:

- A. Interior lots: 6,000 square feet, however, the average lot size for any subdivision (planning unit) shall be a minimum of 7,260 square feet.
- B. Corner lots: 7,700 square feet.
- C. Exception: Whenever a lot is adjacent to a lower density zoning district or to an existing developed parcel with a larger lot size than required in the SF2 District, a transition or buffer shall be provided between the adjacent property and a new subdivision, which may include, but is not limited to, lot size, lot width, lot depth, increased setbacks, or slopes. The purpose of this transition shall be to minimize the disparity between different densities of development. The transition or buffer shall be reviewed under the design guidelines set out in Section 3.2 of the Specific Plan during Site Plan Review.
- D. Flag lots are not permitted.

8.2f Street Frontage Width.

The minimum street frontage width for any new lot created in the SF2 District shall be as follows:

- A. Standard interior lots: Sixty (60) feet.
- B. Corner lots: Sixty-five (65) feet.
- C. Knuckle or Cul de sac lots: Forty (40) feet; provided the average width is sixty (60) feet.

8.2g Setbacks.

The following minimum setbacks shall apply to all new construction within the SF2 District:

- A. Front yard:
 - 1. Main dwelling unit: Twenty (20) feet.

2. Garage: Twenty (20) feet; provided, however, that turn-in entry garages may encroach to within fifteen (15) feet of the front property line. An additional three (3) foot setback variation requirement for no less than every third dwelling unit shall apply to the garage setback.

B. Side yard:

1. Main dwelling unit: Adjacent to interior lot lines there shall be a minimum side yard of five (5) feet. Adjacent to a public right-of-way the minimum side yard shall be fifteen (15) feet. Adjacent to public natural open space the minimum side yard shall be twenty (20) feet).

2. Accessory structures: In the rear one-half of the lot, as specified in Section 8.2d; otherwise the same as required for the main dwelling unit with the exception that where straight-in entry garage gains access via the side yard, the setback shall be twenty (20) feet.

3. Finished slopes exceeding five (5) percent shall not be permitted within fifteen (5) feet of the main dwelling unit. The exception is drainage swales.

C. Rear yard:

1. Main dwelling unit: Twenty (20) feet.

2. Accessory structures: As specified in Section 8.2d, with the exception that where straight-in entry garage gains access via the rear yard, the setback shall be twenty (20) feet.

3. Finished slopes in excess of five (5) percent shall not be permitted within fifteen (15) feet of the main dwelling unit. The exception is drainage swales.

8.2h Lot Coverage.

The maximum lot coverage in the SF2 District shall be thirty-five (35) percent.

8.2i Building Height.

Except as otherwise provided for accessory structures, the maximum building height in the SF2 District shall be thirty (30) feet. For the purposes of this zone, in the case of split levels no vertical section through the building shall measure more than thirty (30) feet.

8.2j Minimum Dwelling Unit Size.

The minimum dwelling unit size within the SF2 District shall be 1,150 square feet exclusive of garage area.

8.2k Parking.

The provisions of Chapter 17.66 of the Zoning Ordinance shall be used to determine the required parking for development in the SF2 District.

8.2l Signs.

The provisions of Section 8.9, Community Signage, shall be used to determine permitted signs in the SF2 District.

8.2m Design Review.

No building permits shall be issued for the construction of any building or structure in the SF2 District until the applicant has Design Review approval pursuant to the provisions of Chapter 17.82 of the Zoning Ordinance. The following exemptions shall apply:

- A. Additions or alterations to an existing structure which do not change the use from one permitted in the District and which do not increase the floor area by more than fifty (50) percent.
- B. Group I, II, and IV accessory structures.
- C. Fences and walls.

Although the above exempted structures do not need formal Design Review approval, their proposed location and design must be approved by the Director of Community Development or his designee, and building permits secured as required, prior to construction or installation.

8.2n Design Standards.

Chapter 17.14 of the Zoning Ordinance contains residential development standards applicable to all projects within the City regardless of zoning district. It is therefore important that the provisions of Chapter 17.14 be considered with the regulations contained herein for the SF2 District.

8.3 SF3: Single-Family Detached Residential District

8.3a Purpose.

The SF3 District is intended to accommodate projects comprised of quality single-family residences at a density of four up to eight dwelling units per gross residential acre (4 up to 8 DU/gross AC). This development is designed for an urban environment with available public services and infrastructure, and with school/park facilities nearby. Relative to the SF2 District, the smaller lot size of the SF3 Zone provides greater affordability and creates the opportunity for innovative lotting and product design. This district is intended to fulfill an intermediate role between SF2 detached and MF1 attached zones in income level, market niche, and project character. Certain uses permitted in the City's Single-Family Residential District are not permitted, due to the character of the proposed development or provision for these uses elsewhere in the community.

8.3b Permitted Uses.

Uses permitted in this district shall include those uses listed below when developed in compliance with the purpose and intent of this zone. Each use shall be evaluated in terms of its design characteristics and specific site location pursuant to the provisions of Chapter 9 of the Specific Plan, Site Plan Review, and Chapter 17.82 of the Zoning Ordinance, Design Review.

- A. Single-family detached dwelling units; one dwelling unit per lot.
- B. Accessory uses and structures pursuant to Section 3.3d below.
- C. Small family day care and residential care facilities pursuant to Chapter 17.16 of the Zoning Ordinance.
- D. Public utility distribution and transmission facilities excluding private radio, television, and paging antenna and towers.
- E. Government buildings and service facilities.
- F. Public parks, playgrounds, community centers, recreation buildings, and elementary, junior high, and high schools.
- G. Designated open space, public or common area.

8.3c Uses Subject to a Conditional Use Permit.

It is recognized that certain uses while similar in characteristics to Permitted Uses in Section 8.3b may have the potential to impact surrounding properties and therefore require additional approval and consideration.

Uses permitted subject to approval of a Use Permit, pursuant to Chapter 17.74 of the Zoning Ordinance in the SF3 District shall be as follows:

- A. Large family day care homes in compliance with the provisions of Chapter 17.16 of the Zoning Ordinance.
- B. Second units in compliance with the provisions of Chapter 17.17 of the Zoning Ordinance.
- C. Keeping of exotic animals or birds, or more than three (3) dogs and/or cats, on the same lot as a permitted dwelling for pets only and not for commercial purposes. The Planning Commission shall impose adequate limitations to assure that the residential character of the property and neighborhood are maintained.
- D. Churches, located on a site a minimum of one (1) acre in size and located on streets designated as arterial, major highway, or secondary highway streets. Churches shall not otherwise be permitted in the SF3 District.
- E. Horticultural uses, including growing of fruit, nuts, vegetables, and ornamental plants for commercial purposes.

8.3d Accessory Uses and Structures.

The following accessory buildings and uses may be located on the same lot with a permitted dwelling, provided that they are found to be compatible with the residential character of the neighborhood and that any buildings or structures be harmonious with the architectural style of the main building. With the exception of open trellis-type patio covers and detached garages, all accessory buildings and/or structures shall be located only in the rear one-half of the lot.

A. Uses:

- 1. Home occupations; subject to the completion and approval of an application for a home occupation issued by the Planning Division and compliance with the provisions of Chapter 17.15 of the Zoning Ordinance.
- 2. Non-commercial hobbies.
- 3. Keeping of household pets (when no commercial activity is involved). For the purpose of this zone, a household pet is an animal clearly considered customary to a residential use, e.g. dogs, cats, birds, and fish. Said pets shall be limited to a maximum of three (3) weaned dogs and/or cats. Birds shall be permitted only inside the main dwelling unit, unless a use permit is obtained.

B. Structures:

1. Group I

Sheds, children's playhouses, and similar enclosures of less than 120 square feet provided they do not exceed a maximum height of six and one-half (6-1/2) feet. Such structures may be located on property line provided the design of the structure complies with the City's Fire and Building Code.

2. Group II

Unenclosed structures such as carports, gazebos, and patio covers (both trellis-type and solid) as well as detached enclosed structures not included in Group I, but containing less than 600 square feet of floor area such as sheds, children's playhouses, and workshops, provided they do not exceed a maximum height of fifteen (15) feet and are located no closer than five (5) feet to a side property line, or ten (10) feet to a rear property line. Attached, enclosed structures in this category shall be located no closer to a property line than the setback prescribed for the main dwelling unit.

3. Group III

Garages, enclosed patios, workshops, cabanas, and similar enclosed structures containing 600 or more square feet of floor area provided they are located no closer to a property line than the setback prescribed for the main dwelling unit, except that straight-in entry garages may require a greater setback.

4. Group IV

- a. Antennas; subject to compliance with the provisions of Chapter 17.67 of the Zoning Ordinance.
- b. Swimming pools, jacuzzis, spas, and associated equipment; provided they may be permitted only within rear yard areas or the enclosed portion of a side yard. Pool equipment located within ten (10) feet of a property line shall be separated from the adjacent property by a minimum six (6) foot high masonry wall. Otherwise there shall be no required setback provided the design and installation complies the with City's Building Codes.
- c. Open trellis-type patio covers may be located in a front yard area but shall not encroach more than five (5) feet into the required setback.

8.3e Lot Area.

Two lotting options are permitted, "deep lots" and "wide lots," to provide variety and flexibility in design. The lot area for any lot created in the SF3 District shall be as follows, in square feet (SF):

	<u>Deep Lot Option</u>	<u>Wide Lot Option</u>
A. Minimum interior lot:	4,600 SF	4,100 SF
B. Minimum corner lot:	5,500 SF	4,650 SF
C. Minimum average lot size for any subdivision (planning unit):	4,800 SF	4,500 SF
D. Exception: Whenever a lot is adjacent to a lower density zoning district or to an existing developed parcel with a larger lot size than required in the SF3 District, a transition or buffer shall be provided between the adjacent property and a new subdivision, which may include, but is not limited to lot size, lot width, lot depth, increased setbacks, fire lanes, or slopes. The purpose of this transition shall be to minimize the disparity between different densities of development. The transition or buffer shall be reviewed under the design review guidelines set out in Section 3.2 of the Specific Plan during Site Plan Review.		
E. No flag lots are permitted.		

8.3f Street Frontage Width.

The minimum street frontage width for any new lot created in the SF3 District shall be as follows, in feet:

	<u>Deep Lot Option</u>	<u>Wide Lot Option</u>
A. Standard interior lot:	46 feet	52 feet
B. Corner lot:	55 feet	62 feet
C. Knuckle or cul de sac lot -- street frontage:	30 feet	35 feet
average width:	46 feet	52 feet

8.3g Setbacks

The following minimum setbacks shall apply to all new construction within the SF3 District, under either lotting option:

A. Front yard:

1. Main dwelling unit: Twenty (20) feet for Deep Lot Option. Fifteen (15) feet for Wide Lot Option.
2. Garage: Twenty (20) feet; provided however, that turn-in entry garages may encroach to within fifteen (15) feet of the front property line. An additional three (3) foot setback variation requirement for no less than every third dwelling unit shall apply to the garage setback.

B. Side yard:

1. Main dwelling unit: Adjacent to interior lot lines there shall be a minimum side yard of five (5) feet. Adjacent to a public right-of-way the minimum side yard shall be fifteen (15) feet. Adjacent to public natural open space, the minimum side yard shall be twenty (20) feet.
2. Accessory structures: In the rear one-half of the lot, as specified in Section 3.3d above, otherwise the same as required for the main dwelling unit with the exception that where straight-in entry garage gain access via the side yard, the setback shall be twenty (20) feet.
3. Finished slopes exceeding five (5) percent shall not be permitted within five (5) feet of the main dwelling unit. The exception is drainage swales.

C. Rear yard:

1. Main dwelling unit: Adjacent to interior lot lines, the setback shall be fifteen (15) feet. Adjacent to public natural open space or public right-of-way, the minimum rear yard shall be twenty (20) feet.

2. Accessory structures: As specified in Section 8.3d above, with the exception that where straight-in entry garage gains access via the rear yard, the setback shall be twenty (20) feet.
3. Finished slopes in excess of five (5) percent shall not be permitted within fifteen (15) feet of the main dwelling unit. The exception is drainage swales.

8.3h Lot Coverage.

The maximum lot coverage in the SF3 District shall be forty-five (45) percent.

8.3i Building Height.

Except as otherwise provided for accessory structures, the maximum building height in the SF3 District shall be thirty (30) feet. For the purposes of this zone, in the case of split levels no vertical section through the building shall measure more than thirty (30) feet.

8.3j Minimum Dwelling Unit Size.

The minimum dwelling unit size within the SF3 District shall be 1,000 square feet exclusive of garage area.

8.3k Parking.

The provisions of Chapter 17.66 of the Zoning Ordinance shall be used to determine the required parking for development in the SF3 District.

8.3l Signs.

The provisions of Section 8.9, Community Signage, shall be used to determine permitted signs in the SF3 District.

8.3m Design Review.

No building permits shall be issued for the construction of any building or structure in the SF3 District until the applicant has obtained Design Review approval pursuant to the provisions of Chapter 17.82 of the Zoning Ordinance. The following exemptions shall apply:

- A. Additions or alternations to an existing structure which do not change the use from one permitted in the District and which do not increase the floor area by more than fifty (50) percent.
- B. Group I, II, and IV accessory structures.
- C. Fences and walls.

Although the above exempted structures do not need formal Design Review approval, their proposed location and design must be approved by the Director of Community Development or his designee, and building permits secured as required, prior to construction or installation.

8.3n Design Standards.

Chapter 17.14 of the Zoning Ordinance contains residential development standards applicable to all projects within the City regardless of zoning district. It is therefore important that the provisions of Chapter 17.14 be considered with the regulations contained herein for the SF3 District.

8.4 MF1: Multi-Family Attached Residential 1 District

8.4a Purpose.

The MF1 District is intended to provide locations for quality residential projects, consisting of products other than single-family detached developments, at densities of up through fifteen (15) units to the gross residential acre, and in compliance with the Cottonwood Hills Specific Plan designation of Multi-Family Attached Residential 1. This zone is employed in an urban environment with available public services and infrastructure, in locations where access and surrounding uses are conducive to low density attached development. With the density range permitted in this zone, possible products include duplexes, townhouses (3 or more units per building), and townhouse/flats combinations. Certain uses permitted in the City's R-2 Medium Density Residential District are not permitted, due to the character of the proposed development or provision of these uses elsewhere in the community.

8.4b Permitted Uses.

Uses permitted in this district shall include those listed below when developed in compliance with the purpose and intent of this zone:

- A. Accessory uses and structures pursuant to Section 8.4d below.
- B. Duplexes and patio homes.
- C. Multiple-family attached dwellings, including triplexes, townhouses, and flats.
- D. Government buildings and service facilities.
- E. Public utility distribution and transmission facilities excluding private radio, television, and paging antennas and towers.
- F. Small family day care and residential care facilities pursuant to Chapter 17.16 of the Zoning Ordinance.
- G. Single-family uses when they comply with all the requirements of the SF2 or SF3 Zoning Districts.

8.4c Uses Subject to a Conditional Use Permit.

It is recognized that certain uses while similar in characteristics to Permitted Uses in Section 8.4b above may have the potential to impact surrounding properties and therefore require additional approval and consideration. Such uses to be permitted in the MF1 District shall require a Use Permit pursuant to Chapter 17.74 of the Zoning Ordinance and shall include the following:

- A. Commercial child day care centers.

- B. Condominiums subject to compliance with all provisions of Chapter 17.30 of the Zoning Ordinance.
- C. Convalescent and retirement homes, rest homes, sanitariums, and similar congregate care facilities.
- D. Large family day care homes in compliance with the provisions of Chapter 17.16 of the Zoning Ordinance.
- E. Tennis clubs and swimming clubs.

8.4d Accessory Uses and Structures.

The following accessory buildings and uses may be located on the same lot with a permitted use provided that they are found to be compatible with the residential character of the neighborhood, and that any buildings or structures be harmonious with the architectural style of the main building(s).

A. Uses:

- 1. Home occupations; subject to the completion and approval of an application for a home occupation permit issued by the Planning Division and in compliance with the provisions of Chapter 17.15 of the Zoning Ordinance.
- 2. Non-commercial hobbies.
- 3. Keeping of household pets (when no commercial activity is involved). For the purpose of this zone, a household pet is an animal clearly considered customary to a residential use, e.g. dogs, cats, birds, and fish. Said pets shall be limited to a maximum of three (3) weaned dogs and/or cats. The maximum number of birds and fish shall be as specified by the City's adopted Animal Control Ordinance.

B. Structures:

- 1. Antennas, satellite dishes, and similar devices; subject to compliance with the provisions of Chapter 17.67 of the Zoning Ordinance.
- 2. Carports and garages.
- 3. Community recreation buildings and facilities for use by the residents of a permitted development.

4. Equipment storage structures not exceeding 400 square feet. On duplex and triplex lots, sheds, children's play houses, and similar enclosures of less than 120 square feet and a height of six and one-half (6-1/2) feet may encroach into the required side and/or rear yard and shall have no required setback provided the design of the structure complies with the City's Fire and Building Codes.
5. Lattice patio covers, and gazebos.
6. Rental offices and management offices; only when they serve the project on which property they are located.
7. Special use rooms such as laundry rooms and pool dressing rooms.
8. Swimming pools, jacuzzis, spas, and associated equipment (provided said equipment is enclosed to reduce noise impacts).

Unless otherwise permitted in this zone, no accessory structures or associated equipment shall be located in a required yard area.

8.4e Lot Area.

The minimum lot area for any lot created in the MF1 District shall be one and one-half (1 1/2) net acres. However, within the boundaries of a MF1 Planning Unit lot, the minimum lot size requirement for individual lots shall be 1,500 square feet for attached units and 3,000 square feet for detached units.

8.4f Area Per Dwelling Unit.

The minimum average area required for each dwelling unit in the MF1 District shall be 2,200 square feet of lot area per unit. To determine the maximum number of units that may be constructed on a given property, divide the total net lot area by the square footage required for the type of product. The resultant number should be rounded down to the nearest whole number.

8.4g Setbacks.

The following minimum standards shall apply to all new construction within the MF1 District:

A. Front yard:

- | | |
|---------------------------------|---|
| 1. Main dwelling unit building: | Average of twenty (20) feet but in no or case shall be less than fifteen (15) feet. |
|---------------------------------|---|

2. Garages:

The setback to a straight-in garage shall be a minimum of eighteen (18) feet from property line or back of sidewalk (if the sidewalk is located within an easement on the property). The setback to the garage may also be five (5) feet subject to Design Review approval and compliance with the following: when the setback is five (5) feet an automatic roll-up garage door shall be utilized for the garage. When the garages front on a street (public or private) no more than fifty (50%) percent of the units shall incorporate the five (5) foot setback.

B. Side yard:

1. Adjacent to interior lot lines there shall be a minimum setback of five (5) feet. Adjacent to a public right-of-way or private street the minimum setback shall be fifteen (15) feet with the exception that where straight-in entry garage gains access via the side yard, the setback shall be twenty (20) feet.
2. For any buildings next to public natural open space or another Planning Unit, the minimum setback shall be twenty (20) feet.
3. Lattice patio covers may encroach to within five (5) feet of a side property line.

C. Rear yard:

1. Adjacent to interior lot lines, a public right-of-way or private street, the minimum setback shall be fifteen (15) feet, with the exception that where straight-in entry garage gains access via the rear yard, the setback shall be twenty (20) feet.
2. For any buildings next to public natural open space or another Planning Unit, the minimum setback shall be twenty (20) feet.
3. Lattice patio covers may encroach to within ten (10) feet of a side property line.

8.4h Lot Coverage.

The maximum lot coverage in the MF1 District shall be fifty (50) percent including all buildings and accessory structures. Attainment of the permitted maximum coverage shall be a secondary consideration to compliance with all other design regulations contained within this zone.

8.4i Building Height.

Except as otherwise provided for accessory structures, the maximum building height in the MF1 District shall be thirty (30) feet. However, within twenty-five (25) feet of an SF1, SF2, or SF3 District the maximum height shall be seventeen (17) feet.

8.4j Minimum Dwelling Unit Size.

The minimum dwelling unit size within the MF1 District exclusive of any balcony or patio area shall be as follows:

- A. Studio units (sleeping quarters within the living room area): 450 square feet.
- B. One (1) bedroom units: 600 square feet.
- C. Two (2) bedroom units or larger: 700 square feet plus 100 square feet for each additional bedroom.

8.4k Open Space.

In addition to any open space required by other provisions of the Zoning Ordinance, all projects developed in the MF1 District shall provide the following open space:

A. Private open space:

- 1. Duplexes and patio homes: Each dwelling unit shall be provided with a usable private open space area in the form of a patio or courtyard with a minimum area of 350 square feet and a minimum dimension of fifteen (15) feet.
- 2. Multiple-family attached dwellings: Each dwelling unit shall be provided with a usable private open space area in the form of a patio, a courtyard, or a balcony as follows:
 - o Units 600 square feet or less: eighty (80) square feet with a minimum dimension of eight (8) feet.
 - o Units larger than 600 square feet of floor area: 140 square feet and a minimum dimension of ten (10) feet.

For the purpose of this zone, private open space shall mean a fenced or otherwise screened area which is devoid of structures and improvements other than those provided for landscape or recreation purposes.

Common open space may be provided in lieu of private open space, if for each one (1) square foot of private open space reduction there shall be one and one-half (1-1/2) square feet of common open space added to the project over and above requirements of this zone.

- B. Common open spaces: Two hundred and fifty (250) square feet of usable open space per unit shall be provided within the boundaries of all projects. Usable open space shall constitute area(s) readily accessible, practical, and generally acceptable for active and/or passive recreational uses. In all instances, however, a majority of the usable common open space shall be devoted primarily to active recreational facilities (i.e., pool court games, par jogging courses, etc.). Common open space shall not include required setback areas or other areas less than twenty (20) feet in width.

8.4l Separation Between Buildings.

The minimum required separation between multi-family main buildings shall be as follows:

- A. Front to front: Twenty (20) feet for one story building plus each additional story shall be setback five (5) additional feet.
- B. Front to rear or rear to rear: Fifteen (15) feet for one story building plus each additional story shall be setback five (5) additional feet.
- C. End wall to front or rear: Ten (10) feet for one story buildings plus each additional story shall be setback five (5) additional feet.
- D. In order to encourage obliquely aligned buildings, where such alignments are used, the distances in A or B, as applicable, may be decreased by five (5) feet.

Unless otherwise provided by any other adopted City regulation, accessory structures may be located without regard to a minimum separation, subject to Design Review approval.

8.4m Walkways.

Where walkways pass between buildings, fences, or other structures, there shall be a minimum separation between said structures of ten (10) feet.

8.4n Laundry Facilities.

All projects developed within the MF1 District shall provide laundry facilities adequate to accommodate the number of units proposed within the project. The minimum number shall be one (1) washer and one (1) dryer per each nine (9) units; however, in no case shall there be less than one (1) washer and dryer provided. This requirement is waived if all dwelling units in the project (Planning Unit) are provided washer/dryer hook-ups.

8.4o Walls and Fences.

Decorative masonry walls a minimum of six (6) feet in height, as measured from the highest grade elevation either side of the wall, shall be provided along all side and rear property lines that abut a major utility easement, natural open space, or another development project (Planning Unit). However, where view opportunities exist, open fencing may be utilized (1) next to public natural open space, provided an adequate fire management zone is provided; and (2) where residential planning units abut, provided a minimum eighteen (18) foot vertical separation is employed. Wood fences may be used to separate private open space areas.

8.4p Storage.

A storage space of eighty-five (85) cubic feet with a minimum dimension of two (2) feet shall be provided for each dwelling unit in a location external to the unit. This requirement is waived if the dwelling unit has its own attached garage or garage on the same individual lot as the main dwelling unit.

8.4q Parking.

The provisions of Chapter 17.66 of the Zoning Ordinance shall be used to determine the required parking for development in the MF1 District.

8.4r Signs.

The provisions of Section 8.9, Community Signage, shall be used to determine permitted signs in the MF1 District.

8.4s Design Review.

No building permits shall be issued for the construction of any building or structure in the MF1 District until the applicant has obtained Design Review approval pursuant to the provisions of Chapter 17.82 of the Zoning Ordinance. The following exemptions shall apply:

- A. Accessory structures other than garages and carports.
- B. Fences and walls.

Although the above exempted structures do not need formal Design Review approval, their proposed location and design must still be approved by the Director of Community Development or his designee, and building permits secured as required, prior to construction or installation.

8.4t Design Standards.

Chapter 17.14 of the Zoning Ordinance contains residential development standards applicable to all projects within the City regardless of zoning district. It is therefore important that the provisions of Chapter 17.14 be considered together with the regulations contained herein for the MF1 District.

8.5 MF2: Multi-Family Attached Residential 2 District

8.5a Purpose.

The MF2 District is intended to provide locations for multiple-family residential projects at densities of up to twenty-four (24) dwellings to the gross residential acre, and in compliance with the Cottonwood Hills Specific Plan designation of Multi-Family Attached Residential 2. This zone is employed in an urban environment with available public services and infrastructure, in locations where access, commercial development, and other surrounding development are conducive to medium density attached development. With the density range permitted in this zone, possible products include two- or three-story flats with surface parking. Certain uses permitted in the City's R-3 High Density District are not permitted, due to the character of the proposed development or provision of these uses elsewhere in the community.

8.5b Permitted Uses.

Uses permitted in this district shall include those listed below when developed in compliance with the purpose and intent of this zone:

- A. Multiple-family dwellings.
- B. Accessory uses and structures pursuant to Section 8.5d below.
- C. Government buildings and service facilities.
- D. Public utility distribution and transmission facilities excluding private radio, television, and paging antennas and towers.
- E. Small family day care and residential care facilities pursuant to Chapter 17.16 of the Zoning Ordinance.
- F. Single-family uses when they comply with all requirements of the SF2 or SF3 Zoning Districts.

8.5c Uses Subject to a Conditional Use Permit.

It is recognized that certain uses while similar in characteristics to Permitted Uses in Section 8.5b above may have the potential to impact surrounding properties and therefore require additional approval and consideration. Such uses to be permitted in the MF2 District shall require a Use Permit pursuant to Chapter 17.74 of the Zoning Ordinance and shall include the following:

- A. Commercial child day care centers.
- B. Condominiums subject to compliance with all provisions of Chapter 17.30 of the Zoning Ordinance.

- C. Convalescent and retirement homes, rest homes, sanitariums, and similar congregate care facilities.
- D. Large family day care homes in compliance with the provisions of Chapter 17.16 of the Zoning Ordinance.
- E. Permitted primary structures which exceed the height limitation specified in Section 8.5j.
- F. Tennis clubs and swimming clubs.

8.5d Accessory Uses and Structures.

The following accessory buildings and uses may be located on the same lot with a permitted use provided that they are found to be compatible with the residential character of the neighborhood, and that any buildings or structures be harmonious with the architectural style of the main buildings.

A. Uses:

- 1. Home occupations; subject to the completion and approval of an application for a home occupation issued by the Planning Division and compliance with the provisions of Chapter 17.15 of the Zoning Ordinance.
- 2. Non-commercial hobbies.
- 3. Keeping of household pets (when no commercial activity is involved. For the purpose of this zone, a household pet is an animal clearly considered customary to a residential use, e.g. dogs, cats, birds and fish. Said pets shall be limited to a maximum of three (3) weaned dogs and/or cats. The maximum number of birds and fish shall as specified by the City's adopted Animal Control Ordinance.

B. Structures:

- 1. Antennas, satellite dishes, and similar devices; subject to compliance with the provisions of Chapter 17.67 of the Zoning Ordinance.
- 2. Carports and garages.
- 3. Community recreational buildings and facilities for use by the residents of a permitted development.
- 4. Equipment storage structures not exceeding 400 square feet.
- 5. Lattice patio covers, and gazebos.

6. Rental offices and management offices; only when they serve the project on which property they are located.
7. Special use rooms such as laundry rooms and pool dressing rooms.
8. Swimming pools, jacuzzis, spas, and associated equipment (provided said equipment is enclosed to reduce noise impacts).

Unless otherwise permitted in this zone, no accessory structures or associated equipment shall be located in a required yard area.

8.5e Lot Area.

The minimum net lot area for any lot created in the MF2 District shall be 7,260 square feet.

8.5f Area Per Dwelling Unit.

The minimum average area required for each dwelling unit in the MF2 District shall be 1,815 square feet of net lot area per unit. To determine the maximum number of units that may be constructed on a given property, divide the total net lot area by the square footage required for the type of product. The resultant number should be rounded down to the nearest whole number.

8.5g Street Frontage Width.

The minimum street frontage width for any new lot created in the MF2 District shall be sixty (60) feet.

8.5h Setbacks.

The following minimum standards shall apply to all new construction within the MF2 District:

- A. Front yard: The front yard setback to any building shall average twenty (20) feet but in no case shall be less than fifteen (15) feet.
- B. Side yard and rear yard:
 1. Main dwelling units: Adjacent to interior lot lines the setback shall be ten (10) feet; adjacent to a public right-of-way the minimum setback shall be fifteen (15) feet.
 2. Garage or enclosed carports: Five (5) feet.
 3. Open carports: Three (3) feet.
 4. For any buildings next to natural open space or another Planning Unit, the minimum setback shall be twenty (20) feet.

5. Lattice patio covers may encroach to within ten (10) feet of a rear property line or a public right-of-way and to within three (3) feet of a side property line not abutting a public right-of-way.

8.5i Lot Coverage.

The maximum lot coverage in the MF2 District shall be sixty (60) percent including all buildings and accessory structures. Attainment of the permitted maximum coverage shall be a secondary consideration to compliance with all other design regulations contained within this zone.

8.5j Building Height.

Except as otherwise provided for accessory structures, the maximum building height in the MF2 District shall be thirty-five (35) feet. However, within twenty-five (25) feet of an SF1, SF2, or SF3 District, the maximum height shall be seventeen (17) feet.

8.5k Minimum Dwelling Unit Size.

The minimum dwelling unit size within the MF2 District exclusive of any balcony or patio area shall be as follows:

- A. Studio units (sleeping quarters within the living room area): 450 square feet.
- B. One (1) bedroom units: 550 square feet.
- C. Two (2) bedroom units or larger: 700 square feet plus 100 square feet for each additional bedroom.

8.5l Open Space.

In addition to any open space required by other provisions of the Zoning Ordinance, all projects developed in the MF2 District shall provide the following open space:

- A. Private open space: Each dwelling unit shall be provided with a usable private open space area in the form of a patio or courtyard with a minimum area of one hundred (100) square feet, or a balcony as follows:
 1. Units 600 square feet or less: A balcony of sixty (60) square feet with a minimum dimension of six (6) feet.
 2. Units larger than 600 square feet of floor area: A balcony of eighty (80) square feet and a minimum dimension of eight (8) feet.

For the purposes of this zone, private open space shall mean a fenced or otherwise screened area which is devoid of structures and improvements other than those provided for landscape or recreation purposes.

Common open space may be provided in lieu of private open space, if for each one (1) square foot of private open space reduction there shall be one and one-half (1-1/2) square feet of common open space added to the project over and above any other requirements of this zone.

- B. Common open spaces: Two hundred and twenty-five (225) square feet of usable common open space per unit shall be provided within the boundaries of all projects. Usable open space shall constitute areas(s) readily accessible, practical, and generally acceptable for active and/or passive recreation uses. In all instances, however, a majority of the usable common open space shall be devoted primarily to active recreational facilities (i.e., pool, court games, par jogging courses, etc.). Common open space shall not include required setback areas or other areas less than twenty (20) feet in width.

8.5m Separation Between Buildings.

The minimum required separation between main buildings shall be as follows:

- A. Front to front: Twenty (20) feet for one story buildings plus each additional story shall be setback five (5) additional feet.
- B. Front to rear and rear to rear: Fifteen (15) feet for one story building plus each additional story shall be setback five (5) additional feet.
- C. End wall to front or rear: Ten (10) feet for one story buildings plus each additional story shall be setback five (5) additional feet.
- D. In order to encourage obliquely aligned buildings, where such alignments are used, the distances in A or B, as applicable, may be decreased by five (5) feet.

Unless otherwise provided by any other adopted City regulation, accessory structures may be located without regard to a minimum separation subject to Design Review approval.

8.5n Walkways.

Where walkways pass between buildings, fences, or other structures, there shall be a minimum separation between said structures of ten (10) feet.

8.5o Laundry Facilities.

All projects developed within the MF2 District shall provide laundry facilities adequate to accommodate the number of units proposed within the project. The minimum number shall be one (1) washer and one (1) dryer per each nine (9) units; however, in no case shall there be less than one (1) washer and dryer provided. This requirement is waived if all dwelling units in the project (Planning Unit) are provided washer/dryer hook-ups.

8.5p Walls and Fences.

Decorative masonry walls a minimum of six (6) feet in height, as measured from the highest grade elevation either side of the wall, shall be provided along all side and rear property lines that abut a major utility easement, natural open space, or another development project (Planning Unit). However, where view opportunities exist, open fencing may be utilized (1) next to public natural open space, provided an adequate fire management zone is provided; and (2) where residential planning units abut, provided a minimum eighteen (18) foot vertical separation is employed. Wood fences may be used to separate private open space areas.

8.5q Storage.

A storage space of eight-five (85) cubic feet with a minimum dimension of two (2) feet shall be provided for each dwelling unit in a location external to the unit. This requirement is waived if the dwelling unit has its own garage.

8.5r Parking.

The provisions of Chapter 17.66 of the Zoning Ordinance shall be used to determine the required parking for development in the MF2 District.

8.5s Signs.

The provisions of Section 8.9, Community Signage, shall be used to determine permitted signs in the MF2 District.

8.5t Design Review.

No building permits shall be issued for the construction of any building or structure in the MF2 District until the applicant has obtained Design Review approval pursuant to the provisions of Chapter 17.82 of the Zoning Ordinance. The following exemptions shall apply:

- A. Accessory structures other than garages and carports.
- B. Fences and walls.

Although these above exempted structures do not need formal Design Review approval, their proposed location and design must still be approved by the Director of Community Development or his designee, and building permits secured as required, prior to construction or installation.

8.5u Design Standards.

Chapter 17.14 of the Zoning Ordinance contains residential development standards applicable to all projects within the City regardless of zoning district. It is therefore important that the provisions of Chapter 17.14 to be considered together with the regulations contained herein for the MF2 District.

8.6 C1: Neighborhood Commercial District

8.6a Purpose.

The intent of the C1 District is to provide locations for retail and office uses which offer the sale of goods and services to the general public and which, through characteristics of their operation, serve primarily the day-to-day shopping needs of local residents. The C1 zone is intended to accommodate the land uses designated Neighborhood Commercial (C1) and Institutional (IN) in the Cottonwood Hills Specific Plan.

8.6b Permitted Uses.

Uses permitted in the C1 District shall include those businesses listed below which operate in compliance with the intent and standards of this district and are conducted entirely within a completely enclosed building. Each business shall be evaluated in terms of its operational characteristics and specific site location.

- A. Accountants, advertising agencies, appraisers, attorneys, business and management consultants, economists, public relations consultants, and other professional offices.
- B. Administrative or executive offices of any type of business.
- C. Apparel stores.
- D. Appliance stores.
- E. Architects, landscape architects, planners, engineers and surveyors, geologists, industrial designers, graphic designers, and interior designers not including retail sales on the premises.
- F. Art and craft galleries or studios.
- G. Barber shops and beauty salons.
- H. Bicycle shops.
- I. Employment agencies, travel agencies, and airline ticket agencies.
- J. Financial institutions, including banks, savings and loan associations, finance companies, and credit unions.
- K. Florists.
- L. Food stores; including markets, bakeries, health food establishments, and candy stores.
- M. General merchandise stores.

- N. Government buildings and service facilities; excluding storage equipment or repair or warehouses.
- O. Hardware stores.
- P. Health and exercise clubs.
- Q. Hobby supply stores.
- R. Insurance brokers and services, investment brokers, real estate brokers and offices, and title and escrow companies.
- S. Jewelry stores.
- T. Media shops; including bookstores, newsstands, and videotape outlets.
- U. Medical complexes; including medical, dental and health-related services; excluding any housing for aged, infirm, or ill persons.
- V. Music stores including sales of instruments, records, and tapes.
- W. Non-profit institutions whose primary purpose is the promotion of the public health and welfare.
- X. Oculists, opticians, and optometrists.
- Y. Office supply and stationery stores.
- Z. Personal service establishments; including barbershops, beauty shops, dry cleaning, and tailors.
- AA. Pet shops; retail sales and grooming only; no boarding of animals.
- BB. Prescription pharmacies.
- CC. Public utility distribution and transmission facilities excluding private radio, television, and paging antenna and towers.
- DD. Quick copy and printing establishments.
- EE. Restaurants and eating places provided they comply with the requirements of Chapter 17.66 of Zoning Ordinance, Parking Requirements; but excluding drive-ins and drive-throughs.
- FF. Schools and studios for dance, music, arts, crafts, and photography.
- GG. Service establishments; such as small appliance repair, watch and jewelry repair, and shoe repair.
- HH. Sporting goods stores.
- II. Toy shops.

- JJ. Vehicle parts sales (new or rebuilt only), and excluding repair and service.
- KK. Other uses that the Planning Commission finds by resolution to be in accord with the purpose of this zone and having characteristics similar to those listed in this section. A list of these uses shall be maintained in the Planning Department for future reference.

8.6c Uses Subject to a Conditional Use Permit.

It is recognized that certain uses while similar in characteristics to Permitted Uses in Section 8.6b may have the potential to impact surrounding properties and therefore require additional approval and consideration. Such uses to be permitted in the C1 District shall require a Use Permit pursuant to Chapter 17.74 of the Zoning Ordinance and shall include the following.

- A. Automatic car washes; provided they shall be located a minimum of two hundred (200) feet from any residential use.
- B. Bars or cocktail lounges; not in conjunction with a restaurant.
- C. Churches including in conjunction with a parochial school.
- D. Commercial or private day-care, nursery, and elementary schools.
- E. Drive-through or drive-in establishments; provided a safe and efficient circulation system can be provided completely on-site.
- F. Game arcades; includes any establishment having five or more mechanical or electrical games of chance, skill or entertainment, whether as the primary use or in conjunction with another business, but excluding vending machines dispensing a product for sale.
- G. Gasoline dispensing establishments, subject to the provisions of Chapter 17.38 of the Zoning Ordinance.
- H. Hotels.
- I. Mortuaries.
- J. Motels.
- K. Office condominiums.
- L. Private non-commercial clubs and lodges.
- M. Restaurants and eating places with outside eating areas.
- N. Small animal veterinary clinics subject to the following addition to all other conditions of the Use Permit:

1. Treatment of animals restricted to dogs, cats, and other small domesticated animals and birds.
 2. The operation shall be conducted in a completely enclosed and sound controlled building in such a way as to produce no objectionable noises or odors outside its walls.
 3. There shall be no outdoor runs or animal holding areas.
 4. There shall be no boarding of animals other than as necessary for recuperation of patients.
 5. The clinic shall have direct access from the parking area.
- O. Structures exceeding the maximum height permitted by Section 8.6f.

8.6d Site Criteria.

There shall be no minimum lot size required for a C1 parcel. However, in establishing and maintaining locations or approving uses for the C1 District it shall be demonstrated that the property is capable of compliance with the following criteria:

- A. The site can support safe and efficient on-site circulation and has convenient access to surface streets with adequate capacity.
- B. There is sufficient parcel size to ensure adequate buffering and screening where needed to protect residential developments.
- C. The site is physically suitable for the proposed type and density of development and capable of permitting full compliance with the City's development standards and practices.

8.6e Setbacks.

The following minimum standards shall apply to all new construction in the C1 District:

- A. Front: The front yard setback for any building in the C1 District shall average twenty (20) feet but in no case shall be less than fifteen (15) feet.
- B. Side and rear yard: No setback shall be required from interior lot lines except adjacent to a public right-of-way or a residential use, in which case the minimum setback shall be fifteen (15) feet.
- C. The setback from a public right-of-way to a parking or driveway area shall be ten (10) feet.
- D. For any buildings next to public natural open space, the minimum setback shall be twenty (20) feet.

8.6f Building Height.

Except as otherwise provided by Section 3.6c, the maximum building height shall be thirty-five (35) feet.

8.6g Landscape Improvements.

All areas not utilized for structures, parking, or other permitted uses shall be landscaped. In addition, the following minimum standards shall apply:

- A. Adjacent to Streets: A continuous area, a minimum of ten (10) feet in depth, shall be landscaped and maintained between parking areas and the public right-of-way. Parking areas should be screened as much as possible utilizing berms, shrubs, and other decorative treatments of sufficient size and height to meet this requirement.
- B. Buffer Landscaping: A continuous visual landscape screen, a minimum of fifteen (15) feet in depth, shall be maintained adjacent to all interior property lines which abut residential uses. At minimum, said buffer shall contain one (1) fifteen (15)-gallon, non-deciduous, umbrella form tree for each thirty (30) lineal feet of boundary length. No structure or use, including parking, drive aisles, or trash enclosures, shall encroach within this area.
- C. Generally: All building sites shall have a minimum landscaped coverage equivalent to ten (10) percent of the total lot area. Such landscaping shall be evenly distributed over the site and consist of an effective combination of trees, ground cover and shrubbery, which may include landscaping required for setbacks of buffers. A reduction in coverage may be sought and approved during the Design Review process in recognition of quality design. For the purposes of this provision quality considerations include the use of courtyards, atriums, creative use of ground floor public space, creative use of water elements, and the incorporation of sculpture or art work in the landscape proposal.

8.6h Parking.

The provisions of Chapter 17.66 shall be used to determine the car parking for development in the C1 District. Adequate on-site parking shall be provided for bicycles. Transit stop requirements, if any, shall be reviewed with the Lake Elsinore Transit System.

8.6i Signs.

The provisions of Section 8.9, Community Signage, shall be used to determine permitted signs in the C1 District.

8.6j Design Review.

No building permits shall be issued for the construction of any building or structure in the C1 District until the applicant has obtained Design Review approval pursuant to the provisions of Chapter 17.82 of the zoning ordinance.

8.6k Design Standards.

Chapter 17.38 of the Zoning Ordinance contains non-residential development standards applicable to all projects within the City regardless of zoning district. It is therefore important that the provisions of Chapter 17.38 be considered together with the regulations contained herein for the C1 District.

8.7 C2: General Commercial District

8.7a Purpose.

The intent of the C2 District is to reserve appropriate locations for the designated General Commercial land use, consistent with the Cottonwood Hills Specific Plan. The purpose is to accommodate a full range of retail stores, offices, and personal and business service establishments offering commodities and services scaled to meet the needs of the area-wide population.

8.7b Permitted Uses.

Uses permitted in the C2 District shall include those businesses listed below which operate in compliance with the intent and standards of this district and are conducted entirely within a completely enclosed building. Each business shall be evaluated in terms of its operational characteristics and specific site location.

- A. Adult entertainment facilities subject to the provisions of Chapter 17.70 of the Zoning Ordinance.
- B. All permitted uses of the C1 District as contained within Section 8.6b of the Specific Plan.
- C. Antique shops and auction galleries.
- D. Bowling alleys; provided they comply with the requirements of Chapter 17.66 of the Zoning Ordinance, Parking Requirements.
- E. Bus depots and transit stations.
- G. Department stores.
- F. Floor covering shops.
- H. Furniture stores.
- I. Home improvement centers.
- J. Hotels.
- K. Motels.
- L. Sales of motor vehicle, motorcycle, and recreational vehicle parts and accessories and service incidental to the sale of parts.
- M. Skating rinks; provided they comply with the requirements of Chapter 17.66 of the Zoning Ordinance, Parking Requirements.
- N. Second hand and thrift shops.

- O. Service businesses similar to retail stores which do not involve warehousing or storage, except accessory storage of commodities sold at retail on the premises.
- P. Theaters; provided they comply with the requirements of Chapter 17.66 of the Zoning Ordinance, Parking Requirements.
- Q. Other uses that the Planning Commission finds by resolution to be in accord with the purpose of this zone and having characteristics similar to those uses listed in this section. A list of these uses shall be maintained in the Planning Department for future reference.

8.7c Uses Subject to a Conditional Use Permit.

It is recognized that certain uses while similar in characteristics to Permitted Uses in Section 8.7b may require outdoor operation and/or have the potential to impact surrounding properties and therefore require additional approval and consideration. Such uses to be permitted in the C2 District shall require a Use Permit pursuant to Chapter 17.74 of the Zoning Ordinance and shall include the following:

- A. Uses permitted subject to a Use Permit in the C1 District as contained in Section 8.6c.
- B. Business colleges and professional schools.
- C. Car washes.
- D. Churches including in conjunction with a parochial school.
- E. Dance halls, discotheques, or any establishment providing live entertainment.
- F. Outdoor sales and display incidental and accessory to a permitted use.
- G. Structures exceeding the maximum height permitted by Section 8.7g.

8.7d Lot Area.

The minimum lot area for lots in the C2 District shall be 25,000 square feet net. Within centers which have Site Plan Review approval pursuant to Chapter 9 of the Specific Plan and which share reciprocal facilities such as parking and access, smaller lots in the form of individual pads may be permitted provided it can be shown that development upon those lots can comply, with the exception of street frontage width, with all of the standards of this zone.

8.7e Street Frontage Width.

The minimum street frontage width of any new lot created in the C2 District shall be 100 feet.

8.7f Setbacks.

The following minimum standards shall apply to all new construction in the C2 District:

- A. Front: The front yard setback for any building in the C2 District shall average twenty (20) feet but in no case shall be less than fifteen (15) feet.
- B. Side and rear yard: No setback shall be required from interior lot lines except adjacent to a public right-of-way or a residential use, in which case the minimum setback shall be fifteen (15) feet.
- C. The setback from a public right-of-way to a parking or driveway area shall be ten (10) feet.
- D. For any buildings next to public natural open space, the minimum setback shall be twenty (20) feet.

8.7g Building Height.

Except as otherwise provided by Section 8.7c, the maximum building height shall be forty-five (45) feet.

8.7h Landscape Improvements.

All areas not utilized for structures, parking, or other permitted uses shall be landscaped. In addition, the following minimum standards shall apply:

- A. Adjacent to Streets: A continuous area, a minimum of ten (10) feet in depth, shall be landscaped and maintained between parking areas and the public right-of-way. Parking areas should be screened as much as possible utilizing berms, shrubs, and other decorative treatments of sufficient size and height to meet this requirement.
- B. Buffer Landscaping: A continuous visual landscape screen, a minimum of fifteen (15) feet in depth, shall be maintained adjacent to all interior property lines which abut residential uses. At minimum, said buffer shall contain one (1) fifteen (15)-gallon, non-deciduous, umbrella form tree for each thirty (30) lineal feet of boundary length. No structure or use, including parking, drive aisles, or trash enclosures, shall encroach within this area.
- C. Generally: All building sites shall have a minimum landscaped coverage equivalent to ten (10) percent of the total lot area. Such landscaping shall be evenly distributed over the site and consist of an effective combination of trees, ground cover and shrubbery, which may include landscaping required for setbacks or buffers. A reduction in coverage may be sought and approved during the Design Review process in recognition of quality design. For the purpose of

this provision, quality considerations include the use of courtyards, atriums, creative use of ground floor public space, creative use of water elements, and the incorporation of sculpture or art work in the landscape proposal.

8.7i Parking.

The provisions of Chapter 17.66 of the Zoning Ordinance shall be used to determine the parking for development in the C2 District. Adequate on-site parking shall be provided for bicycles. Transit stop requirements, if any, shall be reviewed with the Lake Elsinore Transit System.

8.7j Signs.

The provisions of Section 8.9, Community Signage, shall be used to determine permitted signs in the C2 District.

8.7k Design Review.

No building permits shall be issued for the construction of any building or structure in the C2 District until the applicant has obtained Design Review approval pursuant to the provisions of Chapter 17.82 of the zoning ordinance.

8.7l Design Standards.

Chapter 17.38 of the Zoning Ordinance contains non-residential development standards applicable to all projects within the City regardless of zoning district. It is therefore important that the provisions of Chapter 17.38 be considered together with the regulations contained herein for the C2 District.

8.8 OS: Open Space District

8.8a Purpose.

The intent of the OS District is to reserve land for public park and recreational uses; and to secure natural open space as a permanent public resource, in order to protect the public health, safety, and welfare and preserve environmental and scenic resources.

8.8b Establishment.

Land placed in the OS District shall include land under public or private ownership as follows:

- A. Public parks and playgrounds, including neighborhood and community parks.
- B. Drainage or flood control channels, creeks, rivers, lakes and floodplains, including riparian habitat areas.
- C. Land which would endanger the health, safety, and public welfare of the residents, to include:
 - 1. Areas where terrain is too steep to build upon or where grading of the land may endanger public health and safety due to erosion by flooding.
 - 2. Areas subject to flooding or inundation from storm water.
 - 3. Geologically unstable areas.
 - 4. Greenbelt areas which are formed by land development by preserving the land as a natural area.
- D. Areas of outstanding cultural or historical value.

8.8c Uses Permitted.

- A. No building or land shall be used, and no building or structure shall be designed, erected, structurally altered or enlarged except for the following purposes:
 - 1. Drainage channels, bridges or crossings, watercourses, spreading grounds, settling basins, freeways, parkways, public streets, park drives, utility access roads, hiking/riding trails, and fire lanes.
 - 2. Privately owned or public recreational areas, parks, playgrounds, wildlife preserves, and such buildings and structures as are related thereto, but permitting no commercial uses.

3. Utility facilities, such as pump stations, transmission towers, water reservoirs, substations, and similar structures.
4. Agricultural uses provided an agreement between the City and the land owner can be made which will allow him operate under the California Land Conservation Act.
5. Similar uses to those listed in this section, which in the opinion of Planning Commission, would not be detrimental or incompatible with the intent and purpose, as set forth in this zone.

B. Uses prohibited in this district area:

1. Residential uses.
2. Commercial uses other than those incidental to recreational uses permitted in Section 8.8c above.
3. Industrial uses.

8.8d Building Height.

No building shall exceed twenty-five (25) feet in height or no more than one-story.

8.8e Lot Coverage.

No structure shall be permitted except accessory buildings or those structures related to public park and recreational facilities or utilities. In no case shall buildings or structures exceed thirty (30) percent of the total area in use.

8.8f Signs.

The provisions of Section 8.9, Community Signage, shall be used to determine permitted signs in the OS District.

8.9 Community Signage

8.9a Purpose.

The purpose of this section is to define standards which contribute to a comprehensive system of temporary and permanent signs for the Cottonwood Hills community -- which enhance aesthetic compatibility while being effective in fulfilling the informational needs of the community. The purpose and intent of this section is to:

- A. Assure that all signs are designed, erected and maintained in a manner to enhance, rather than detract from the ultimate design and appearance of the Cottonwood Hills community; providing for business identification, but eliminating clutter and confusion which detract from community appearance;
- B. Prohibit the installation and maintenance of signs which unduly distract motorists' attention from driving, and which detract from the attention that should be devoted to traffic movement and to signs and signals promoting traffic safety;
- C. Prevent installation and maintenance of signs which singly or conjunctionally have an injurious effect on the people and the economic well-being of the City;
- D. Assure that size and location do not constitute an obstacle to effective fire protection and fire fighting techniques; nor constitute a direct or potential danger to vehicular or pedestrian traffic, especially in the event of structural failure during periods of inclement weather and earthquakes, or in the event of impaired vision due to improper size and/or location;
- E. Provide for the coordination of signage on a community-wide basis, as a contributing element to the visual character of the community;
- F. Incorporate the long-range marketing needs of a master-planned community in signage standards and signage procedures;
- G. Otherwise protect the public health, safety, and promote the public welfare.

8.9b Definitions.

- A. For the purpose of this section, certain terms used herein are defined as follows:
 1. "Balloon" means an inflatable bag or other inflatable device of any size;

2. "Building Frontage" means the lineal extent of a building or unit along either a street or a public parking area serving the business, not including loading or service areas;
3. "Copy Area" means the area of the sign face, exclusive of margins, in which copy may be placed.
4. "Decorative Wall Sign" means any sign, figures, or letters affixed to a free-standing garden, entry, or retaining wall of a development project.
5. "Double-faced Sign" means a single sign with two (2) parallel sign faces back-to-back;
6. "Electric Sign" means an advertising structure served or energized with electrical current for purpose of illuminating or for any other purpose;
7. "Free-standing Sign" means any temporary or permanent sign not attached to a building;
8. "Freeway" means a highway with respect to which the owners of abutting lands have no right of easement or access to or from their abutting lands, or in respect to which such owners have only limited or restricted easement or access and which is declared to be such in compliance with the Streets and Highways Code of the State;
9. "Ground Sign" means a permanent or temporary sign enclosed on all four sides, supported by a solid pedestal or columns which are not calculated toward the sign area;
10. "Monument Sign" means a permanent low profile sign, not exceeding six (6) feet in height, except as otherwise noted, supported by a solid pedestal extending under the entire length of the sign;
11. "Nonadvertising Sign" means an sign posted on private property containing thereon a regulatory or warning notice and upon which no advertising matter is displayed;
12. "Permanent Sign" means any sign which is installed or displayed on a long-term, continuous basis with no permit period limitation;
13. "Planning Unit" means the individual project area identified by number in the Cottonwood Hills Specific Plan, with a designated land use and Specific Plan zoning;

14. "Political Sign" means a sign relating to a forthcoming public election or referendum indicating the name and/or picture of an individual seeking election to a public office, or a sign pertaining to issues, or a sign pertaining to the advocating by persons, groups, or parties of the political views or policies;
15. "Portable Sign" means any movable sign that is not permanently secured or attached to an approved permanently established structure, support or anchor;
16. "Projecting Sign" means any sign which is affixed or attached to, and is supported solely by a building wall or structure, or parts thereof, and extends beyond building wall, or structure or parts thereof more than twelve (12) inches and whose angle of incidence to said building wall, structure or parts thereof, is greater than thirty (30) degrees;
17. "Roofline" means the height above finish grade of the uppermost beam, rafter, ridge board, or purlin of any building;
18. "Roof Sign" is any sign erected, constructed and maintained wholly or partially above the roofline;
19. "Sign" means and includes every announcement, declaration, demonstration, display, illustration, insignia, surface or space when erected or maintained in view of the general public for identification, advertisement or promotion of the interests of any business or person;
20. "Sign Area" means the entire area within the outside border of the sign. The area of a sign having no continuous border or lacking a border shall mean the entire area within a single continuous perimeter formed by no more than eight straight lines enclosing the extreme limits of writing, representations, emblem, or any fixture or similar character, integral part of the display or used as a border excluding the necessary supports or uprights on which such sign is placed. Where a sign has two or more faces, the area of all faces shall be included in determining the area of the sign, except that where two such faces are placed back-to-back and are at no point more than three feet from one another, the area of the sign shall be taken as the area of one face if the two faces of equal area, or as the area of the larger face if the two faces are of unequal area;
21. "Street Frontage" means the lineal extent of a parcel of land along a street;
22. "Temporary Sign" means any sign which is installed or displayed for a fixed or limited period, subject to a sign permit;

23. "Wall Sign" includes all flat signs, either of solid face construction or individual letters, which are placed against the exterior wall of any building or structure and extending not more than one (1) foot from the face of the building and having the advertisement on one (1) sign face only.

8.9c Permit Required.

A sign permit shall be obtained from the Community Development Department prior to the placing, erecting, moving, reconstructing, altering, or displaying of any exterior signs including change of face or copy on existing signs unless exempted by Section 8.9f, and not including merely refurbishing existing signs. A building permit and/or electrical permit may also be required.

8.9d Procedure.

- A. The Cottonwood Hills Community Signage Program, to be submitted with the Site Plan Review application for Phase One of the community, shall be adopted by the Planning Commission as a guideline for sign design and placement within the specific plan area.
- B. Application for sign approval shall be made upon forms provided by the City and shall have and be accompanied by the following information and materials:
1. Name, address and telephone number(s) of the owner of the property;
 2. Name, address and telephone number(s) of the applicant;
 3. Name, address and telephone number(s) of the sign contractor;
 4. Location of building, structure or lot to which, or upon which, the sign or other advertising structure is to be attached or erected, together with a statement showing the street frontage of such lot, if applicable;
 5. Three (3) copies of plan showing:
 - a. Position of sign or other advertising structure in relation to adjacent building or structures;
 - b. The design and size, structural details and calculations signed by a registered professional engineer, if required by the Chief Building Official;
 - c. A current photograph(s) showing existing signs on the premises and adjacent property, and certifying the date on which the photographs were taken;

- d. A statement showing the size and dimensions of all signs existing on the premises at the time of making such applications;
 - e. Such other information as the City shall deem reasonable and necessary to insure safety of construction and compliance with the intent of this section.
- C. Fees. Every applicant, before the granting of a permit, shall pay to the Planning Division the permit fees as established by resolution for each sign or other advertising structure regulated by this section.
- D. Issuance of Permits. It shall be the duty of the Planning and Building Divisions, upon the filing of an application for a sign permit, to examine such plans and specifications and other data and the premises upon which it is proposed to erect the sign or advertising structure; and if it shall appear that the proposed structure is in compliance with all the requirements of the approved Cottonwood Hills Community Sign Program and this section of the Specific Plan, the Director of Community Development or his designee shall then issue the sign permit.
- E. Revocation of Permit. The Community Development Director is authorized and empowered to revoke any permit upon failure of the holder thereof to comply with any provisions of this section, with a written statement for reasons of revocation.
- F. Temporary Signs. Temporary advertising signs for new commercial and residential developments shall be subject to the review and approval of the Director of Community Development. Such advertising sign permits shall be valid for the permit period specified in the schedule outlined in Table 8.1, with permits renewable for successive time periods thereafter. As a condition of approval, a five hundred (500) dollar cash bond and right of entry agreement for each sign shall be required in order to guarantee prompt removal upon expiration of approval period.

8.9e Stop Orders.

The issuance of a sign permit shall not constitute a waiver of this section or any ordinance of the City, and the Building Division is authorized to stop any sign or advertising structure installation which is being carried on in violation of this title, or of any other ordinance of the City.

Table 8.1

TEMPORARY SIGN PERMIT PERIOD SCHEDULE

Sign Type	Initial Permit Period
Off-site Freeway Signs	5 Years
Roadside Signs	5 Years
Community Entrance Signs	5 Years
Public Facilities Signs	3 Years
Residential Project Identification Signs	3 Years
Model Directional Signs	2 Years
Model Complex Signs	2 Years
Model Complex Flags	2 Years
Commercial and Residential Construction Signs	1 Year
Commercial Project Marketing Signs	1 Year
Commercial Sales and Leasing Signs	1 Year

8.9f Exemptions.

A. The following non-illuminated signs shall be permitted in all zoning districts with no permit required, subject to the limitations provided in this section, or as otherwise provided by state law:

1. One (1) double-faced or two (2) single-faced real estate signs per street frontage not exceeding six (6) feet in area nor six (6) feet in height pertaining to the sale or rental of the property on which displayed, provided that such signs shall be removed at the time the property is sold or rented;

On vacant parcels larger than 20,000 square feet in area, one (1) double-faced real estate sign per street frontage not exceeding thirty-two (32) square feet in area may be placed in lieu of the smaller sign, provided that it shall be a minimum of fifteen (15) feet from any street right-of-way or driveway and shall not exceed ten (10) feet in height;

2. One (1) professional nameplate or occupational sign denoting only the name and occupation of an occupant in a commercial building or public institutional building, provided that said sign does not exceed two (2) square feet in area and is attached to and mounted parallel to the face of the building not exceeding four (4) inches from the wall;
3. One (1) single-faced identification nameplate or sign on an apartment house, boarding or rooming house or similar uses, not exceeding three (3) square feet in area; provided that said sign is attached to and mounted parallel to the face of the building; not exceeding four (4) inches from the wall;
4. One (1) nameplate, denoting only the name of occupants of a dwelling, and not exceeding two (2) square feet in area not located closer than two (2) feet to the property line;
5. Traffic or other municipal signs, legal notices, railroad crossing or danger signs;
6. Nonadvertising warning signs or trespassing signs on private property posted no closer than one hundred (100) feet apart not exceeding three (3) feet in area;
7. Nonadvertising signs of public utility companies as may be required in their operations in providing services for the health and welfare of the general public, or as required by any law or regulations of the state or any agency thereof;

8. On-site directional signs for public and private developments, denoting the entrance, exit and direction of traffic flow and not exceeding four (4) square feet in area; provided such sign is not prohibited or further regulated by other sections of this title and any other ordinance of the City;
9. Civil interest signs limited to religious, charitable, educational, or cultural events. Each business may display one (1) such sign in its window containing a maximum of four (4) square feet in area, for not more than thirty (30) days before the event takes place. The sign must be removed within twenty-four (24) hours after the event takes place;
10. Nonadvertising displays commemorating legal holidays; providing, however, that said displays are not detrimental to public health, safety and general welfare;
11. Off-site directional signs for the location of residential open houses and garage sales not exceeding three (3) square feet in area and subject to the regulations in Section 8.9r, Off-site Directional Signs;
12. Temporary political signs subject to the regulations in Section 8.9l, Temporary Political Signs.

8.9g Nonconforming Signs.

Any permanent sign legally erected and maintained at the time of adoption of this title, although each sign does not conform to the regulations herein specified, may continue to be used. However, at the time of any change to the sign including change of face of the sign, the sign shall be removed or made to conform to the provisions of this title.

All other nonconforming signs and advertising devices shall be removed within sixty (60) days from the date of adoption of this title.

8.9h Removal of Obsolete Signs.

It shall be the responsibility of the property owner to have signs pertaining to enterprises or occupants that are no longer using a property removed or the sign copy obliterated within sixty (60) days after the associated enterprise or occupant has vacated the premises.

8.9i Maintenance.

All signs and sign structures shall be periodically inspected and maintained at reasonable intervals including replacement of defective parts, painting, repainting, cleaning and other acts required to maintain the sign. The Director of Development Services shall require corrections or removal of any sign deemed to be in violation of this title or any other ordinance of the City.

8.9j Prohibited Signs.

All signs not specifically permitted by other provisions of this section shall be prohibited. The following signs shall not be permitted in any district:

- A. Portable signs, including free-standing and wheeled or other signs and inflatable signs or balloons containing signs;
- B. No vehicle containing any advertising matter, words, symbols, or pictures shall be so parked whether on public or private property for the primary purpose of advertising or directing attention to a business;
- C. Signs which incorporate in any manner any flashing, moving, or intermittent lighting;
- D. Rotating or animated signs, or signs which contain any moving parts;
- E. No sign, lights or other advertising structure shall be located in such a manner as to constitute a hazard to pedestrian or vehicular traffic, or in such a manner as to obstruct free and clear vision at any location where, by reason of the position, shape, color or movement may interfere with, obstruct the view of, or be confused with any authorized traffic sign, signal or device. Nor shall such sign or advertising structure make use of any word, phrase, symbol or character in such a manner as to interfere with, mislead or confuse traffic;
- F. Spinners or similar advertising devices;
- G. Signs which exceed the roofline or parapet to which such signs are attached;
- H. Signs on vehicles, trailers, boats, or other similar property parked on public or private property within the City limits for the purpose of advertising said vehicle, trailer, boat, or similar property for rent, sale, or lease, unless:
 - 1. Such property is on the premises owned, rented or leased by the owner of the vehicle, trailer, boat or similar property, and the subject premises is occupied by the owner as a business or residence and subject to the provisions of Section 17.66.020(F) of the Zoning Ordinance; or
 - 2. Such property is on a business premises licensed by the City to engage in the sale, rental or lease of such property; or
 - 3. Only one (1) sign, one (1) square foot or less in area, is displayed which does not interfere with driver visibility as may be required by the State Vehicle Code.
- I. Any off-site advertising except as otherwise provided in this section.

8.9k Advertising on Public Property.

- A. No person, except as public officer or City employee in the performance of his duty shall paste, post, paint or erect any flag, pennant, sign or notice of any kind or cause the same to be done upon public property, street, bridge, or sidewalk within the City and no person shall attach any item to private utility company poles without prior written approval from the utility company to which such poles belong.
- B. Exceptions. Signs for special events to the benefit of the entire community and authorized by the City Manager.

8.9l Temporary Political Signs.

- A. General. Political signs are permitted in all districts subject to the following limitations:
 - 1. Time Limits. No sign shall be posted more than sixty (60) days prior to the election to which it pertains. All political signs shall be removed within seven (7) days following the election to which they pertain;
 - 2. Sponsor Identification. The name, address, and telephone number of the person or organization responsible for posting a political sign shall be affixed to each sign in a permanent waterproof manner.
- B. Exceptions. Political signs shall be prohibited in locations listed below:
 - 1. Public Right-of-way. No political sign shall be posted within the street right-of-way (including, but not limited to, median islands, tract entry planters, treewells, and parkways), or on any traffic-control sign or device;
 - 2. Public Facilities. No political sign shall be posted on any building or on any land owned by the City.

8.9m Community-Level Signage.

- A. Temporary Roadside Signs. The purpose of temporary roadside signs is to reinforce directional information provided by any off-site freeway signs. Roadside signs may identify the community from either outside or inside the specific plan area boundaries, provided that when placed outside the boundaries, the property on which the sign is located is vacant. No more than three (3) temporary roadside signs will be permitted, and each shall be a ground sign not exceeding thirty-two (32) square feet in sign area nor fifteen (15) feet in height.

- B. Permanent Community Entrance Monuments. Community entrance monuments introduce incoming traffic to Cottonwood Hills. These permanent signs are oriented toward traffic entering at the major and secondary community entrances. No more than three (3) such signs are permitted in the Specific Plan area. Entrance monuments shall be single- or double-faced and average no more than eight (8) feet in height. All such signs shall reflect an architectural theme consistent with the project design.
- C. Temporary Community Entrance Signs. Temporary community entrance signs identify and market Cottonwood Hills to incoming traffic during the construction build-out period. All signs shall be located within the specific plan boundary. No more than three (3) signs are permitted and each shall be a ground sign, double-faced, not exceeding one hundred (100) square feet in sign area nor twenty (20) feet in height.
- D. Subdivision Directional Signs. The purpose of subdivision directional signs is to direct vehicular traffic from major entrances into the community to residential projects. The signs are permitted in setback areas and median strips at right angles to the major roads. One (1) directional sign may be installed at each turn from community entrances and thoroughfares of Cottonwood Hills to the entrance of each separately marked residential project. Directional signs shall be single- or double-faced ground signs with a maximum height of fifteen (15) feet and a maximum width of six (6) feet.
- E. Temporary Community Entrance Flags. Flags shall be permitted to call attention to permanent community entrance monuments. Flags shall not be higher than twenty (20) feet from ground level and may be used only to display the corporate emblem of the project developer or an emblem identifying the Cottonwood Hills community.

8.9n Public Facilities Signage.

- A. Public Facility Uses. Public facilities signs identify school sites in the SF2 and SF3 residential districts; and park sites, open space areas, and multi-purpose trails, located in the OS open space district to users and passersby.
- B. Temporary Public Facilities Signs. Temporary public facilities signs identify sites of proposed public facilities and are utilized to communicate information pertaining to future use of these sites. One (1) sign per street frontage per future facility site is permitted. Signs shall be ground or free-standing signs, single-faced, with a maximum sign area of eighty (80) square feet and a maximum height of fifteen (15) feet.

- C. Permanent Public Facilities Signs. One (1) sign per principal site entrance is allowed for permanent identification of public facilities. Signs shall be monument signs, single- or double-faced with a copy area no greater than fifty (50) square feet and a maximum height of six (6) feet.

8.90 Residential Project Signage.

- A. Residential Uses. Residential project signage identifies individual residential planning units and model complexes, and provides information and direction needed to navigate from community-level streets into residential projects and model complexes and rental offices. These signs are permitted in single-family detached residential planning units, zoned SF1, SF2 or SF3, and in the multi-family attached projects, which are located in either the MF1 or MF2 zoning districts.
- B. Permanent Residential Project Entry Signs. Residential project entry signs identify individual single-family and multi-family projects. Two (2) single-faced or one (1) double-faced sign oriented toward the public right-of-way may be installed at each main entrance to residential projects. A minimum of one (1) entry sign per planning unit shall be provided. Project entry signs shall be monuments or decorative wall signs, not exceeding an average of six (6) feet in height.
- C. Temporary Residential Project Identification Signs. Temporary residential project identification signs identify individual residential projects and provide information about the projects during the sales period. One (1) sign is permitted per subdivision, located at the primary entrance to the subdivision. Such signs shall be single- or double-faced ground signs, not exceeding one hundred (100) square feet in sign area and twenty (20) feet in height.
- D. Temporary Model Directional Signs. Temporary model directional signs are used to direct vehicular traffic from the project entrance to the model complex. One (1) directional sign per turn from the project entry to the model complex will be permitted. Such signs shall be single-faced ground signs, not exceeding twenty-four (24) square feet of sign area and no more than ten (10) feet in height.
- E. Temporary Model Complex Signs. Various signs shall be used to provide identification and directional information relating to the model complex. Identification signs for the model complex, model unit, model parking area, and sales office are permitted. Standards for these signs shall be provided in the Cottonwood Hills Community Sign Program.
- F. Temporary Model Complex Flags. Flags shall be permitted to call attention to the residential model complex. A maximum of twenty (20) flags, each four (4) feet high and six (6) feet wide, may be placed around the complex. Flags shall not be higher than twenty (20) feet

from ground level and may be used only to display the corporate emblem of the project developer or an emblem identifying the residential development.

- G. Temporary Residential Construction Signs. Construction signs identify the development under construction and denote the developer, architect, engineer, and/or contractor. One (1) sign along each street frontage per residential planning unit shall be permitted. Such signs shall be ground signs, not exceeding one hundred (100) square feet in sign area nor be greater than sixteen (16) feet in height.

8.9p Commercial Project Signage.

- A. Commercial Uses. Commercial project signage identifies use areas in the C1 and C2 zoning districts. In the Cottonwood Hills Specific Plan, this includes all commercial and institutional planning units.
- B. Businesses or establishments within a commercial planning unit shall comply with a project uniform sign program approved by the Director of Community Development, based on the Cottonwood Hills Community Signage Program.
- C. No sign shall be permitted that does not pertain to the planning unit as an overall development or to an approved business conducted on the premises, except as provided in Section 8.9f, Exemptions.
- D. Permanent Commercial Center Signs. Commercial center signs provide permanent identification of commercial and institutional projects from adjacent public streets. Three types of center signs are permitted: (1) project entry signs, located on either one or both sides of each site entry; (2) corner signs at the intersection of public streets, at the project perimeter; and (3) major tenant directory signs. All signs shall be single- or double-faced depending on location, and shall be monument signs or decorative wall signs in combination with a landscaping or planter treatment. All signage shall be subject to the following standards:
1. The total sign area shall not exceed thirty (30) square feet per one hundred (100) lineal of street frontage on which signage is located, provided, however, that the total maximum sign area shall not exceed three hundred twenty (320) square feet;
 2. No sign shall exceed six (6) feet in height;
 3. Signs shall reflect the architectural design of the buildings with which they are associated, and shall incorporate unifying features such as materials;

4. No portion of any sign or supporting structure shall be located closer than five (5) feet to any property line, nor be located in such a manner as to constitute a hazard to pedestrian or vehicular traffic;
5. No sign is permitted for frontages on local residential streets;
6. All commercial center signs shall include the address of the center in numerals and/or letters at least six (6) inches high. Addresses shall not be obscured by landscaping or other obstructions;

E. Permanent Tenant Monument Signs. Tenant monument signs provide secondary tenant identification to passing motorists, complementing other permanent commercial signage. Businesses in a separate building and occupying at least one hundred (100) feet of frontage on one street may be permitted a single- or double-faced monument sign, subject to the following standards:

1. Sign area per street frontage shall not exceed twenty (20) square feet per one hundred (100) lineal feet of the street frontage on which the sign is located, provided, however, that no one sign shall exceed sixty (60) square feet;
2. Maximum height of monument signs shall not exceed six (6) feet above the public sidewalk;
3. Signs shall reflect the architectural design of the building with which they are associated, and shall incorporate unifying features such as materials;
4. No portion of any sign or supporting structure shall be located closer than five (5) feet to any property line, nor be located in such a manner as to constitute a hazard to pedestrian or vehicular traffic;
5. No sign is permitted for frontages on local residential streets;
6. All tenant monument signs shall include the address of the business in numerals and/or letters at least six (6) inches high. Addresses shall not be obscured by landscaping or other obstructions;

F. Permanent Tenant Identification Signs. Tenant identification signs indicate the occupant of specific buildings within a commercial or institutional project. One (1) wall sign per street frontage per tenant is permitted. The total sign area permitted per building frontage shall not exceed one (1) square foot per lineal foot of building frontage on which the sign is located subject to the following:

1. Building frontages may not be combined to permit a larger sign on any one building frontage;
2. Signs shall be attached to the building or canopy, or suspended from a beam, parallel to the building face. No portion of any sign or its supporting structure may project more than six (6) inches from the face of the building or structure to which it is attached.

In addition, small suspended or projecting tenant identification signs oriented to pedestrians may be permitted subject to the following:

1. A maximum of one (1) such sign per building frontage is permitted provided that it is perpendicular to the main face of the building and suspended from a canopy or projects not more than three (3) feet from the building face;
2. Signs shall not exceed two (2) square feet in area and shall have a minimum ground clearance of eight (8) feet.
3. All such signs shall be non-energized and non-electrical.

G. Permanent Commercial Informational Signs. Informational signage for directing auto, service, and pedestrian traffic is permitted. This may include backdoor signs which identify rear entrances to buildings; signs providing business hours and related information; and signs which designate or direct traffic. Such signage shall be addressed in the uniform sign program for each planning unit.

H. Temporary Commercial Project Marketing Signs. Temporary commercial project marketing signs identify the commercial and institutional planning units and provide information about the proposed projects during the initial marketing period. Temporary project marketing signs shall be ground signs, single- or double-faced, with a maximum sign area of one hundred twenty (120) square feet and a height not exceeding fourteen (14) feet.

I. Temporary Commercial Construction Signs. Temporary construction signs identify the commercial or institutional project under construction and denote the developer, architect, engineer, and/or contractor. One (1) such sign shall be permitted at each commercial or institutional lot during the initial construction period. Signs shall be single- or double-faced ground signs, with a maximum sign area of one hundred (100) square feet and a maximum height of sixteen (16) feet.

J. Temporary Commercial Sales and Leasing Signs. Temporary sales and leasing signs identify real estate property in each commercial/institutional project during the subsequent sales and leasing period. One (1) such sign is permitted for each commercial and institutional lot. Sales and leasing signs shall be single- or double-faced ground signs with a sign area not exceeding forty (40) square feet and a maximum height of ten (10) feet.

K. Temporary Window Signs. Temporary window signs, including signs painted on windows and banners, shall be permitted subject to the following:

1. They shall be permitted only inside a window of the business to which such sign pertains;
2. Total area occupied by said sign shall not exceed more than twenty-five (25) percent of the window area through which they are displayed;
3. Signs shall be displayed in a neat and orderly manner and shall not contain any words, symbols or pictures that may be offensive to the general public;
4. Window signs shall not be displayed for more than thirty-one (31) days consecutively.

L. Temporary Advertising Devices. Temporary advertising devices, such as pennants, banners and flags, shall be permitted for grand openings, change of ownership and special promotions, subject to the following regulations:

1. All temporary pennants, banners and flags shall require a permit and shall be subject to the review and approval of the Planning Division;
2. Pennants, banners and flags shall be displayed only at the location where the grand opening occurs and shall not be displayed for more than thirty-one (31) consecutive days;
3. Pennants, banners and flags for change of ownership of the business shall not be displayed for more than thirty-one (31) consecutive days;
4. Pennants, banners and flags for special promotions shall be permitted four (4) times a year subject to the following:
 - a. They shall not be displayed more than ten (10) consecutive days.
5. Flags and pennants shall contain no advertising;
6. The display of banners shall be subject to the following additional regulations:
 - a. Sign area of banners shall not exceed one (1) square foot per lineal foot of building frontage on which the sign is located, except that no banner shall be larger than one hundred (100) square feet;

- b. Only one (1) banner shall be permitted per building frontage;
- c. Banners shall be attached to the building or canopy parallel to the building face. No portion of any banner shall project more than six (6) inches from the face of the building or canopy to which it is attached.

8.9g Signs for Gasoline Dispensing Establishments.

The following regulations shall apply to all signs and advertising structures for service stations, including mini-markets or similar associated uses:

- A. One monument sign per street frontage may be permitted subject to the following:
 - 1. Sign area shall not exceed twenty (20) square feet per one hundred (100) lineal feet of street frontage, plus twenty-four (24) square feet. Price signing shall be included within this sign area;
 - 2. Maximum height of the sign shall not exceed six (6) feet above the adjacent public sidewalk;
 - 3. Signs shall reflect the architectural design of the building with which they are associated and shall incorporate unifying features such as materials;
 - 4. Street frontages may not be combined to permit a larger sign on any frontage;
 - 5. All monument signs shall include the address of the business in numerals and/or letters at least six (6) inches high. Addresses shall not be obscured by landscaping or other obstructions;
 - 6. All monument signs shall be located on one side or in the median of each of the establishment's primary entrances and shall be landscaped.
- B. The total sign area of all wall signs per building frontage shall not exceed one square foot per lineal foot of building frontage on which the sign is located;
- C. Signs above pump and pump islands shall be limited to directions for use of pumps and payments, or other signs required by state regulations, and sign area shall not exceed a total of ten (10) square feet per pump island;
- D. Temporary window signs, including signs painted on windows and banners, shall be permitted subject to the following:
 - 1. They shall be permitted only inside a window of the business to which such signs pertain;

2. Total area occupies by said signs shall not exceed more than twenty-five (25) percent of the window area through which they are displayed, whichever is less;
 3. Signs shall be displayed in a neat and orderly manner and shall not contains any words, symbols or pictures that may be offensive to the general public;
 4. Window signs shall not be displayed for more than thirty-one (31) days consecutively.
- E. Temporary advertising signs may be permitted subject to the provisions of Section 8.9g, Temporary Advertising Devices.

8.9r Temporary Off-Site Directional Signs.

- A. Off-site directional signs for the location of residential open houses and garage sales, subsequent to the initial residential marketing, are permitted subject to the following:
1. Signs may only be posted on weekends between six (6) p.m. on Friday and six (6) p.m. on Sunday, on legal holidays between eight (8) a.m. and six (6) p.m. and between eight (8) a.m. and two (2) p.m. on one weekday designated by the City Council;
 2. Signs may be posted within the public right-of-way only within parkways, treewells, and tract entry planters. Signs may not be posted in median islands, on utility poles, light standards, traffic signals, street trees or in any fashion that would interfere with traffic signals or sight visibility at intersections and driveways;
 3. There shall be no more than one (1) sign per direction of traffic at any intersection within the public right-of-way;
 4. Signs shall be at least one thousand (1,000) feet apart, except at intersections;
 5. Maximum area of directional signs shall not exceed three (3) square feet nor shall any sign be erected in excess of four (4) feet in height;
 6. Signs may be posted on private property;
- B. Signs in violation of this section may be subject to removal and disposition without notice or warning.

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**Community-Wide
Implementation Element**
Chapter 9

9.1b Specific Plan Changes

Minor changes to the approved Specific Plan may be made by the Community Development Director, provided that such changes are consistent with the purpose and character of the approved Specific Plan. All modifications or amendments to the approved Specific Plan, other than such minor changes, shall be processed as a Specific Plan Amendment and shall be subject to all Specific Plan procedures.

The following modifications constitute minor changes to the approved Specific Plan:

1. Transfer of Units: Transfer of dwelling units from one residential planning unit to another is permitted, under the criteria specified in Section 3.3a.
2. Planning Unit Size: The gross acreage of a planning unit (including manufactured slopes, and residential collector, local, and private streets as applicable) may vary up to and including a maximum of 5 percent in size from the acreage specified in the Specific Plan. An additional gross acreage change may be permitted for residential planning units in conjunction with a dwelling unit transfer, as specified in Section 3.3a. For parkland acreage, the gross acreage in the specific plan area must total at least 57.1 acres, with a maximum credit of 5.0 acres per elementary school.
3. Alternate Uses: Alternate uses for the elementary school sites and institutional sites are permitted, as described in Sections 5.1b and 4.2a, respectively.
4. Roadways: Minor changes in roadway alignments and street sections are allowed, provided such changes are consistent with the streetscape concept for major roads, and subject to the approval of the City Engineer.
5. Phasing Program: The phasing program may be modified, provided the objectives of the program (as set out in Section 9.3) continue to be met, and the terms of the Development Agreement between the City and developer(s) are honored.

Chapter 9 - COMMUNITY-WIDE IMPLEMENTATION ELEMENT

9.1 Implementation Process

9.1a Development Processing

As diagrammed in Figure 9.1, the Cottonwood Hills Specific Plan constitutes one step in a series of steps in securing City approval of development within the specific plan area. While conforming to the City General Plan, the adopted Specific Plan becomes the basis for reviewing subsequent tract maps and permits. Zoning controls are prescribed in Chapter 8, while design guidelines are specified in each plan element.

Companion documents to the Specific Plan include the Environmental Impact Report (EIR) and Development Agreement. The EIR cites the existing conditions in the specific plan area; anticipated impacts of development as proposed in the Specific Plan; and mitigation measures which are in turn incorporated into the Specific Plan. Appendix B summarizes the environmental impacts and mitigation from the EIR. The Development Agreement specifies the phasing and financing of public infrastructure and facilities, based on the Specific Plan. This is discussed further in Section 9.3.

The following administrative standards apply to the implementation process:

1. The tentative or parcel map and Site Plan Review package (as described in Section 9.2) shall substantially conform to the standards and guidelines of the Specific Plan.
2. The final subdivision map or project shall substantially conform with the approved tentative or parcel map as well as approved Site Plan Review package.
3. Building permits for dwelling units shall be issued when a final subdivision has been recorded. Permits may be issued for model units prior to the final map recordation subject to the requirements of the City.
4. Mitigation measures for environmental impacts shall be reviewed during the Tentative Map/Site Plan Review stage. The Tentative Map shall be conditioned as necessary to mitigate any remaining impacts at the construction documents and construction stages.

9.2 Site Plan Review

The purpose of the Site Plan Review process is to ensure that each proposed development project is consistent with the Specific Plan. This process is based on the site plan review specified in Section 17.99.140 of the Specific Plan District ordinance.

Site Plan Review is required for both residential and non-residential uses, on a planning unit basis. Chapter 10 provides a review list for each planning unit to facilitate both design and engineering and City review. This includes (1) land use controls, (2) design guidelines, and (3) environmental mitigation measures. Deviations from the design guidelines outlined in Sections 3.2, 4.1c, 4.2c, 5.1c, 5.2, 6.2, and 7.2 of the Specific Plan should be addressed during Site Plan Review. Variances from the development standards of the applicable zoning district and signage standards are addressed in Chapter 8.

As indicated in Figure 9.1, Site Plan Review will usually occur simultaneously with tentative or parcel map review and approval. For vesting tentative maps or the vesting portions of a tentative map, Site Plan Review is similar in the scope of review required for vesting. Conditions of approval for tentative maps should reflect the Site Plan Review process.

9.2a Site Plan Review Application

In addition to the information ordinarily submitted with the tentative map or parcel map application, the following design information shall be submitted in the Site Plan Review package:

1. A grading plan showing proposed finished grades superimposed over the existing topography, drainage proposals, and boundary information.
2. A site plan, showing lot or site dimensions; traffic and pedestrian circulation; location, widths, grades and types of improvements proposed for all streets, parking areas and driveways, walkways, trails, utilities, and other public improvements; location of fencing, and loading and drop-off areas; location and grading of permanent community entrances and project entries.
3. Location and grades of existing and proposed buildings and structures, if development is multi-family attached housing or non-residential. Representative plot plans if project is single-family detached residential.
4. Representative building plans and elevations, and tabulation of proposed dwelling units if applicable.
5. Conceptual landscaping plan including plant palette; indication of areas to receive permanent irrigation; and existing trees and vegetation to be preserved.

9.3 Phasing Program

The phasing program for the Cottonwood Hills community is based on the following objectives:

- o Provide for coherent, orderly build-out of the community (as opposed to scattered, uncoordinated development) within the context of market conditions.
- o Insure adequate public facilities and services concurrent with private development.
- o Develop a range of housing opportunities at a variety of densities as the community develops, market conditions permitting.
- o Protect the public health, safety, and general welfare.

9.3a Development Phasing

While the build-out rate and order in which projects are developed are primarily a function of market conditions, a general phasing program can be projected. Figure 9.2 depicts a phasing program for the proposed residential and commercial uses in the specific plan area. Anticipated build-out would be approximately ten years, beginning in 1989/1990.

The phasing program for residential development considers the following factors:

- o Phasing of grading, and balancing of cut and fill to the extent practical.
- o Provision of adequate access to and from the specific plan areas as a whole.
- o Phasing of infrastructure in terms of availability and capacity, including community streets and utilities.
- o Marketing visibility and access.
- o Location of school and park district boundaries and the provision of schools and parks.
- o When possible, balancing residential products and the availability of units in a range of prices.

Table 9.1 summarizes the projected residential build-out and associated community population. Sites for attached residential development will be available in the phase shown, but may not be fully built-out depending on market conditions. Planning Units 1 and 2 are located in an "Open Phase," with development subject to market opportunities. Phases 5 and 6 are interchangeable, that is, they can occur in any sequence or concurrently, provided Cottonwood Hills Road/Holland Road is improved to the eastern boundary.

Commercial and institutional project sites will be available in the phase shown, but will be developed when adequate demand warrants. Planning

6. Representative fencing diagrams.
7. Location and tabulation of open space areas, including public, common area, and private open space.
8. Traffic noise analysis, if applicable.
9. Preliminary hydrology study for major flow areas and a preliminary hydraulic design for the storm drain system within the mapped area.

With the Phase I Site Plan Review Application, the Cottonwood Hills Community Signage Program shall be submitted. This program shall comply with Section 8.9 of this Plan, and shall provide standards for sign type, location, size, typeface, and materials for both permanent and temporary signage.

9.2b Administration

The Site Plan Review shall be conducted by the Planning Commission at a noticed public hearing. At this hearing, the Commission shall review the Site Plan Review package submitted, and may receive comments from the public concerning the proposed development and the manner in which it will effect the subject property and surrounding properties.

The Planning Commission shall make the following findings before approving a Site Plan Review Application:

1. The project as approved will comply with the goals and objectives of the General Plan, and the zoning district and development standards specified in the Specific Plan.
2. The project complies with the design guidelines outlined in the Specific Plan and other applicable provisions of the Municipal Code.
3. Conditions and safeguards pursuant to the Specific Plan, including guarantees and evidence of compliance, have been incorporated into the approval of the subject project.

The decision of the Planning Commission shall be final ten (10) days from the date of decision unless an appeal is filed with the City Council pursuant to the City procedures outlined in Chapter 17.80 of the Zoning Ordinance. A Site Plan Review approval shall lapse and become void concurrently with the map which the Site Plan Review accompanies.

- o To control siltation off-site, construct required on-site temporary or permanent drainage facilities concurrently with grading operations.
- o Utilize techniques to minimize erosion and control sediment transport during construction, such as sandbagging, berming, and jute matting.
- o Condition and plant all manufactured slopes greater than five feet in height to provide for rapid slope stabilization during and after construction.
- o In conducting grading operations, mitigate the impacts on abutting residential projects, such as noise and dust generation.

It is anticipated that areas designated to receive riparian habitat replacement will not always be available concurrently with habitat loss due to grading. Adequate coordination should be undertaken between phases to ensure mitigation of riparian habitat losses as prescribed in Section 6.2b.

9.3e Residential Monitoring Program

A monitoring program is necessary to track residential development in the Cottonwood Hills community. This program would serve three purposes:

- o To track residential development in terms of units with certificates of occupancy for purposes of administering the phasing program.
- o To monitor units approved in tentative maps by planning unit and for the overall specific plan area, to insure conformance with the unit transfer provision outlined in Section 3.3.
- o To assist in tracking students generated by the community as it is built out, to determine school facility needs.

The City of Lake Elsinore will establish and maintain the monitoring program. However, for the purpose of tracking approved dwelling units, the applicant shall provide a set of revised statistical tables and figures with each tentative map submission. The tables and figures should include any unit transfer revisions under the proposed tentative map, and should be labeled as to date and tentative map number. The items to be kept up-to-date by the applicant shall include:

Specific Plan Map
 Table 2.1 Land Use Tabulation
 Table 3.1 Housing Tabulation
 Table 5.1 Student Generation
 Table 9.1 Residential Phasing Program
 Table 9.2 Transportation Phasing Program

Unit 3 is situated in the "Open Phase"; while Planning Units 4 and 16, the neighborhood commercial sites, will be available for early development.

9.3b Transportation Phasing

Table 9.2 outlines the area-wide and community transportation facilities necessary in each of the development phases. Adequate community-level improvements are required in relation to building occupancy for each phase. Local and residential collector streets should be built in conjunction with project subdivisions.

Widening of Railroad Canyon Road to a minimum four-lane section is required at a cumulative 5,000 ADT for the community, or 500 detached dwelling units, whichever comes first. During construction of Railroad Canyon Road, access to Cottonwood Hills will be maintained from Canyon Lake (from the northeast) to Cottonwood Hills Road. This will ensure access for residents and police and fire protection services, as well as neighbors to the site who use Cottonwood Canyon and Lost Roads.

In order to maintain the existing access to the east, connections from improved Cottonwood Hills Road to the existing "Cottonwood Canyon Road/Holland Road" dirt road within the community are prescribed for Phases 1, 2, 3, and 4.

The provision of transit stops associated with commercial areas is addressed in Section 7.4.

9.3c Public Facilities and Services Phasing

Public facilities and services should be provided as needed as the community develops. Table 9.3 presents the phasing program for public facilities. Adequate area-wide and community-level improvements and actions are required in relation to residential unit occupancy and/or phase sequence.

Under the phasing program, the school construction items represent targets for building schools on-site to serve community residents. All sites are reserved concurrently with the phase in which they are located. During the course of community development, the appropriate school districts will determine if each site is required and when it will be developed. If a site is not needed, its alternate use is specified in Section 5.1b.

9.3d Phasing of Grading

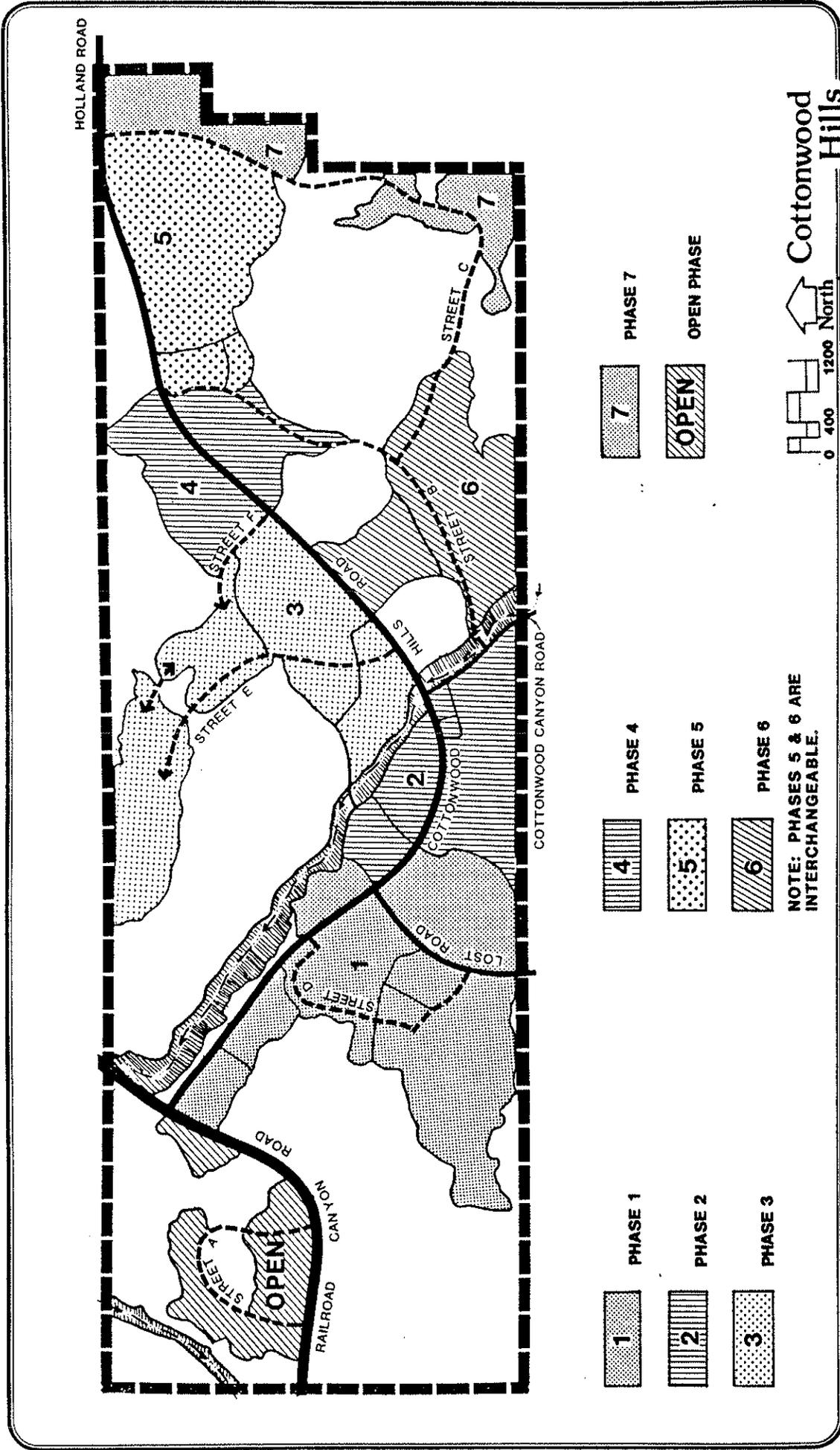
The guidelines below should be followed in phasing of grading operations:

- o Accomplish earth moving in stages, in order to avoid clearing the ground far in advance of grading.
- o To the extent practical, match mass grading to the phase shown in Figure 9.2 or to the subareas of these phases, balancing cut and fill within phases.

Table 9.1

RESIDENTIAL PHASING PROGRAM

	SF1 DU	SF2 DU	SF3 DU	MF1 DU	MF2 DU	Total Dwelling Units	Estimated Population	Estimated Elementary Students
<u>PHASE 1</u>								
Phase	-	431	221	109	216	977	2,609	391
Cumulative	-	431	221	109	216	977	2,609	391
<u>PHASE 2</u>								
Phase	-	218	-	166	398	782	2,088	313
Cumulative	-	649	221	275	614	1,759	4,697	704
<u>PHASE 3</u>								
Phase	-	300	233	-	-	533	1,423	213
Cumulative	-	949	454	275	614	2,292	6,120	917
<u>PHASE 4</u>								
Phase	-	168	188	-	-	356	951	146
Cumulative	-	1,117	642	275	614	2,648	7,071	1,063



Phasing Plan 9.2

Table 9.1 (continued)

RESIDENTIAL PHASING PROGRAM

	SF1 DU	SF2 DU	SF3 DU	MF1 DU	MF2 DU	Total Dwelling Units	Estimated Population	Estimated Elementary Students
<u>PHASE 5</u>								
Phase	-	379	-	-	-	379	1,012	155
Cumulative	-	1,496	642	275	614	3,027	8,083	1,218
<u>PHASE 6</u>								
Phase	-	397	-	-	-	397	1,060	159
Cumulative	-	1,893	642	275	614	3,424	9,143	1,377
<u>PHASE 7</u>								
Phase	93	-	-	-	-	93	248	38
Cumulative	93	1,893	642	275	614	3,517	9,391	1,415
<u>OPEN PHASE</u>								
Phase	-	-	-	274	484	758	2,024	303
Cumulative	93	1,893	642	549	1,098	4,275	11,415	1,718

Table 9.2

TRANSPORTATION PHASING PROGRAM

Phase (1)	Required Improvements	Timing
<u>PHASE 1</u>		
Cumulatives: 977 Dwelling Units 13.4 Acres of Commercial		
	<u>Area-Wide</u>	
	<ul style="list-style-type: none"> Widen Railroad Canyon Road to a minimum four-lane section, including modifications to Cottonwood Creek culvert, if required, and reclamation of sand and gravel operations areas. 	<ul style="list-style-type: none"> At 5,000 cumulative ADT or 500 detached dwelling units, whichever comes first.
	<u>Community</u>	
	<ul style="list-style-type: none"> Install signal at Railroad Canyon Road/Cottonwood Hills Road intersection. Construct Cottonwood Hills Road to a minimum two-lane section from Railroad Canyon Road to Lost Road. Construct Lost Road to a minimum two-lane section from Cottonwood Hills Road, tapering at the community boundary. Provide a temporary connection between Cottonwood Hills Road and the existing dirt road, "Cottonwood Canyon Road". 	<ul style="list-style-type: none"> In conjunction with widening Railroad Canyon Road. At occupancy of first dwelling unit in Phase 1. At occupancy of first dwelling unit in Planning Units 6, 9, or 10. At occupancy of first dwelling unit in Phase 1.
	<u>Local</u>	
	<ul style="list-style-type: none"> Build local and private streets within new subdivisions. 	<ul style="list-style-type: none"> With subdivision improvements.

(1) Cumulatives exclude Open Phase.

Table 9.2 (continued)

TRANSPORTATION PHASING PROGRAM

Phase (1)	Required Improvements	Timing
<u>PHASE 2</u>		
Cumulatives: 1,759 Dwelling Units 16.7 Acres of Commercial/Institutional	<p><u>Community</u></p> <ul style="list-style-type: none"> o Complete/construct Cottonwood Hills Road to full width from Railroad Canyon Road to Cottonwood Canyon Road. o Complete Lost Road to full four-lane width, from Cottonwood Hills Road, tapering at the community boundary. o Construct Cottonwood Canyon Road to full section, from Cottonwood Hills Road, tapering at the community boundary. o Provide a temporary connection between Cottonwood Hills Road and the existing dirt road, "Cottonwood Canyon Road". <p><u>Local</u></p> <ul style="list-style-type: none"> o Build local and private streets within new subdivisions. 	<ul style="list-style-type: none"> o At occupancy of first dwelling unit in Phase 2. o At occupancy of first dwelling unit in Planning Unit 14. o At occupancy of first dwelling unit in Planning Unit 14. o At occupancy of first dwelling unit in Planning Unit 12 or 13. <ul style="list-style-type: none"> o With subdivision improvements.
<u>PHASE 3</u>		
Cumulatives: 2,292 Dwelling Units 20.6 Acres of Commercial/Institutional	<p><u>Community</u></p> <ul style="list-style-type: none"> o Construct Cottonwood Hills Road to a minimum two-lane section from Cottonwood Canyon Road to Street "E". Construct Cottonwood Hills Road to a two-lane section with landscaping of right-of-way right-of-way from Street "E" to eastern entrance of Planning Unit 21 at Street "F". o Install signal at Cottonwood Hills Road/Lost Road intersection. o Provide a temporary connection between Cottonwood Hills Road and the existing dirt road, "Holland Road". <p><u>Local</u></p> <ul style="list-style-type: none"> o Build local and residential collector streets within new subdivisions. 	<ul style="list-style-type: none"> o At occupancy of first dwelling unit in Phase 3. o Same. o Same. <ul style="list-style-type: none"> o With subdivision improvements.

(1) Cumulatives exclude Open Phase.

Table 9.2 (continued)

TRANSPORTATION PHASING PROGRAM

Phase (1)	Required Improvements	Timing
<u>PHASE 4</u>	<p data-bbox="354 1402 375 1507"><u>Community</u></p> <ul style="list-style-type: none"> <li data-bbox="399 804 459 1507">o Construct Cottonwood Hills Road to a two-lane section with landscaping of right-of-way from eastern entrance of Planning Unit 21 at Street "f" to Street "B". <li data-bbox="483 804 524 1507">o Provide a temporary connection between Street "B" and the existing dirt road, "Holland Road". <p data-bbox="548 1455 570 1507"><u>Local</u></p> <ul style="list-style-type: none"> <li data-bbox="594 825 630 1507">o Build local and residential collector streets within new subdivisions. 	<ul style="list-style-type: none"> <li data-bbox="399 216 440 674">o At occupancy of first dwelling unit in Phase 4. <li data-bbox="483 594 505 674">o Same. <li data-bbox="594 310 613 674">o With subdivision improvements.
<u>PHASE 5</u>	<p data-bbox="678 1402 699 1507"><u>Community</u></p> <ul style="list-style-type: none"> <li data-bbox="740 783 781 1507">o Complete Cottonwood Hills Road to full width from Street "E" to Cottonwood Canyon Road. <li data-bbox="805 804 846 1507">o Construct Cottonwood Hills Road to a two-lane section with landscaping of right-of-way from Street "B" to Street "C". <li data-bbox="870 867 911 1507">o Construct Holland Road as half-width improvement from Street "C" to community boundary. <p data-bbox="935 1455 956 1507"><u>Local</u></p> <ul style="list-style-type: none"> <li data-bbox="980 825 1019 1507">o Build local and residential collector streets within new subdivisions. 	<ul style="list-style-type: none"> <li data-bbox="740 216 781 674">o At occupancy of first dwelling unit in Phase 5. <li data-bbox="805 594 826 674">o Same <li data-bbox="870 594 891 674">o Same <li data-bbox="980 310 1000 674">o With subdivision improvements.

NOTE: This phase may be interchanged with Phase 6.

(1) Cumulatives exclude Open Phase.

Table 9.2 (continued)

TRANSPORTATION PHASING PROGRAM

Phase (1)	Required Improvements	Timing
<u>PHASE 6</u>	<u>Community</u>	
Cumulatives: 3,424 Dwelling Units 20.6 Acres of Commercial/Institutional	<ul style="list-style-type: none"> o Construct Cottonwood Hills Road to a two-lane section with landscaping of right-of-way from Street "B" to Street "C". o Construct Holland Road as half-width improvement from Street "C" to community boundary. 	<ul style="list-style-type: none"> o At occupancy of first dwelling unit in Phase 6. o Same.
	<u>Local</u>	
NOTE: This phase may be interchanged with Phase 5.	<ul style="list-style-type: none"> o Build local and residential collector streets within new subdivisions. 	<ul style="list-style-type: none"> o With subdivision improvements.
	<u>Community</u>	
Cumulatives: 3,517 Dwelling Units 20.6 Acres of Commercial/Institutional	<ul style="list-style-type: none"> o Construct rural collector portion of Street "C". <p data-bbox="724 1461 745 1528"><u>Local</u></p> <ul style="list-style-type: none"> o Build local streets within new subdivisions. 	<ul style="list-style-type: none"> o At occupancy of first dwelling unit in Phase 7. o With subdivision improvements.
	<u>Local</u>	
Adds 758 Dwelling Units Adds 7.8 Acres of Commercial	<ul style="list-style-type: none"> o Build private and public streets within new subdivisions. 	<ul style="list-style-type: none"> o With subdivision improvements.

(1) Cumulatives exclude Open Phase.

Table 9.3

PUBLIC FACILITIES PHASING PROGRAM

Phase	Required Improvements/Actions	Timing
<u>PHASE 1</u>	<u>Area-Wide</u>	
	o Construct new trunk sewer connecting Railroad Canyon Road developments to existing trunk sewer west of Interstate 15.	o At occupancy of first dwelling unit in Phase 1, unless an interim connection is provided.
	<u>Community</u>	
	o Deed West Neighborhood Park site to City.	o At occupancy of first dwelling unit in Phase 1.
	o Reserve West Elementary School site for Lake Elsinore Unified School District.	
	o Deed Lower Cottonwood Creek (most of Open Space B1 and all of B2) to City.	
	o Install western community water facilities and connection to regional water system.	
	o Create Cottonwood Hills lighting and open space maintenance district, or equivalent.	
	<u>Local</u>	
	o Provide utilities and drainage facilities within new subdivisions and streets.	o With subdivisions.
	o Deed designated public natural open space to City.	

Table 9.3 - (continued)

PUBLIC FACILITIES PHASING PROGRAM

Phase	Required Improvements/Actions	Timing
<u>PHASE 2</u>	<p><u>Community</u></p> <ul style="list-style-type: none"> o Deed remainder of Lower Cottonwood Creek (Open Space B1) to City. o Deed Upper Cottonwood Creek in Parkway Park and within expanded parkway of Cottonwood Canyon Road (Open Space D) to City. o Build West Neighborhood Park. <p><u>Local</u></p> <ul style="list-style-type: none"> o Provide utilities and drainage facilities within new subdivisions and streets. o Deed designated public natural open space to City. 	<ul style="list-style-type: none"> o At occupancy of first dwelling unit in Phase 2, or 977 cumulative DU, whichever comes first. o With subdivisions.
<u>PHASE 3</u>	<p><u>Area-Wide</u></p> <ul style="list-style-type: none"> o Deed Community Park site to City, including Upper Cottonwood Creek (Open Space C). <p><u>Community</u></p> <ul style="list-style-type: none"> o Construct West Elementary School. o Reserve Central Elementary School site for Lake Elsinore Unified School District. o Install eastern community water facilities. o Modify school district boundaries to match logical road and subdivision boundaries. <p><u>Local</u></p> <ul style="list-style-type: none"> o Provide utilities and drainage facilities within new subdivisions and streets. o Deed designated public natural open space to City. 	<ul style="list-style-type: none"> o At occupancy of first dwelling unit in Phase 3. o At occupancy of first dwelling unit in Phase 3, or 1,759 cumulative DU, whichever comes first. o With subdivisions.

Table 9.3 - (continued)

PUBLIC FACILITIES PHASING PROGRAM

Phase	Required Improvements/Actions	Timing
<u>PHASE 4</u>	<p data-bbox="354 1388 370 1493"><u>Area-Wide</u></p> <ul style="list-style-type: none"> <li data-bbox="402 772 451 1493">o Build one-half of Cottonwood Hills' share of Community Park improvements. <p data-bbox="467 1430 483 1493"><u>Local</u></p> <ul style="list-style-type: none"> <li data-bbox="516 856 548 1493">o Provide utilities and drainage facilities within new subdivisions and streets. <li data-bbox="573 884 597 1493">o Deed designated public natural open space to City. 	<ul style="list-style-type: none"> <li data-bbox="402 163 451 653">o At occupancy of first dwelling unit in Phase 4, or 2,292 cumulative DU, whichever comes first. <li data-bbox="508 436 532 653">o With subdivisions.
<u>PHASE 5</u>	<p data-bbox="638 1367 654 1493"><u>Community 1</u></p> <ul style="list-style-type: none"> <li data-bbox="686 772 719 1493">o Reserve East Elementary School site for Menifee Union School District. <li data-bbox="743 989 768 1493">o Deed East Neighborhood Park site to City. <p data-bbox="784 1367 800 1493"><u>Community 2</u></p> <ul style="list-style-type: none"> <li data-bbox="833 1087 857 1493">o Construct East Neighborhood Park. <p data-bbox="889 1430 906 1493"><u>Local</u></p> <ul style="list-style-type: none"> <li data-bbox="938 856 979 1493">o Provide utilities and drainage facilities within new subdivisions and streets. <li data-bbox="1003 884 1027 1493">o Deed designated natural public open space to City. 	<ul style="list-style-type: none"> <li data-bbox="686 195 727 653">o At occupancy of first dwelling unit in Phase 5. <li data-bbox="833 163 898 653">o At occupancy of last dwelling unit in Phase 5, or 3,027 cumulative DU, whichever comes first. <li data-bbox="938 436 963 653">o With subdivisions.
<u>PHASE 6</u>	<p data-bbox="1044 1430 1060 1493"><u>Local</u></p> <ul style="list-style-type: none"> <li data-bbox="1092 856 1133 1493">o Provide utilities and drainage facilities within new subdivisions and streets. <li data-bbox="1157 884 1174 1493">o Deed designated public natural open space to City. 	<ul style="list-style-type: none"> <li data-bbox="1092 436 1117 653">o With subdivisions.

NOTE: This phase may be interchanged with Phase 6.

NOTE: This phase may be interchanged with Phase 5.

Table 9.3 - (continued)

PUBLIC FACILITIES PHASING PROGRAM

Phase	Required Improvements/Actions	Timing
<u>PHASE 7</u>	<p><u>Community</u></p> <ul style="list-style-type: none"> o Construct East Elementary School, if needed. <p><u>Local</u></p> <ul style="list-style-type: none"> o Provide utilities and drainage facilities within new subdivisions and streets. o Deed designated public natural open space to City. 	<ul style="list-style-type: none"> o At occupancy of last dwelling unit in Phase 7. o With subdivisions.
<u>OPEN PHASE</u>	<p><u>Area-Wide 1</u></p> <ul style="list-style-type: none"> o Deed San Jacinto River (Open Space A) to City. <p><u>Area-Wide 2</u></p> <ul style="list-style-type: none"> o Build one-half of Cottonwood Hills' share of Community Park improvements. <p><u>Community</u></p> <ul style="list-style-type: none"> o Construct Central Elementary School, if needed. <p><u>Local</u></p> <ul style="list-style-type: none"> o Provide utilities and drainage facilities within new subdivisions and streets. o Deed designated public natural open space to City. 	<ul style="list-style-type: none"> o At occupancy of first dwelling unit in Open Phase. o At occupancy of last dwelling unit in Open Phase. o At occupancy of last dwelling unit in Open Phase. o With subdivisions.

9.4 Facilities Financing Program

A facilities financing program is important to implementation of the Cottonwood Hills Specific Plan. The program should assure the timely financing of public facilities, streets, utilities, and other necessary capital improvements, and should provide for the subsequent maintenance of improvements.

There are a number of financing mechanisms available for funding and maintaining public facilities and services. Tables in the appropriate land use element outline the options available for public facilities, public services, open space, and circulation improvements.

9.4a Road Benefit District

Riverside County has created the Menifee Valley Road and Bridge Benefit District to fund widening and improvements to Railroad Canyon Road, Newport Road, and other regional facilities including the portion of the Railroad Canyon Road within Cottonwood Hills. The district includes the portion of Railroad Canyon Road West of Cottonwood Hills, but does not encompass interchange improvements at Interstate 15. The district is administered by the County and includes the entire Cottonwood Hills plan area. The amount of the assessment would be a cost per dwelling or equivalent, based on an estimate of the cost of improvements to be constructed as approved by the Board of Supervisors.

Cottonwood Hills will participate in the County District, or alternately, in a similar City district, to provide for the timely construction of Railroad Canyon Road.

9.4b City Development Agreement

A development agreement between the developer(s) of Cottonwood Hills and the City of Lake Elsinore shall be prepared and approved prior to recordation of the first Final Map. This agreement should specify the respective obligations of the City and developer in regard to the financing, phasing, provision and operation of public facilities and services, and the phasing and rights to private development.

Possible funding implementation mechanisms under the development agreement include the following:

- o Impact fees.
- o Mello-Roos district.
- o Other forms of assessment districts.
- o Facilities benefit assessment.
- o Conventional subdivision financing.
- o Turnkey construction.
- o Land reservation, dedication, deeding in fee, or easement.
- o Hook-up charges.
- o Reimbursement agreements.

Potential operations and maintenance measures include:

- o City general fund.
- o Special assessment or service districts.
- o Property owners association(s).
- o User fees.
- o Community-contributed service.

9.4c School Agreements

Prior to issuance of residential building permits, the project developer shall enter into an agreement(s) with the appropriate school district(s) to mitigate school impacts. This should establish the method, amount, and timing of school financing.

The options for development of the elementary school sites, including land and improvements, are outlined in Table 5.5a. It is anticipated that school fees will be paid for high school impacts as required under State law.

9.4d Community Lighting and Open Space Maintenance District

The Cottonwood Hills lighting and open space maintenance district shall be established to maintain public open space areas and provide lighting for streets and public areas. This district may exclude lighting and open space maintenance associated with Railroad Canyon Road. All property owners within the community will be assessed to provide district funding. The district should be created prior to issuance of the first building permit, and will be administered by the City.

As an alternative, the Cottonwood Hills community may be annexed into Lake Elsinore's City-wide Landscaping and Street Lighting District. Under this alternative, the community would constitute a separate zone with assessments appropriate to the level of maintenance and amount of improvements to be maintained in the specific plan area.

If for some reason the district approach proves impractical or unsatisfactory, maintenance responsibilities should revert to a master property owners association or master homeowners association.

9.4e Property Owner Associations

Property owner associations will be established on a planning unit basis as required. These associations are anticipated for the following projects:

- o Multi-family attached residential (MF1 and MF2) with condominium ownership.

- o Some single-family detached residential (SF1, SF2, SF3), where there are common area open spaces and/or facilities to be maintained and managed.
- o Commercial centers with multiple ownerships.

Generally, property owner associations are authorized to collect fees through project CC&R's (codes, covenants, and restrictions).

Appendices

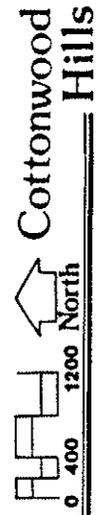
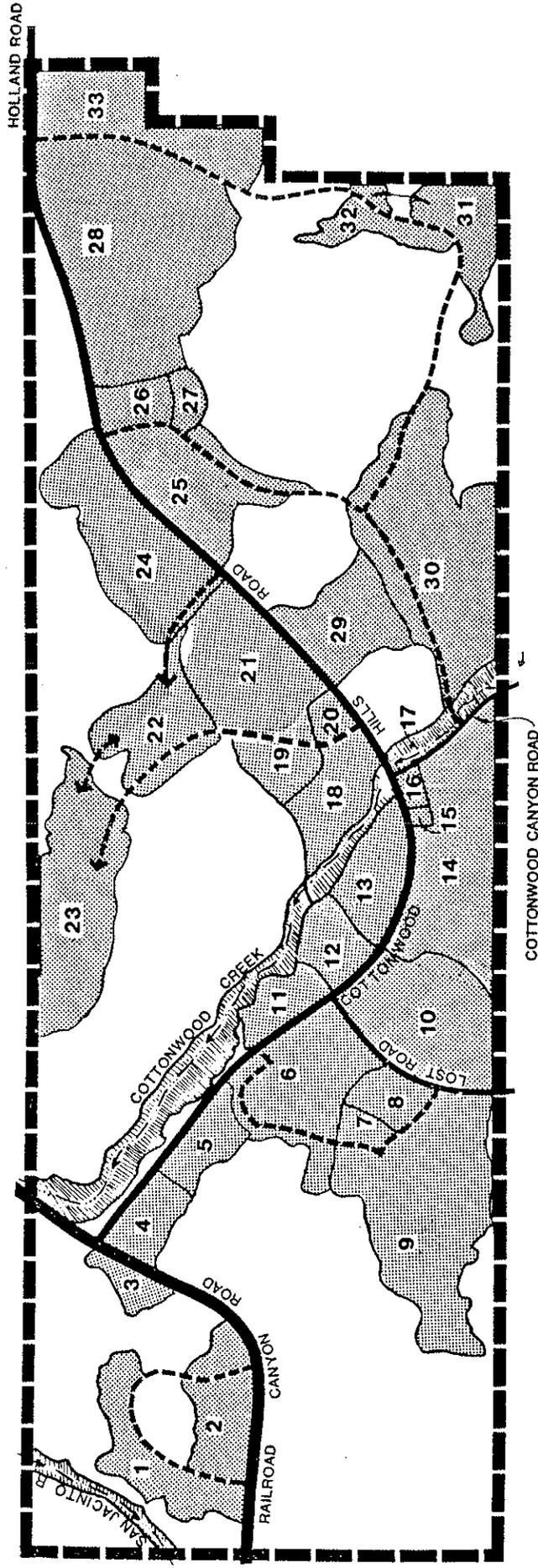
Planning Units

Chapter 10



Chapter 10 - PLANNING UNITS

This chapter summarizes planning information for each planning unit within the specific plan area. The locations of planning units are shown in Figure 10.1.



Planning Units 10.1

Project Design Consultants

3-14-89

10.1 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 1

Acres: 26.1
Density: 10.5 DU/AC
Dwelling Units: 274

Land Use: Multi-family Attached Residential 1 (MF1)
Possible Product(s): Attached—townhouses, patio homes, duplexes.

Zoning: MF1 District

Phasing: "Open Phase"
School District: Lake Elsinore Unified School District

Design Features:

- o Grading to preserve topographic mound at western boundary of specific plan area per Figure 6.5.
- o Daylighting next to San Jacinto River to extent practical.
- o Incorporation of cut slope variations, Figure 6.6.
- o Views of river and adjacent open space, and future estate lots in Canyon Lake Hills.
- o Preservation of Open Space "F" as common area in conjunction with Planning Unit 2.
- o Attached residential design guidelines, Table 3.3.
- o Minor road design, Section 7.1c.
- o Optional community entrance, Table 7.1a.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o San Jacinto River riparian area, Figure 3.5.
- o Design treatment for Street "A", Figure 7.10.
- o Parkway treatment for Railroad Canyon Road, Figure 7.4 and Table 7.1c.
- o Open space interfaces, Figure 6.2.

Access:

- o Residential collector Street "A", single-loaded through this project.
- o Median breaks in Railroad Canyon Road, per Section 7.1a.
- o Potential transit stop, Section 7.4.

Additional Mitigation Measures:

- o San Jacinto River riparian habitat to be preserved and buffered, Figure 3.5.
- o Mitigation of traffic noise from Railroad Canyon Road to acceptable level, Section 7.2d.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.2 COTTONWOOD HILLS SPECIFIC PLAN
Planning Unit 2

Acres: 26.9
Density: 18.0 DU/AC
Dwelling Units: 484

Land Use: Multi-family Attached Residential 2 (MF2)
Possible Product(s): Attached--walk-up apartments, condominiums.

Zoning: MF2 District

Phasing: "Open Phase"
School District: Lake Elsinore Unified School District

Design Features:

- o Grading to create large buildable pads, and elevation differentials with Railroad Canyon Road, Figure 6.7.
- o Incorporation of cut slope variations, Figure 6.6.
- o Views of adjacent open space.
- o Preservation of Open Space "F" as common area in conjunction with Planning Unit 1.
- o Attached residential design guidelines, Table 3.3.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Open space interfaces, Figure 6.2.
- o Parkway treatment for Railroad Canyon Road, Figure 7.4 and Table 7.1c.
- o Design treatment for Street "A", Figure 7.10.

Access:

- o Residential collector Street "A" as loop road.
- o Median breaks in Railroad Canyon Road, per Section 7.1a.
- o Potential transit stop, Section 7.4.

Additional Mitigation Measures:

- o Mitigation of on-site cultural resource, Section 6.7.
- o Mitigation of traffic noise from Railroad Canyon Road to acceptable level, Section 7.2d.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.3 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 3

Acres: 7.8 gross acres
Land Use: General Commercial (C2)
Zoning: C2 District
Phasing: "Open Phase"

Design Features:

- o Grading to create flat pad.
- o Grading to set pad low enough for visibility from Railroad Canyon Road.
- o Compatibility with Planning Unit 4, Table 4.2.
- o Commercial design guidelines, Table 4.2.
- o Incorporation of cut slope variations, Figure 6.5.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Open space interface, Figure 6.3. See also Table 4.2.
- o Parkway treatment for Railroad Canyon Road, Figure 7.4 and Table 7.1c.

Access:

- o Signal at intersection of Railroad Canyon Road and Cottonwood Hills Road per Section 7.1a, with access drive into project.
- o Secondary access from Railroad Canyon Road (right-in/right-out).
- o Potential transit stop, Section 7.4.
- o Restrictions on trail-related parking, Section 7.6b.
- o Bicycle parking, Table 4.2.

Additional Mitigation Measures:

- o Extensive shading of parking and pedestrian areas, Table 4.2.
- o Fire management zone, Section 6.2d.
- o Water conservation, Section 6.9.

10.4 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 4

Acres: 13.4 gross acres
Land Use: Neighborhood Commercial (C1)
Zoning: C1 District
Phasing: 1

Design Features:

- o Grading to create flat pad.
- o Grading to set pad low enough for visibility from Railroad Canyon Road.
- o Incorporation of cut slope variations, Figure 6.6.
- o Creation of identifiable community entrance, Table 7.1a.
- o Compatibility with Planning Unit 3, Table 4.2.
- o Commercial design guidelines, Table 4.2.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Open space interface, Figure 6.3. See also Table 4.2.
- o Parkway treatment for Railroad Canyon Road, Figure 7.4 and Table 7.1c.
- o Parkway treatment for Cottonwood Hills Road, Figure 7.5 and Table 7.1c.
- o Buffer treatment between commercial and other uses, Table 4.2.

Access:

- o Signal at intersection of Cottonwood Hills Road and Railroad Canyon Road, per Section 7.1a.
- o Access from Cottonwood Hills Road at median break and right-in/right-out; and from Railroad Canyon Road (right-in/right-out).
- o Potential transit stop, Section 7.4.
- o Restrictions on trail-related parking, Section 7.6b.
- o Bicycle parking, Table 4.2.

Special Mitigation Measures:

- o Extensive shading of parking and pedestrian areas, Table 4.2.
- o Fire management zone, Section 6.2d.
- o Water conservation, Section 6.9.

10.5 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 5

Acres: 12.8
Density: 16.9 DU/AC
Dwelling Units: 216

Land Use: Multi-family Attached Residential 2 (MF2)
Possible Product(s): Attached--walk-up apartments, condominiums.

Zoning: MF2 District

Phasing: 1
School District: Lake Elsinore Unified School District

Design Features:

- o Grading to create large buildable pad(s).
- o Grading to produce elevation differentials with Cottonwood Hills Road, Figure 6.7.
- o Incorporation of cut slope variations, Figure 6.6.
- o Views of riparian area and adjacent open space along Cottonwood Creek.
- o Attached residential design guidelines, Table 3.3.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Open space interface, Figure 6.2.
- o Parkway treatment for Cottonwood Hills Road, Figure 7.5 and Table 7.1c.
- o Buffer treatment between commercial and other land uses, Table 4.2.
- o Buffer treatment between attached and detached residential, Table 3.3.

Access:

- o Median breaks in Cottonwood Hills Road, per Section 7.1b.

Additional Mitigation Measures:

- o Mitigation of traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Mitigation of on-site cultural resource, Section 6.7.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.6 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 6

Acres: 46.1
Density: 4.8 DU/AC
Dwelling Units: 221

Land Use: Single-family Detached Residential 3 (SF3)
Possible Product(s): Small lot detached.

Zoning: SF3 District

Phasing: 1
School District: Lake Elsinore Unified School District

Design Features:

- o Terraced grading per Figure 6.7.
- o Views of riparian area and adjacent open space along Cottonwood Creek.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Open space interface, Figure 6.2.
- o Buffer treatment between detached residential uses, Table 3.2.
- o Parkway treatment for Cottonwood Hills Road, Figure 7.5 and Lost Road, Figure 7.9; and Table 7.1c.
- o Design treatment for Street "D", Figure 7.12.

Access:

- o Median breaks at Cottonwood Hills Road, per Section 7.1b.
- o Access from Lost Road.
- o Street "D" runs through project.
- o Provision of access from project into West Neighborhood Park and West Elementary School.

Additional Mitigation Measures:

- o Significant rock outcrops to be preserved where feasible.
- o Mitigation of on-site cultural resource, Section 6.7.
- o Mitigation of traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.7 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 7

Acres: 5.0 gross acres
Land Use: West Neighborhood Park (NP)
Zoning: OS District
Phasing: 1

Design Features:

- o View of riparian area and central promontory.
- o Grading to create large flat pad area(s).
- o Coordination with West Elementary School (Planning Unit 8).
- o Neighborhood park design guidelines, Table 5.3.

Special Interfaces:

- o Design treatment for Street "D", Figure 7.12.
- o Buffer treatment between neighborhood park and detached residential uses, Table 5.3.

Access:

- o Access from linkage Street "D".
- o Possible indirect access from West Elementary School.
- o Bicycle parking, Table 5.3.

Additional Mitigation Measures:

- o Mitigation of on-site cultural resource, Section 6.7.
- o Water conservation, Section 6.9.

10.8 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 8

Acres: 10.6 gross acres
10.0 net acres

Land Use: West Elementary School (ES)
Alternate Land Use: 5.0 acres Neighborhood Park (NP) and remainder as
Single-family Detached Residential 3 (SF3)

Zoning: SF3 District

Phasing: 1
School District: Lake Elsinore Unified School District

Design Features:

- o Grading to create large flat pad area(s).
- o Coordination with West Neighborhood Park (Planning Unit 7).
- o Elementary school design guidelines, Table 5.2.

Special Interfaces:

- o Design treatment for Street "D" , Figure 7.12.
- o Parkway treatment for Lost Road, Figure 7.9 and Table 7.1c.
- o Interface with adjacent residential, Table 5.2.

Access:

- o Access from Lost Road via Street "D".
- o Possible indirect access from West Neighborhood Park.
- o Sidewalks from surrounding residential.
- o Bicycle parking, Table 5.2.

Additional Mitigation Measures:

- o Water conservation, Section 6.9.

10.9 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 9

Acres: 76.4
Density: 3.6 DU/AC
Dwelling Units: 275

Land Use: Single-family Detached Residential 2 (SF2)
Possible Product(s): 7,260 sq. ft. lot detached.

Zoning: SF2 District

Phasing: 1
School District: Lake Elsinore Unified School District

Design Features:

- o Terraced grading per Figure 6.7.
- o Incorporation of cut slope variations, Figure 6.6.
- o Views of adjacent open space, creek area, and mountains off-site.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Design treatment for Street "D", Figure 7.12.
- o Parkway treatment for Lost Road, Figure 7.9 and Table 7.1c.
- o Open space interfaces, Figure 6.2.
- o Off-site interface separating detached residential and adjacent use, Figure 3.4 (Section c).

Access:

- o Access from Lost Road via Street "D" and other local street(s).
- o Indirect access from Cottonwood Hills Road via Street "D".

Additional Mitigation Measures:

- o Significant rock outcrops to be preserved where possible.
- o Mitigation of on-site biological resource, Section 6.8.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.10 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 10

Acres: 45.4

Density: 3.4 DU/AC

Dwelling Units: 156

Land Use: Single-family Detached Residential 2 (SF2)

Possible Product(s): 7,260 sq.ft lot detached.

Zoning: SF2 District

Phasing: 1

School District: Lake Elsinore Unified School District

Design Features:

- o Terraced grading per Figure 6.7.
- o Views of riparian area and open space.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Parkway treatment for Lost Road, Figure 7.9 and Cottonwood Hills Road, Figure 7.5; and Table 7.1c.
- o Off-site interface separating detached residential and adjacent use, Figure 3.4 (Section c).

Access:

- o Median break at Cottonwood Hills Road per Section 7.1b.
- o Access from Lost Road.
- o Local street linkage to Planning Unit 14.

Additional Mitigation Measures:

- o Mitigation of traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.11 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 11

Acres: 10.9
Density: 10.0 DU/AC
Dwelling Units: 109

Land Use: Multi-family Attached Residential 1 (MF1)
Possible Product(s): Attached--townhouses, patio homes, duplexes.

Zoning: MF1 District

Phasing: 1
School District: Lake Elsinore Unified School District

Design Features:

- o Grading to create large buildable pads.
- o Daylighting or minor cuts or fills next to riparian area.
- o Views of creek riparian area and adjacent open space.
- o Attached residential design guidelines, Table 3.3.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Parkway treatment for Cottonwood Hills Road, Figure 7.5 and Table 7.1c.
- o Creek riparian area, Figure 3.6.
- o Buffer treatment between attached residential uses, Table 3.3.

Access:

- o Median break and possible right-in/right-out on Cottonwood Hills Road, per Section 7.1b.
- o Possible combined entry with Planning Unit 12 at signalized intersection of Cottonwood Hills Road and Lost Road.

Additional Mitigation Measures:

- o Cottonwood Creek riparian habitat to be preserved and buffered.
- o Mitigation of traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.12 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 12

Acres: 13.8
Density: 12.0 DU/AC
Dwelling Units: 166

Land Use: Multi-family Attached Residential 1 (MF1)
Possible Product(s): Attached—townhouses, patio homes, duplexes.

Zoning: MF1 District

Phasing: 2
School District: Lake Elsinore Unified School District

Design Features:

- o Grading to create large buildable pads.
- o Daylighting or minor fills next to riparian area.
- o Views of creek riparian areas and adjacent open space.
- o Attached residential design guidelines, Table 3.3.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Parkway treatment for Cottonwood Hills Road, Figure 7.5 and Table 7.1c.
- o Creek riparian area, Figure 3.6.
- o Buffer treatment between attached residential uses, Table 3.3.

Access:

- o Possible combined entry with Planning Unit 11 at signalized intersection of Cottonwood Hills Road and Lost Road.
- o Possible right-in/right-out access from Cottonwood Hills Road.

Additional Mitigation Measures:

- o Cottonwood Creek riparian habitat to be preserved and buffered.
- o Mitigation of traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.13 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 13

Acres: 19.9
Density: 20.0 DU/AC
Dwelling Units: 398

Land Use: Multi-family Attached Residential 2 (MF2)
Possible Product(s): Attached--walk-up apartments, condominiums.

Zoning: MF2 District

Phasing: 2
School District: Lake Elsinore Unified School District

Design Features:

- o Grading to create large buildable pads.
- o Daylighting or minor fills next to riparian area.
- o Views of creek riparian area and adjacent open space.
- o Attached residential design guidelines, Table 3.3.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Parkway treatment for Cottonwood Hills Road, 7.5 and Table 7.1c.
- o Creek riparian area, Figure 3.6.
- o Buffer treatment between attached residential uses, Table 3.3.

Access:

- o Median break and possible right-in/right-out on Cottonwood Hills Road, per Section 7.1b.

Additional Mitigation Measures:

- o Cottonwood Creek riparian habitat to be preserved and buffered.
- o Mitigation of traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.14 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 14

Acres: 58.9
Density: 3.7 DU/AC
Dwelling Units: 218

Land Use: Single-family Detached Residential 2 (SF2)
Possible Product(s): 7,260 sq. ft. lot, detached.

Zoning: SF2 District

Phasing: 2
School District: Lake Elsinore Unified School District

Design Features:

- o Terraced grading where possible, per Figure 6.7.
- o Views of creek, riparian area, and adjacent open space.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Parkway treatment for Cottonwood Hills Road, Figure 7.5 and Table 7.1c.
- o Parkway treatment for Cottonwood Canyon Road, Figure 7.8 and Table 7.1c.
- o Off-site interface separating detached residential and adjacent use, Figure 3.4 (Sections c and d).
- o Buffer treatment between detached residential and commercial uses, Table 3.2.

Access:

- o Median break(s) in Cottonwood Hills Road, per Section 7.1b.
- o Access from Cottonwood Canyon Road.
- o Local street linkage to Planning Unit 10.
- o Local street linkage to property to south of specific plan area, providing access in addition to Cottonwood Canyon Road.

Additional Mitigation Measures:

- o Mitigation of traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Fire management zone as required, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.15 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 15

Acres: 1.3 gross acres
Land Use: Institutional (IN)
Alternate Land Use: Neighborhood Commercial (C1)
Zoning: C1 District
Alternate Zoning: C1 District
Phasing: 2

Design Features:

- o Grading to create flat pad and elevation differential from Planning Unit 14.
- o Compatibility with adjacent commercial area.
- o Institutional design guidelines, Table 4.3.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Parkway treatment for Cottonwood Hills Road, Figure 7.5 and Table 7.1c.
- o Buffer treatment between institutional and detached residential use, Table 4.3.

Access:

- o Directly from local street at median break in Cottonwood Hills Road, per Section 7.1b.
- o Possible right-in/right-out access from Cottonwood Hills Road.

Additional Mitigation Measures:

- o Extensive shading of parking and pedestrian areas, Table 4.3.
- o Water conservation, Section 6.9.

10.16 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 16

Acres: 2.0 gross acres
Land Use: Neighborhood Commercial (C1)
Zoning: C1 District
Phasing: 2

Design Features:

- o Grading to create flat pad and elevation differential from Planning Unit 14.
- o Grading to set pad low enough for visibility from Cottonwood Hills Road and Cottonwood Canyon Road.
- o Compatibility with institutional site.
- o Commercial design guidelines, Table 4.2.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Parkway treatment for Cottonwood Hills Road, Figure 7.5 and Table 7.1c.
- o Parkway treatment for Cottonwood Canyon Road, Figure 7.8 and Table 7.1c.
- o Buffer treatment between commercial and detached residential use, Table 4.2.
- o Interface with non-residential use, Table 4.2.

Access:

- o Direct access from Cottonwood Canyon Road.
- o Right-in/right-out access from Cottonwood Hills Road.
- o Potential transit stop, Section 7.4.
- o Bicycle parking, Table 4.2.

Additional Mitigation Measures:

- o Extensive shading of parking and pedestrian areas, Table 4.2.
- o Water conservation, Section 6.9.

10.17 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 17

Acres: 4.4. gross acres
Land Use: Parkway Park (PP)
Zoning: OS District
Phasing: 2

Design Features:

- o Regrading and enhancement of Cottonwood Creek as riparian habitat and visual resource.
- o Minimize grading into adjacent topographic mound, Section 6.4.

Special Interfaces:

- o Parkway treatment for Cottonwood Hills Road, Figure 7.5 and Table 7.1c.
- o Parkway treatment for Cottonwood Canyon Road, Figure 7.8 and Table 7.1c.

Access:

- o Hiking/riding trail corridor parallel to Cottonwood Creek, Section 7.6b.

Additional Mitigation Measures:

- o Riparian replacement, Section 6.2b.
- o Water conservation, Section 6.9.

10.18 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 18

Acres: 27.7 gross acres
Land Use: Community Park (CP)
Zoning: OS District
Phasing: 3

Design Features:

- o Grading to create large flat pad(s) for active recreation in eastern portion of site.
- o Preservation and enhancement of Cottonwood Creek as riparian habitat and visual resource.
- o Incorporation of cut slope variations, Figure 6.6.
- o Coordination with Central Elementary School (Planning Unit 19), Table 5.4.
- o Community park design guidelines, Table 5.4.

Special Interfaces:

- o Parkway treatment for Cottonwood Hills Road, Figure 7.5 and Table 7.1c.
- o Design treatment for Street "E", Figure 7.10.

Access:

- o Direct access from residential collector Street "E".
- o Possible direct access from Cottonwood Hills Road.
- o Possible indirect access from Central Elementary School.
- o Special walkway from Street "E" through school/park complex to pedestrian/equestrian trail corridor.
- o Pedestrian/equestrian trail corridor parallel to Cottonwood Creek, Section 7.6b.
- o Potential transit stop, Section 7.4.
- o Bicycle parking, Table 5.4.

Additional Mitigation Measures:

- o Riparian replacement, Section 6.2b.
- o Water conservation, Section 6.9.

10.19 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 19

Acres: 11.3 gross acres
10.0 net acres

Land Use: Central Elementary School (ES)
Alternate Use: 5.0 acres Community Park (CP) and remainder as
Single-family Detached Residential 3 (SF3)

Zoning: SF3 District

Phasing: 3
School District: Lake Elsinore Unified School District

Design Features:

- o Grading to create large flat pad area(s).
- o Incorporation of cut slope variations, Figure 6.6.
- o Coordination with Community Park (Planning Unit 18).
- o View of adjacent open space.
- o Elementary school design guidelines, Table 5.2.

Special Interfaces:

- o Design treatment for Street "E", Figure 7.10.

Access:

- o Directly from residential collector Street "E".
- o Possible indirect access from Community Park.
- o Special walkway from Street "E" through school/park complex to pedestrian/equestrian trail corridor in Community Park.
- o Bicycle parking, Table 5.2.

Additional Mitigation Measures:

- o Water conservation, Section 6.9.

10.20 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 20

Acres: 3.9 gross acres, one or two sites.

Land Use: Institutional (IN)
Alternate Land Use: Single-family Detached Residential 2 (SF2)

Zoning: C1 District
Alternate Zoning: SF3 District

Phasing: 3

Design Features:

- o Grading to create flat pad area(s).
- o High visibility from Cottonwood Hills Road.
- o Institutional design guidelines, Table 4.3.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Parkway treatment for Cottonwood Hills Road, Figure 7.5 and Table 7.1c.
- o Design treatment for Street "E", Figure 7.10.
- o Buffer treatment between institutional use and residential areas, Table 4.3.

Access:

- o Directly from residential collector Street "E".
- o Possible right-in/right-out from Cottonwood Hills Road.

Additional Mitigation Measures:

- o Extensive shading of parking and pedestrian areas, Table 4.3.
- o Water conservation, Section 6.9.

10.21 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 21

Acres: 46.5
Density: 5.0 DU/AC
Dwelling Units: 233

Land Use: Single-family Detached Residential 3 (SF3)
Possible Product(s): Small lot detached.

Zoning: SF3 District

Phasing: 3
School District: Lake Elsinore Unified School District

Design Features:

- o Terraced grading per Figure 6.7.
- o Possible provision for community storm drain and sewer lines threaded through project.
- o Views of nearby natural open space.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Parkway treatment for Cottonwood Hills Road, Figure 7.6 and Table 7.1c.
- o Design treatment for residential collector Streets "E" and "F", Figure 7.10.
- o Buffer treatment between detached residential uses and next to institutional, Table 3.2.

Access:

- o Indirectly from median breaks in Cottonwood Hills Road, at Street "E" and Street "F", per Section 7.1b.
- o Directly from residential collector Street "E" and Street "F".
- o Provision for access to Planning Unit 20 if converted from IN to SF3 use.

Additional Mitigation Measures:

- o Mitigation of on-site cultural resource, Section 6.7.
- o Mitigation of possible traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.22 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 22

Acres: 30.0
Density: 3.4 DU/AC
Dwelling Units: 102

Land Use: Single-family Detached Residential 2 (SF2)
Possible Product(s): 7,260 sq. ft. lot, detached.

Zoning: SF2 District

Phasing: 3
School District: Lake Elsinore Unified School District

Design Features:

- o Terraced grading per Figure 6.7.
- o Incorporation of cut slope variations, Figure 6.6.
- o Views of adjacent open space.
- o Preservation of Open Space "J" as common area in conjunction with Planning Unit 23.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Open space interfaces, Figure 6.2.
- o Buffer treatment between detached residential uses, Table 3.2.
- o Design treatment for Street "E" and Street "F", Figure 7.10.

Access:

- o Directly from residential collector Streets "E" and "F".

Additional Mitigation Measures:

- o Mitigation of historic cemetery, Section 6.7.
- o Mitigation of on-site biological resource, Section 6.8.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.23 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 23

Acres: 55.0
Density: 3.6 DU/AC
Dwelling Units: 198

Land Use: Single-family detached Residential 2 (SF2)
Possible Product(s): 7,260 sq. ft. lot, detached.

Zoning: SF2 District

Phasing: 3
School District: Lake Elsinore Unified School District

Design Features:

- o Terraced grading per Figure 6.7.
- o Incorporation of cut slope variations, Figure 6.6.
- o Views of adjacent open space and Canyon Lake community.
- o Preservation of Open Space "J" as common area in conjunction with Planning Unit 22.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Open space interfaces, Figure 6.2.
- o Off-site interface separating detached residential and adjacent uses, Figure 3.3 (Section a).
- o Design treatment for Street "E", Figure 7.10.

Access:

- o Directly from residential collectors Streets "E" and "F".

Additional Mitigation Measures:

- o Mitigation of historic cemetery, Section 6.7.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.24 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 24

Acres: 46.8
Density: 3.6 DU/AC
Dwelling Units: 168

Land Use: Single-family Detached Residential 2 (SF2)
Possible Product(s): 7,260 sq. ft. lot, detached.

Zoning: SF2 District

Phasing: 4
School Districts: Menifee Union School District
Perris Union High School District

Design Features:

- o Terraced grading per Figure 6.7.
- o Incorporation of cut slope variations, Figure 6.6.
- o Possible provision for community storm drain and sewer lines threaded through project.
- o Views of adjacent open space and across valley.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Open space interfaces, Figure 6.2.
- o Parkway treatment for Cottonwood Hills Road, Figure 7.6 and Table 7.1c.
- o Off-site interface separating detached residential and adjacent use, similar to Figure 3.4 (Section c).

Access:

- o Access from median breaks in Cottonwood Hills Road, onto local streets and onto Street "F" linking Planning Units 21, 22, 23, and 24.

Additional Mitigation Measures:

- o Mitigation of possible traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.25 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 25

Acres: 36.9
Density: 5.1 DU/AC
Dwelling Units: 188

Land Use: Single-family Detached Residential 3 (SF3)
Possible Product(s): Small lot detached.

Zoning: SF3 District

Phasing: 4
School Districts: Menifee Union School District
Perris Union High School District

Design Features:

- o Terraced grading per Figure 6.7.
- o Incorporation of cut slope variations, Figure 6.6.
- o Views of adjacent open space and across valley.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Open space interfaces, Figure 6.2.
- o Parkway treatment for Cottonwood Hills Road, Figure 7.6 and Table 7.1c.
- o Design treatment for Street "B", Figure 7.10.

Access:

- o From median breaks in Cottonwood Hills Road, one of which at Street "B", see Section 7.1b. Possible right-in/right-out(s).
- o Directly from residential collector Street "B".

Additional Mitigation Measures:

- o Mitigation of possible traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.26 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 26

Acres: 10.7 gross acres
10.0 net acres

Land Use: East Elementary School (ES)
Alternate Use: 5.0 acres Neighborhood Park (NP) and remainder as
Single-family Detached Residential 2 (SF2)

Zoning: SF2 District

Phasing: 5
School District: Menifee Union School District

Design Features:

- o Grading to create large flat pad area(s).
- o Coordination with East Neighborhood Park (Planning Unit 27).
- o Elementary school design guidelines, Table 5.2.
- o Possible provision for community storm drain and sewer lines threaded through project.

Special Interfaces:

- o Parkway treatment for Cottonwood Hills Road, Figure 7.6 and Table 7.1c.
- o Design treatment for Street "B", Figure 7.10.
- o Buffer treatment between elementary school and detached residential use, Table 5.2.

Access:

- o Access from Cottonwood Hills Road via residential collector Street "B".
- o Possible indirect access from East Neighborhood Park.
- o Special walkway through Planning Unit 28 to provide pedestrian access to school/park complex, Section 7.6a.
- o Potential transit stop, Section 7.4.
- o Bicycle parking, Table 5.2.

Additional Mitigation Measures:

- o Water conservation, Section 6.9.

10.27 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 27

Acres: 5.0 gross acres
Land Use: East Neighborhood Park (NP)
Zoning: OS District
Phasing: 5

Design Features:

- o Grading to create large flat pad area(s).
- o Incorporation of cut slope variations, Figure 6.6.
- o Coordination with East Elementary School (Planning Unit 26).
- o Neighborhood park design guidelines, Table 5.3.

Special Interfaces:

- o Design treatment for Street "B", Figure 7.10.
- o Buffer treatment between neighborhood park and detached residential uses, Table 5.3.

Access:

- o Direct access from residential collector Street "B".
- o Possible indirect access from East Elementary School.
- o Special walkway through detached residential (Planning Unit 28) to provide pedestrian access, Section 7.6a.
- o Bicycle parking, Table 5.3.

Additional Mitigation Measures:

- o Water conservation, Section 6.9.

10.28 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 28

Acres: 105.3
Density: 3.6 DU/AC
Dwelling Units: 379

Land Use: Single-family Detached Residential 2 (SF2)
Possible Product(s): 7,260 sq. ft. lot, detached.

Zoning: SF2 District

Phasing: 5
School Districts: Menifee Union School District
Perris Union High School District

Design Features:

- o Terraced grading per Figure 6.7.
- o Possible provision for community storm drain and sewer lines threaded through project.
- o Incorporation of cut slope variations, Figure 6.6.
- o Views of adjacent open space and down valley.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces

- o Parkway treatment for Cottonwood Hills Road, Figure 7.6 and Table 7.1c.
- o Design treatment for Street "C", Figure 7.10.

Access:

- o From median breaks in Cottonwood Hills Road, per Section 7.1b. Possible right-in/right-out(s).
- o Directly from Street "C", coordinating intersections with Planning Unit 33.
- o Special walkway through project to provide pedestrian access to east school/park complex, Section 7.6a.

Additional Mitigation Measures:

- o Mitigation of possible traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9

10.29 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 29

Acres: 29.0
Density: 3.5 DU/AC
Dwelling Units: 102

Land Use: Single-family Detached Residential 2 (SF2)
Possible Product(s): 7,260 sq. ft. lot, detached.

Zoning: SF2 District

Phasing: 6
School District: Lake Elsinore Unified School District

Design Features:

- o Terraced grading per Figure 6.7.
- o Preservation of major topographic features, Figure 6.5.
- o Incorporation of cut slope variations, Figure 6.6.
- o Views of nearby open space and creek areas.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Parkway treatment for Cottonwood Hills Road, Figure 7.6 and Table 7.1c.
- o Open space interfaces, Figure 6.2.
- o Coordination with design of Planning Unit 30.

Access:

- o From median break in Cottonwood Hills Road, per Section 7.1b. Possible right-in/right out(s).
- o From residential collector Street "B", from Cottonwood Canyon Road creek crossing, through Planning Units 30 and 25, to Cottonwood Hills Road.

Additional Mitigation Measures:

- o Mitigation of possible traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.30 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 30

Acres: 81.9
Density: 3.6 DU/AC
Dwelling Units: 295

Land Use: Single-family Detached Residential 2 (SF2)
Possible Product(s): 7,260 sq. ft. lot, detached.

Zoning: SF2 District

Phasing: 6
School District: Lake Elsinore Unified School District

Design Features:

- o Daylighting next to enhanced creek area to extent practical.
- o Terraced grading per Figure 6.7.
- o Preservation for major topographic features, Figure 6.5.
- o Incorporation of cut slope variations, Figure 6.6.
- o Views of adjacent natural open space and creek areas; screening of views off-site to south.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Open space interfaces, Figure 6.2.
- o Design treatment for Streets "B" and "C", Figure 7.10.
- o Off-site interface between detached residential and adjacent land use, Figures 3.3 and 3.4 (Section d, similar to Section a).
- o Coordination with upper Cottonwood Creek enhancement, Section 6.2b.
- o Coordination with design of Planning Unit 29.

Access:

- o Street "B" from Cottonwood Canyon road creek crossing, through project, onto Planning Unit 25, to Cottonwood Hills Road.
- o Street "C" from Street "B" through project, onto Planning Units 31, 32, and 28/33, to Holland Road.

Additional Mitigation Measures:

- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.31 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 31

Acres: 21.9
Density: 1.2 DU/AC
Dwelling Units: 26 DU

Land Use: Single-family Detached Residential 1 (SF1)
Possible Product(s): 1/2-acre estate lot, detached.

Zoning: SF1 District

Phasing: 7
School Districts: Menifee Union School District
Perris Union High School District

Design Features:

- o Grading to create a building pad on each lot, emphasizing daylighting.
- o Views of adjacent natural open space and off-site.
- o Preservation of Open Space "N" as common area or lotted out with open space easement.
- o Detailed residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Design treatment for Street "C", Figure 7.11.
- o Off-site interface separating detached residential and adjacent use, Figure 3.3 (Section b) and similar to Figure 3.4 (Section c). Buffer along the specific plan area boundary a minimum of fifty (50) feet in width, Table 3.2.
- o Open space interface, Figure 6.2.

Access:

- o Local streets off of Street "C".
- o Lots fronting on Street "C".

Additional Mitigation Measures:

- o Significant rock outcrops to be preserved where possible.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.32 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 32

Acres: 9.8
Density: 1.3 DU/AC
Dwelling Units: 13

Land Use: Single-family Detached Residential 1 (SF1)
Possible Product(s): 1/2-acre estate lot, detached.

Zoning: SF1 District

Phasing: 7
School Districts: Menifee Union School District
Perris Union High School District

Design Features:

- o Grading to create a building pad on each lot.
- o Incorporation of cut slope variations, Figure 6.6.
- o Views of adjacent natural open space.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

- o Design treatment for Street "C", Figure 7.11.
- o Open space interface, Figure 6.2.

Access:

- o Local street(s) off of Street "C".
- o Lots fronting on Street "C".

Additional Mitigation Measures:

- o Significant rock outcrops to be preserved where possible.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.

10.33 COTTONWOOD HILLS SPECIFIC PLAN
PLANNING UNIT 33

Acres: 37.8
Density: 1.4 DU/AC
Dwelling Units: 54

Land Use: Single-family Detached Residential 1 (SF1)
Possible Product(s): 1/2-acre estate lot, detached.

Zoning: SF1 District

Phasing: 7
School Districts: Menifee Union School District
Perris Union High School District

Design Features:

- o Terraced grading in southern portion, per Figure 6.7.
- o Minimal grading to retain existing land form in northern portion.
- o Views of adjacent natural open space and off-site.
- o Detached residential design guidelines, Table 3.2.
- o Minor road design, Section 7.1c.
- o Fire management program, Section 6.2d.

Special Interfaces:

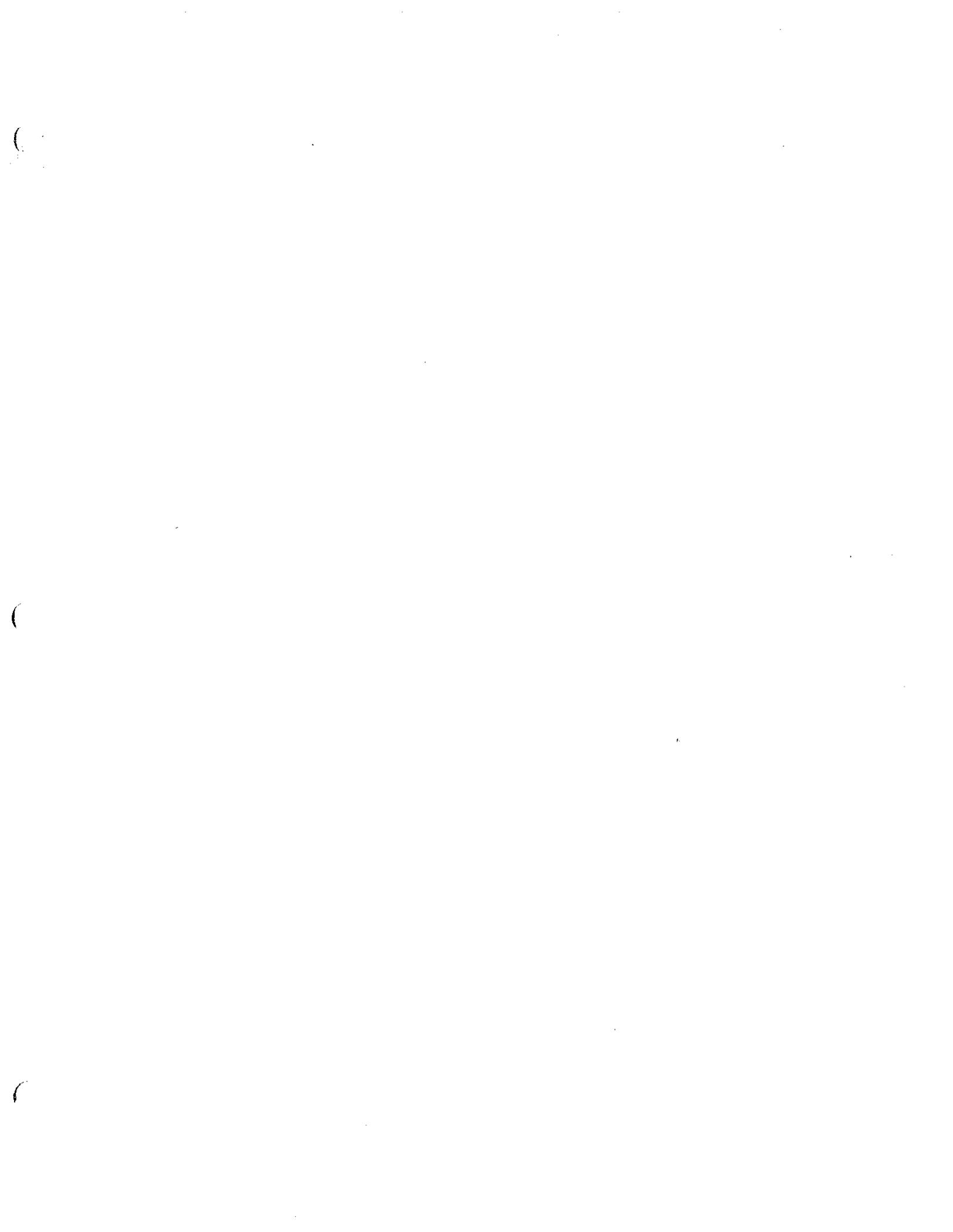
- o Design treatment for Street "C", Figures 7.10 and 7.11.
- o Parkway treatment for Holland Road, Figure 7.7 and Table 7.1c.
- o Off-site interface separating detached residential and adjacent use, Figure 3.3 (Section b). Buffer along the specific plan area boundary a minimum of fifty (50) feet in width, Table 3.2.
- o Open space interface, Figure 6.2.

Access:

- o Local streets off of Street "C". No access from Holland Road.
- o Lots fronting on Street "C".
- o Coordination of Street "C" intersections with Planning Unit 28.

Additional Mitigation Measures:

- o Significant rock outcrops to be preserved where possible.
- o Mitigation of on-site cultural resource, Section 6.7.
- o Mitigation of possible traffic noise from Cottonwood Hills Road to acceptable level, Section 7.2d.
- o Fire management zone, Section 6.2d.
- o Energy conservation, Section 6.6.
- o Water conservation, Section 6.9.





Appendix A - LEGAL DESCRIPTION

10

11

12

ADDENDUM "A" TO GRANT DEED

PARCEL 1:

The North half of the Northwest quarter of the Southwest quarter of Section 11, Township 6 South, Range 3 West, San Bernardino Base and Meridian, according to the official plat thereof.

Excepting therefrom that portion conveyed to the State of California for highway purposes by deeds recorded July 23, 1951 in Book 1289, page 115, Official Records; July 30, 1951, in Book 1290, page 556, Official Records and June 26, 1968 as Instrument No. 60040.

PARCEL 2:

Section 7, Township 6 South, Range 3 West, San Bernardino Base and Meridian, according to the official plat thereof.

PARCEL 3:

The Northwest quarter of the Northwest quarter; and the West half of the Southwest quarter of the Northwest quarter of Section 8, Township 6 South, Range 3 West, San Bernardino Base and Meridian according to the official plat thereof.

PARCEL 4:

Section 11, Township 6 South, Range 4 West, San Bernardino Base and Meridian according to the Official Plat thereof.

Excepting therefrom that portion lying within a strip of land 200 feet wide as conveyed to Temescal Water Company by deed recorded December 11, 1928 in Book 792, Page 292 of Deeds, Riverside County Records.

Also excepting therefrom, Railroad Canyon Road as conveyed to the County of Riverside by deed recorded May 24, 1949, in Book 1078, Page 585 Official Records.

Also excepting therefrom that portion described as follows:

Beginning at a point on the Southerly boundary of a County road as conveyed to the County of Riverside by Deed recorded May 24, 1949 in Book 1078, Page 585 of Official Records of Riverside County, from which point the Northeast corner of said Section 11 bears North 53° 59' East, a distance of 4249.40 feet; thence along said Southerly boundary line on an 1130 foot radius curve left (the long chord of which bears South 84° 12' East, 265.30 feet) a distance of 265.90 feet; thence South 25° 20' 30" West, 586.82 feet; thence North 64° 39' 30" West, 250.00 feet; thence North 25° 20' 30" East, 498.08 feet to the point of beginning.

290799

RECORDING REQUESTED BY

CHARLES H. CARTER
ATTORNEY AT LAW

AND WHEN RECORDED MAIL THIS DEED AND, UNLESS OTHERWISE SHOWN BELOW, MAIL TAX STATEMENTS TO

NAME CHARLES H. CARTER
ADDRESS Attorney at Law
CITY & STATE ZIP P. O. Box 917
Corona, CA 91718-0817

RECEIVED FOR RECORD
AT 11:00 O'CLOCK A.M.

DEC 26 1985

Recorded in Official Records
of Riverside County, Calif. No. *William E. ...*
Form 1 REC-100EN

9/3

Title Order No.

Fee No.

SPACE ABOVE THIS LINE FOR RECORDER'S USE

GRANT DEED

The undersigned declares that the documentary transfer tax is \$ Nil and is
 computed on the full value of the interest or property conveyed, or is
 computed on the full value less the value of liens or encumbrances remaining thereon at the time of sale. The land,
tenements or realty is located in
 unincorporated area city of _____ and

FOR A VALUABLE CONSIDERATION, receipt of which is hereby acknowledged,

ZORA E. CHRISTENSEN, a Widow,

hereby GRANT(S) to CLYDE C. CHRISTENSEN, TRUSTEE OF THE ZORA E. CHRISTENSEN TRUST, all
of my right, title and interest in and to

the following described real property in the Unincorporated Area of the
county of Riverside state of California:

Please see the attached Addendum "A".

Dated 12-4-85

Zora E. Christensen
ZORA E. CHRISTENSEN

STATE OF CALIFORNIA
COUNTY OF RIVERSIDE

On 12-4-85 before me, the undersigned, a Notary Public in and for said County and State, personally appeared ZORA E. CHRISTENSEN

Known to me to be the person whose name is subscribed to the within instrument and acknowledged that she executed the same.

Susan Licciardi
Signature of Notary

FOR NOTARY SEAL OR STAMP



Assessor's Parcel No. See Addendum "A" Attached

MAIL TAX STATEMENTS TO PARTY SHOWN ON FOLLOWING LINE; IF NO PARTY SO SHOWN, MAIL AS DIRECTED ABOVE
CLYDE C. CHRISTENSEN, TRUSTEE, 27581 Pacheco Trail, Hemet, CA 92344
Name Street Address City & State

290799

ADDENDUM "A" TO GRANT DEED (CONTINUED)

PARCEL 5:

Section 12, Township 6 South, Range 4 West, San Bernardino Base and Meridian, according to the Official Plat thereof.

Said land is also situated in the unincorporated area of Riverside County.

ASSESSOR'S NOS:

PARCEL 1: 35715-0001-2

PARCEL 2: 358-110-001 and 358-120-001

PARCEL 3: 358-130-001

PARCEL 4: 363-210-001
363-210-003
363-210-004
363-210-005
363-210-006
363-200-002

PARCEL 5: 363-230-001
363-230-002
363-230-003
363-220-001
363-220-002
363-220-003
363-220-004
363-220-005

END RECORDED DOCUMENT

Order No. 147 3137-87
Exoner No.
Lead No.

PAID
Doc. Transfer Fee
WILLIAM L. GIBNEY
Asst. C. Recorder

ACCEPTED FOR RECORD
at the office of the
Recorder of Deeds
of the County of Orange
California
SEP 01 1987

RECORDED
INDEXED
FILED

13375
WHEN RECORDED MAIL TO:
Clyde C. Christensen, Trustee
37381 Pacheco Trail
Hemet, Ca. 92344

SPACE ABOVE THIS LINE FOR RECORDER'S USE

MAIL TAX STATEMENTS TO:

SAME AS ABOVE

DOCUMENTARY TRANSFER TAX 8.50
Computed on the consideration or value of property conveyed; OR
Computed on the consideration or value less than of such interest
retaining in 1976 of 20%

Signature of Recorder or a duly authorized officer: [Signature]
WILSON BROTHERS TITLE INSURANCE COMPANY

AP# 365-210-002-3

CORPORATION GRANT DEED

FOR A VALUABLE CONSIDERATION, receipt of which is hereby acknowledged,

CORONA FOOTBALL COMPANY, a California corporation, Successor by Merger to Temescal Properties, Inc., a California corporation a corporation organized under the laws of the State of California . does hereby

GRANT to

CLYDE C. CHRISTENSEN, Trustee of the Estate of Hans Christensen, deceased, pursuant to Decree of Distribution recorded June 28, 1976 an Instrument No. 92734 of Official Records. the real property in the SH/AS unincorporated area County of RIVERSIDE State of California, described as

That portion of the former California Southern Company right of way lying within the No. 4 West Quarter of Section 11, Township 6 South, Range 4 West, San Bernardino Base and Meridian, as conveyed to Temescal Water Company, a California corporation by document recorded December 11, 1928 in Book 793 page 292 of Deeds, records of Riverside County, California.

17777777

END RECORDED DOCUMENT

Dated December 4, 1987

STATE OF CALIFORNIA ORANGE COUNTY OF ORANGE
On December 8, 1987 before me,

the undersigned a Notary Public in and for said State, personally appeared:
of Kenneth H. Klepp

the Daniel T. Gray
personally known to me (or known to me on the basis of satisfactory evidence to be the persons who executed the within instrument as
Vice Assistant Secretary

of RCE Development Inc.

For the purpose of the foregoing, and acknowledged to me that each of the persons executing the within instrument purports to do so by virtue of his position of his board of directors

WITH this document and official seal.
Notary Carrie Berglund

WILLIAM L. GIBNEY
Asst. C. Recorder
CORONA FOOTBALL COMPANY, a California corporation, successor by merger to Temescal Properties, Inc., a California corporation

By [Signature] President

By [Signature] Asst. Secretary



(This area for official records use) 1146 (1987)

MAIL TAX STATEMENTS AS DIRECTED ABOVE

Appendix B - SUMMARY OF ENVIRONMENTAL IMPACTS AND MITIGATION

IV. SUMMARY OF ENVIRONMENTAL IMPACTS

A. SIGNIFICANT ENVIRONMENTAL IMPACTS WHICH CANNOT BE MITIGATED

Most of the impacts identified in the EIR have been found to be either not significant or mitigated by the criteria and design guidelines in the proposed specific plan, or conditions of project approval which have been imposed by the City of Lake Elsinore. Certain impacts have been identified, however, which are considered significant by the city and that cannot be mitigated within the scope of the project. These impacts are cumulative in nature--involving not only this project, but other existing and proposed developments in the area. Such impacts include loss of biological habitat, reduction in the visual aesthetics of the area, poor air quality, and traffic congestion. These are discussed in Section VI.D. of the EIR.

B. SIGNIFICANT IMPACTS WHICH CAN BE MITIGATED

Impacts

As with any major development, the project would involve major alterations of landforms. Areas would be graded for streets and public utilities and to create building pads, either terraced as individual lots along residential streets or larger pads for multi-family and commercial development areas.

Mitigation

The specific plan includes a set of grading guidelines that incorporate the criteria used in the city's Grading Ordinance and would minimize the effects of grading. The grading guidelines specify areas where day-light cuts, contouring of manufactured slopes, terracing, landscaping and curving of slopes, and grade separations will be used.

These guidelines would minimize the visual disturbances resulting from grading. Further compensation for grading would be provided by the preservation of natural open space on the property. The amount and distribution of natural open space proposed within the plan, including areas along the San Jacinto River, Cottonwood Creek, and large areas of slopes, knolls, and ridgelines, would provide mitigation for the effects of grading.

1. Grading (Section V.B.)

Specific Plan References

Section 6.1 (Open Space System);
Figure 6.1 (Community Open Space);
Table 6.1 (Community Open Space System); Section 6.4a (Grading Objectives); Section 6.4b (Community Grading Concept); Figure 6.4 (Grading Concept); Figure 6.5 (Grading Approaches); Section 6.4c (Grading Guidelines); Figure 6.6 (Cut Slope Variations); Figure 6.7 (Residential Grading)

2. Visual Quality (Section V.C.)

Impacts

Current views of the property from homes along its northern and southern boundaries would be changed. A manufactured slope with a maximum height of 30 feet would be located adjacent to the existing trailer homes along the northern boundary. The proximity of new residential neighborhoods would also alter the views from homes near the southern boundary.

Mitigation

Visual impacts would be minimized by preserving some of the existing scenic resources and by using sensitive grading and landscaping techniques. Views within the property contain elements which provide interest and variety, specifically riparian woodland areas and steep ridgelines in the northern and southern portions of the property. These areas would be preserved in open space. Grade separations and uniform fencing and landscaping would be used to create a visual buffer between developed and natural areas.

Specific Plan References

Figure 6.1 (Community Open Space);
Table 6.1 (Community Open Space System); Figure 6.4 (Grading Concept);
Figure 6.7 (Residential Grading);
Table 3.2 (Detached Residential Design Guidelines); Figures 3.2 through 3.5 (Off-site Interfaces)

3. Biological Resources (Section V.D.)

Impacts

Implementation of the Cottonwood Hills Specific Plan would result in three major impacts on the existing biological community. The project would (1) affect up to 5 acres of riparian habitat, mostly in isolated areas not directly associated with the main drainages, (2) eliminate all of the appropriate Stephen's kangaroo rat habitat from the property; and (3) result in the loss of approximately 45 percent of the habitat used by the black-tailed gnatcatcher and the orange-throated whiptail.

Mitigation

Compensation for the planned filling and clearing of wetlands would be provided by implementation of a wetland enhancement program designed to restore disturbed areas of the Cottonwood Creek drainage natural conditions. The detailed wetland enhancement program will be developed in consultation with the California Department of Fish and Game and the Army Corps of Engineers, in accordance with the requirements of a Section 1603 Streambed Alteration Agreement and a Section 404 permit, respectively.

Specific Plan References

Section 6.2b (Riparian Habitat Areas);
Figure 6.1 and Table 6.1--Open Space Areas C, D, and Parkway Park are designated for habitat enhancement.

Specific Plan References

Section 6.8b (Wildlife)

Mitigation

Compensation for the loss of kangaroo rat habitat will be provided by participation in the development of conservation measures through the recently initiated HCP process or an interim city program consistent with the goals of the HCP process and in consultation with the USFWS. Participation could include funding of conservation measures, research and development of management program information, or contributions towards acquisition of appropriate habitat areas.

Section 6.8a (Vegetation); Section 6.8b (Wildlife)

Compensation for the loss of gnatcatcher and orange-throated whiptail habitat would be provided by the preservation of approximately 637 acres of coastal sage scrub in large parcels on the property.

4. Cultural Resources (Section V.E.)

Mitigation

In the event any grading or construction activity would affect the Manker family cemetery site, appropriate measures in conformance with state law regarding the removal of small family burial sites would have to be followed.

Compensation for disturbances to cultural resources would be provided by a program to recover information

Impacts

Implementation of the proposed Cottonwood Hills Specific Plan would result in disturbances to several archaeological sites, possibly including the Manker family cemetery.

With the exception of the cemetery, all of the archaeological sites on the property are in locations that will be developed in the proposed specific plan. The loss of the information

Specific Plan References

Section 6.7 (Cultural Resources)

Impacts

contained in the sites would be considered a significant impact.

Mitigation

from sites that cannot be preserved. At the Manker family homestead, the initial clearing operations for grading would be monitored by archaeologists to determine if there are any structures or historical artifacts that could be recovered. In the event significant historic material is discovered, it would be removed before grading occurs.

5. Traffic Circulation (Section V.F.)

Impacts

The project population would generate 43,190 external daily vehicle trips, 4,050 of which would occur during the evening peak hour. All intersections in the project vicinity are projected to operate at LOS A in the evening peak-hour for existing plus project traffic conditions with recommended improvements. Cumulative development within the area will generate an estimated 68,360 ADT and 6,680 evening peak-hour trips. Under the engineering design standards for an arterial highway, improvements to Railroad Canyon Road will be required.

Mitigation

A variety of phased traffic improvements will be included within the project. These include the widening of Railroad Canyon Road, installation of roads, traffic signals and other intersection improvements, and parking and access restrictions.

Specific Plan References

Specific Plan References

Section 7.1a (Railroad Canyon Road);
Section 7.1b (Major Road System);
Figure 7.2 (Community Street System);
Table 7.1a (Streetscape Design Guidelines/Overall Community); Section 7.3 (Auto Parking)

6. Services and Utilities (Section V.G.)

Impacts

Estimates of consumption and generation figures for the project are as follows:

Water: 2.3 million gallons per day

Wastewater: 1.1 million gallons per day

Electricity: 3.1 million kwh per month

Natural Gas: 30 million cubic feet per day

Elementary School: 1,379 students (Lake Elsinore School District)

Mitigation

The project site would be annexed to and pay applicable fees to the Elsinore Valley Municipal Water District (EVMWD).

The proposed specific plan calls for a contribution towards the extension of a new outfall along Railroad Canyon Road and for payment of the applicable sewer service fee required by the EVMWD.

The construction of all improvements within the project would be paid for by the developer. The extension to the project site of electrical and gas lines would be the responsibility of the appropriate utility service.

Gas service would be provided by Southern California Gas; there will be no significant impact and no required mitigation.

Three elementary school sites (of 10 gross acres each) are designated in the specific plan area. The proposed

Specific Plan References

Section 5.4a (Water Service); Table 5.5c (Public Facility Implementation Options-Utilities)

Section 5.4b (Sewer Service); Table 5.5c (Public Facility Implementation Options-Utilities)

Section 5.4c (Gas and Electric Service); Table 5.5c (Public Facility Implementation Options-Utilities)

Section 5.1b (School Facilities); Table 5.2 (Elementary School Design Guidelines); Table 5.5a (Public

Specific Plan References

Facility Implementation Options--On-site Facilities)

Mitigation

size of the sites meets both districts' standards for school sizes. School fees would be paid via a community-wide financing program.

Impacts

405 students (Memiffee Union School District)

High School:
83 students (Perris Union High School District)

707 students (Elsinore Union High School District)

Solid Waste: 16,425 tons per year of residential waste, plus additional commercial waste.

Section 5.3d (Solid Waste Service)

The El Sobrante Landfill presently has the capacity to operate for 14 more years; an expansion is planned which will extend the life of the landfill an additional 20 years. No mitigation is required.

Park Acreage: 42.1 acres would be provided, whereas 57 are required.

Section 5.2a (Parks Requirements);
Section 5.2b (Neighborhood Parks);
Section 5.2c (Community Park);
Section 5.2d (Creek Park); Table 5.3 (Neighborhood Park Design Guidelines); Table 5.4 (Community Park Design Guidelines)

The specific plan provides four park sites (two neighborhood parks, one community park, and one Parkway Park) on a total of 42.1 acres. In addition the proposed plan calls for the provision of usable open space, scenic areas, trail corridors, and bikeways.

7. Hydrology and Water Quality (Section V.H.)

Specific Plan References

Section 6.5a (Drainage Objectives);
Section 6.5b (Drainage Concepts);
Table 6.4 (Drainage Concepts); Figure 6.7 (Drainage Concept); Section 9.3d (Phasing of Grading)

Mitigation

In each of the four drainage areas, after studies have been conducted, necessary measures will be taken to control the rate of runoff flow. Typical measures to control erosion

Impacts

Potential effects on the hydrology and water quality of the area are of two types: increased sediment discharge from erosion during grading and construction activities, and the typical

Impacts

effects on surface runoff resulting from urbanization.

Mitigation

include the use of temporary ditches, sandbagging, desilting basins, and erosion control landscaping. Grading and improvement plans for the development, which must be approved by the city prior to issuing any grading permits, shall include erosion control designs using these or other measures for the project during construction.

Specific Plan References

Impacts

Existing information indicates that the most significant seismic event affecting the site would be a 7.0 magnitude earthquake occurring on the Elsinore fault zone. In the vicinity of Boring 7, the soils below the water table have a high liquefaction potential during a major ground-shaking event. The on-site silty sands and sandy silts taken from alluvial areas can be expected to shrink on the order of 15 to 20 percent when recompacted.

The material from bedrock excavations could bulk on the order of 5 percent. Subsidence of natural ground due to the movement of construction equipment is expected to be 0.2 foot in proposed fill areas.

Mitigation

A variety of standard measures for providing foundation stability would be incorporated into the project. These include removal of unsuitable material, compaction techniques, drainage facilities, continuous observation and testing of soils as grading progresses, and other measures outlined in the geological report.

Specific Plan References

Section 6.4c (Grading Guidelines)

8. Geology and Soils (Section V.I.)

9. Noise (Section V.J.)

Impacts

Residential development proposed adjacent to the major roads in the project, including Railroad Canyon Road, the western segment of Cottonwood Hills Road, and possibly the eastern segment of Cottonwood Hills Road, would be subject to significant noise levels.

Mitigation

The grading designs and schematic wall locations proposed in the specific plan would provide mitigation of exterior noise levels for all residential development proposed. More detailed analysis may be performed at the time when more precise grading information is submitted, in conjunction with tentative tract maps, and it may be possible to reduce or eliminate sound walls in some locations. When building plans are available for multi-family units adjacent to the major roads, additional analysis may also be necessary to insure that interior noise levels are acceptable.

Specific Plan References

Section 6.4a (Grading Objectives);
Section 6.4c (Grading Guidelines);
Figure 6.7 (Residential Grading);
Figures 7.4 through 7.8 (Street Sections); Section 7.2a (Streetscape Design Objectives); Table 7.1c (Streetscape Design Guidelines/Adjacent Development Projects)

10. Air Quality (Section V.K.)

Impacts

Short-term dust impacts and other pollutants would result from grading and construction activities. Long-term impacts to air quality would result from activities associated with the permanent occupation of the proposed development. Long-term source of pollution would include: emissions from vehicular traffic to and from the development, local emissions from the burning of natural gas for space and water heating in the development, and air basin-wide emis-

Mitigation

Tactics designed to reduce energy consumption and fugitive dust generation would be implemented as part of required conservation measures and grading controls. The design of the residential areas promotes the shading effect of landscaping for streets, parking areas, and building walls; thereby reducing hydrocarbon evaporation rates. The locations of the neighborhood commercial centers, parking restrictions, and wide parkway strip facilitate planning for transit

Specific Plan References

Section 6.1 (Open Space System);
Section 6.6 (Energy Conservation);
Section 7.2b (Streetscape Concept);
Section 7.4 (Public Transportation);
Section 7.5 (Bicycle Circulation);
Section 7.6 (Pedestrian/Equestrian Circulation)

Impacts

sions from power plants generating electricity for use in the development. The proposed project alone is considered to have no direct significant effect on the generation of air pollutants. However, as long as the SCAB is considered a nonattainment area for air pollutants, all sources of emissions in the basin may be considered as contributing to a significant cumulative impact on air quality.

Mitigation

The specific plan also provides for a system of bicycle and pedestrian paths that link the various neighborhoods in the community.

Specific Plan References

11. Energy Conservation (Section V.L.)

Impacts

The following quantities of energy will be consumed per month: 3.1 million kwh of electricity, 30 million cubic feet of natural gas, and 12,319 gallons of gasoline. The energy requirements will be met with no significant impact on the service facilities, and therefore, the energy consumption does not represent a significant impact. Nonetheless, the nonrenewable aspect of long-term energy supplies dictates that steps be taken to minimize energy consumption whenever possible.

Mitigation

Construction within the specific plan area will be consistent with Title 24 of the California Administrative Code, which establishes energy conservation standards for residential and non-residential buildings. In addition, the development would be partially "self-sustaining" because of the balance of land uses provided. This balance will allow the residents of the specific plan area to have access to neighborhood shopping centers, parks, and schools which are in close proximity to their residences, reducing energy consumption for automobile trips.

Specific Plan References

Section 6.6 (Energy Conservation)

C. IMPACTS FOUND NOT TO BE SIGNIFICANT

Of the issues reviewed during preparation of the EIR, several were identified as either being not significant or being completely mitigated without requirements for special conditions on the project. These included land use, growth inducement, certain utility and service requirements, and several effects related to geology, soils, hydrology, and water quality.

D. CUMULATIVE IMPACTS ASSOCIATED WITH PROJECT IMPLEMENTATION

The project calls for the development of a new planned community to accommodate the residential growth anticipated in the area. In this respect, it contributes to the cumulative effects that are typically associated with land development. These include increased demands for services and energy, increased traffic and air emissions, and regional reductions in biological habitat. The details of these contributions are outlined in the various sections of this EIR. The project incorporates several measures which promote the conservation of resources--a balance of land uses and large areas of open space. However, these measures do not reduce these impacts below a level of significance. This project would contribute substantially to the regional adverse conditions. These cumulative impacts are considered significant.

The proposed project is representative of a type of development occurring at a rapid rate in southern California generally and the Lake Elsinore area in particular. Construction of housing developments requiring massive amounts of grading and designed around the automobile as the dominant form of transportation; the ever-increasing demand on limited water resources; and the ever-increasing generation of trash, sewage, and other environmental contaminants associated with this type of development have resulted in complex environmental health problems. Particularly acute in the Lake Elsinore area are the following issues:

1. Loss of biologically significant habitat
2. Deterioration of the visual quality of the area
3. Poor air quality
4. Traffic congestion.

These topical issues are addressed in Section VI.D of this EIR.

Appendix C - GENERAL PLAN CONFORMANCE

C.1	Land Use Element	C-2
C.2	Draft General Plan Land Use Policies and Objectives	C-6
C.3	Circulation Element	C-14
C.4	Environmental Resources Management Element	C-16
	C.4a Preservation of Natural Resources	C-16
	C.4b Managed Productivity of Natural Resources	C-17
	C.4c Outdoor Recreation	C-18
	C.4d Public Health and Safety	C-20
C.5	Noise Element	C-22
C.6	Community Design Element	C-23
C.7	Housing Element	C-25

Appendix C - GENERAL PLAN CONFORMANCE

This appendix addresses the conformance of the Cottonwood Hills Specific Plan to the City of Lake Elsinore General Plan.

C.1 Land Use Element

The General Plan 1982 Land Use Map does not encompass the specific plan area. However, conformance to the Land Use Policies of the General Plan as adopted in 1982 is discussed below.

POLICY 1.1:

It is the policy of the City to establish and maintain a balance of land uses throughout the community.

The Specific Plan contributes to a balance of land uses throughout the Lake Elsinore community by providing for a mix of residential uses with supporting commercial and public facilities on-site; and by adding to the population base to support commercial, industrial, and recreational development in the lake-downtown areas.

Specific Plan References — Section 2.1, Specific Plan Goals. Section 2.2, Overall Specific Plan.

Objective 1.1a:

Encourage the development of both existing and new neighborhoods in an orderly fashion, wherever growth does not exceed the capacity of the community to provide necessary services and facilities.

A Specific Plan is employed to guide Cottonwood Hills development, as required by the General Plan for large land ownerships. The Plan permits development of a new residential neighborhood in an orderly fashion, utilizing the specific plan process to ensure adequate development controls and provision of necessary services and facilities.

Specific Plan References — Section 2.2, Overall Specific Plan. Section 2.3, Specific Plan Designation. Section 2.5, Conformance with Specific Plan District Ordinance. Chapter 8, Community-Wide Implementation Element.

Objective 1.1b:

Encourage the development of commercial centers at strategic points in the Planning Area.

The Specific Plan designates appropriate locations and types of commercial development within the proposed community. Design guidelines and standards address access, parking, loading, and appearance to ensure attractive, functional commercial centers.

Specific Plan References — Section 4.1, Commercial Land Use.

Objective 1.1c:

Enhance and encourage the revitalization of the Central Business District to serve the needs of permanent residents and tourists.

Cottonwood Hills' commercial uses are intended to predominantly serve the community's and nearby residents' retail and service needs. These complement the planned specialty commercial in downtown Lake Elsinore.

Specific Plan References — 4.1, Commercial Land Use.

Objective 1.1d:

Encourage the location of industries which are compatible with the Planning Area's resources, climate, and appearance.

Given the topography, accessibility, and neighboring uses of the specific plan area, industrial development is not appropriate to the Cottonwood Hills community.

Specific Plan References — Section 2.2, Overall Specific Plan.

POLICY 1.2:

It is the policy of the City of Lake Elsinore to recognize the importance of land uses in determining the quality of life and its effect on the environment.

Objective 1.2a:

Create an environment which is satisfying to the residents of the community and which will appeal to the many people in Southern California who seek locations for recreation purposes or for permanent residence in Lake Elsinore.

The Specific Plan arranges land uses to limit incursions into sensitive environmental resources, and specifies grading guidelines and buffer designs to provide for environmentally sensitive development. Design guidelines govern the visual quality of developments and transitions between land uses. Air quality mitigation measures include bicycle facilities, transit accommodation, and pedestrian facilities; energy conservation; location of support facilities for residential development within the community; and open space preservation.

Specific Plan References -- Section 2.2, Overall Specific Plan. Section 3.2, Residential Design Guidelines. Section 4.1, Commercial Land Use. Section 4.2, Institutional Land Use. Section 5.2, Public Parks. Chapter 6, Resources Management Element. Section 7.4, Public Transportation. Section 7.5, Bicycle Circulation. Section 7.6a, Pedestrian System.

POLICY 1.3:

It is the policy of the City to insure that adequate public services and facilities are provided in a timely and adequate manner.

The Specific Plan outlines phasing and implementation programs to ensure adequate public services and facilities in a timely manner.

Specific Plan References -- Section 5.5, Public Facilities and Services Implementation. Section 7.7, Circulation Implementation. Section 9.3, Phasing Program. Section 9.4, Financing Program.

Objective 1.3a:

Correct existing sewer and water deficiencies prior to, or concurrent with, the extension of services and facilities to undeveloped areas.

Adequate water and sewer facilities will be provided concurrently with development under the Specific Plan.

Specific Plan References -- Section 5.4, Utilities. Section 5.5, Public Facilities and Services Implementation. Section 9.3c, Public Facilities and Services Phasing.

Objective 1.3b:

Provide adequate solid waste disposal facilities which are suitably located to serve the Planning Area.

Existing regional landfill operations are sufficient to accommodate solid waste disposal from the Cottonwood Hills community.

Specific Plan References -- Section 5.3d, Solid Waste Services.

Objective 1.3c:

Provide adequate school facilities and services to all new development within the City.

Adequate school facilities and services will be provided for all new development approved under the Specific Plan, in coordination with the relevant school districts. School agreements are required in conjunction with permitting of residential projects.

Specific Plan References — Section 5.1, Public Schools. Section 5.5, Public Facilities and Services Implementation. Section 9.3c, Public Facilities and Services Phasing. Section 9.4c, School Agreements.

C.2 Draft General Plan Land Use Policies and Objectives

A revised General Plan for the City of Lake Elsinore is under preparation. Conformance to the adopted draft policies and objectives for Land Use is addressed below.

POLICY 1:

The City shall not amend, alter, or apply the General Plan or Zoning designations in any manner which would clearly or likely cause potential harm to the health, safety, or general welfare of the citizens and residents of the City of Lake Elsinore.

The proposed Specific Plan does not clearly or likely cause potential harm to the health, safety, or general welfare of the citizens of the City.

Specific Plan References -- Section 2.2, Overall Specific Plan. Section 2.5, Conformance with Specific Plan District Ordinance. Chapter 8, Zoning Element. Section 9.1, Implementation Process.

POLICY 2:

The City shall not amend, alter, or apply the General Plan or Zoning designations in any manner which would result in an INCONSISTENCY between the General Plan and the Zoning Code. Simultaneous amendments to either the General Plan or a Zoning designation are permitted when they are elements of a single project proposal.

The Specific Plan provides consistency between the proposed land uses and the development standards which act as zoning controls.

Specific Plan References -- Section 2.2, Overall Specific Plan. Section 3.1, Residential Land Use. Section 4.1, Commercial Land Use. Chapter 8, Zoning Element.

POLICY 3:

The City shall not approve any proposed land use project or modification of the General Plan and Zoning which clearly can be seen to cause significant environmental harm to the City of Lake Elsinore and/or its citizens and residents unless substantial factual information is submitted and made a part of the public record so that well founded overriding considerations can be made.

The proposed land use project does not cause significant environmental harm to the City or its residents, but proposes preservation of significant resources and/or mitigation of negative environmental impacts.

Specific Plan References -- Chapter 6, Resources Management Element. Section 9.1, Implementation Process. Appendix B, Summary of Environmental Impacts and Mitigation.

POLICY 4:

The City shall give its first preferred consideration to those development proposals which provide for regional and subregional commercial/industrial uses which clearly avoid any significant conflict with the land uses and potential land uses of the downtown Central City Core as a "Specialty Commercial" development center. Said Specialty Commercial uses include, but are not limited to: professional office uses, specialty retail uses, restaurant/outdoor cafe uses, and other similar land uses which do not generate high volume vehicular movements.

Cottonwood Hills' commercial uses are intended to predominantly serve the community's and nearby residents' retail and service needs. These complement the specialty commercial in downtown Lake Elsinore and regional commercial and industrial uses along Interstate 15.

Specific Plan References -- Section 4.1, Commercial Land Use.

POLICY 5:

The City shall give its second preferred consideration to those development proposals which provide for middle and upper income residential development. The need for such land use development is viewed as critically needed within the City to provide for improved urban standards and community balance.

While accommodating a range of housing densities and income levels, the Specific Plan emphasizes single-family detached residential for middle and upper-middle income residents. The community represents an improvement over certain development which already exists in the City.

Specific Plan References -- Section 3.1, Residential Land Use.

POLICY 6:

The City shall give its third preferred consideration to those development proposals which provide for YEAR AROUND RECREATIONAL USES. The City shall give priority to those recreational development proposals which provide facilities that clearly lend themselves to year around use by the public.

The Specific Plan provides year around recreational facilities in the form of public neighborhood parks, a public community park, a parkway park, school playground facilities, residential project recreational facilities, and an open space network.

Specific Plan References -- Section 5.2, Public Parks. Section 5.1, Public Schools. Section 6.1, Open Space System. Section 7.6, Pedestrian/Equestrian Circulation.

POLICY 7:

The City shall insure that where high density, multiple family, residential land uses are proposed that said uses are: (1) not greater than 10 net acres in size either by themselves or as an accumulation of 10 net acres with surrounding adjacent high density, multiple family, residential uses; and (2) that the design of such high density, multiple family, residential uses provides for adequate open common areas. This Policy shall not apply to any current or proposed Specific Plans which may be approved by the City.

Multiple-family residential uses are situated in proximity to community amenities such as river/creek open space corridors or public parks, and neighborhood commercial areas. All attached projects provide on-site open space common areas and recreational facilities.

Specific Plan References -- Section 3.1, Residential Land Use. Section 3.2, Residential Design Guidelines. Section 5.2d, Project Recreational Facilities.

POLICY 8:

The City shall view Lake Elsinore as the primary center or beginning point for the overall development of the City now and in the future. In this view, the City shall endeavor to generally reduce the intensity of land uses as land use projects distance themselves from the Lake. Major commercial and/or industrial centers shall also be viewed as land use centers where surrounding land use intensity is gradually reduced as projects distance themselves from such centers. Ultimate land use patterns within the City shall result from a full consideration of: existing and proposed infrastructure facilities, environmental conditions and limitations, and social and community needs.

Moving out from the downtown business district and the Interstate 15 corridor as the focus of intense development, the specific plan area is considered an outlying area. Accordingly, development is proposed at an average density of 2.17 dwelling units per acre. Within the Cottonwood Hills community, higher density housing is clustered along Railroad Canyon Road and along Cottonwood Hills Road in the west central portion of the

project. Lower density residential is planned in the more remote eastern and southern areas of the community to be compatible with existing rural residential uses.

Specific Plan References — Section 2.1, Overall Specific Plan. Section 2.3, Specific Plan Designation. Section 3.1, Residential Land Use. Section 5.4, Utilities. Section 7.1, Street System. Section 9.3, Phasing Program.

POLICY 9:

Established rural residential property shall not have high intensity land uses placed adjacent to and contiguous with its boundaries.

In Cottonwood Hills, lower density detached residential uses in the form of conventional (SF2) and estate (SF1) single-family lots are placed adjacent to established rural residential property. In addition, landscaped and natural open space buffers are specified where appropriate.

Specific Plan References — Section 3.1, Residential Land Use. Section 3.2, Residential Design Guidelines.

Objective 1:

Revise the City's Zoning and Land Development ordinances where necessary to implement and achieve consistency with the land use designations of the General Plan.

Not applicable to Specific Plan.

Objective 2:

Extensive rezonings and General Plan changes shall be undertaken to promote the land use proposals of the General Plan and to recognize and legalize those current uses which are worthy of preservation.

Not applicable to Specific Plan.

Objective 3:

Require that Specific Plans be submitted by developers of large land ownerships for those areas designated SPA (Specific Plan Area) and encourage the submission of Specific Plans for those areas which are not designated, but are deemed appropriate for Specific Plan requirements.

The Cottonwood Hills property is a large landholding, and is accordingly submitted to the City for development under a Specific Plan, with an SPA (Specific Plan Area) designation.

Specific Plan References — Section 1.2, Significance of Specific Plan.

Objective 4:

Plan residential neighborhoods so nearby supporting facilities and services are in scale with these neighborhoods and adequately serve their nearby neighborhoods. In this regard, identify and designate specific planning areas as neighborhoods.

The Cottonwood Hills plan distributes the proposed commercial/institutional uses and school/park facilities to serve the various residential subareas of the community.

Specific Plan References — Section 4.1, Commercial Land Use. Section 4.2, Institutional Land Use. Section 5.1, Public Schools. Section 5.2, Public Parks.

Objective 5:

Insure that land uses generating high vehicular traffic volumes have access to major transportation routes, incorporate open spaces, provide off-street parking and bus loading lanes. Bus loading lanes will only be required on major collector streets and where lot frontages are of adequate width. Landscaping will not be required around these loading lanes.

For Cottonwood Hills, the more intense land uses such as multi-family residential and neighborhood commercial are located in the western portion of the specific plan area, with access to major transportation routes and potential transit stops. These land uses will incorporate adequate off-street parking and open space.

Specific Plan References — Section 2.2, Overall Specific Plan. Section 7.1, Street System. Section 7.4, Public Transportation.

Objective 6:

Insure that ordinances are adopted which will require the undergrounding of all utilities in all zones. Mandatory undergrounding of utilities will not be required for single "in-fill" lots in older neighborhoods where no existing utility undergrounding has occurred and for agriculturally zoned parcels over five (5) acres in size.

Under the Specific Plan, all community utility lines will be placed underground.

Specific Plan References — Section 5.4, Utilities. Section 7.2, Streetscape Design.

Objective 7:

Promote the development of bike lanes, Transit facilities, and Park and Ride facilities by the passage of ordinances and the implementation of special developer fees and capital improvement projects designed to maximize long term transportation needs.

The Specific Plan addresses alternative transportation modes including transit, bicycle travel, and pedestrian movement.

Specific Plan References — Section 7.4, Public Transportation. Section 7.5, Bicycle Circulation. Section 7.6a, Pedestrian System. Section 7.7, Circulation Implementation.

Objective 8:

Implement a revised Circulation Element that proposes a wholly adequate circulation plan for the entire citywide road system and connecting regional circulation systems.

Cottonwood Hills provides its own internal circulation system while linking into the City/County regional system. The project will contribute financially to the needed upgrading of Railroad Canyon Road.

Specific Plan References — Section 7.1, Street System. Section 9.3b, Transportation Phasing.

Objective 9:

Place buffer areas adjacent to critical wildlife habitats or other valuable resource areas to insure their long term viability.

Buffer areas are provided adjacent to significant habitat areas planned for preservation.

Specific Plan References — Section 3.2, Residential Design Guidelines. Section 6.2, Open Space Design Guidelines.

Objective 10:

Require all signage (especially free standing) to adhere to strict scenic policies which provide for a 15 year "Grandfathering" of all existing signage to ordinance standards.

Not applicable to Specific Plan.

Objective 11:

Require development projects to incorporate natural features and unique scenic features into their site development plans and encourage "clustering" and similar design principles to achieve these goals.

The Specific Plan preserves the distinctive landforms and drainage ways of the site in open space while placing development on relatively flat, non-sensitive areas.

Specific Plan References — Section 2.3, Specific Plan Designation. Section 6.1, Open Space System. Section 6.4, Grading.

Objective 12:

No structure(s) - in any zone - shall be erected without consideration of the extent to which the lake viewshed of other properties, developed or undeveloped, are blocked by said proposed structure(s).

Not applicable to Specific Plan.

Objective 13:

Scenic corridors shall be identified and made mandatory setbacks around the perimeter of Lake Elsinore.

Not applicable to Specific Plan.

Objective 14:

Hill top development in all zones shall be prohibited. Where an independent consultant architect is employed by the City at applicant expense to review exceptions to this policy objective, findings must be made that clearly show that: (1) the design of the basic structure is employed to achieve a positive aesthetic impact; and (2) the majority of the Planning Commission members and the City Council agree with the Consultant architect's finding of long term beneficial aesthetic impacts from said development. All existing and proposed legal parcels subject to this prohibition shall be proposed legal parcels subject to this prohibition shall be permitted to "cluster" a proposed land use on the non-hill top portion of said parcels.

With the exception of some estate lot (SF1) sites, all development in Cottonwood Hills is proposed in non-hilltop areas with minimal encroachment into steep slopes. Development is clustered in the flatter portions of the specific plan area, and a terraced grading approach is utilized for residential development in moderately sloped areas. The

estate lot projects utilize the flatter portions of the hilly southeast area, creating a series of development clusters with natural open space included in many lots or daylighted grading.

Specific Plan References — Section 6.4, Grading. Section 3.2, Residential Design Guidelines.

Objective 15:

Undersized, legal, non-conforming parcels shall be provided with incentives which will encourage their combination in order to provide development projects that have greater aesthetic and community benefits.

Not applicable to Specific Plan.

Objective 16:

Grading and drainage standards shall be adopted that provide for maximum protection of existing hillside slopes and natural terrain features.

The Specific Plan outlines a series of grading and drainage guidelines to protect steep slopes and major drainage ways. These guidelines are based on the intent of existing City standards, tailored to natural site conditions and development opportunities within the Specific plan area. The implementation program provides a process for reviewing the design and engineering of individual planning unit projects to meet Specific Plan guidelines.

Specific Plan References — Section 6.2, Open Space Design Guidelines. Section 6.4, Grading. Section 6.4, Drainage and Hydrology. Section 9.2, Site Plan Review.

C.3 Circulation Element

The General Plan 1992 Land Use Map with roadway designations does not include the Cottonwood Hills property. However, conformance to this policy and objectives of the Circulation Element (General Plan, 1982) is demonstrated below.

POLICY 2.1:

It is the policy of the City of Lake Elsinore to provide for safe, fast and efficient movement of people and goods within Lake Elsinore and between Lake Elsinore and other parts of the region by an integrated system of streets, freeways, public transit and other transportation facilities.

The Specific Plan ties into and augments the City and region's integrated system of streets, freeways, public transit, and other transportation facilities.

Specific Plan References -- Chapter 7, Circulation Element.

Objective 2.1a:

Plan for and implement a network of arterial, major and collector roads that connect the various parts of the City together.

Cottonwood Hills provides for a network of community major, secondary, collector, and local streets while contributing to the upgrading of Railroad Canyon Road, an arterial highway.

Specific Plan References -- Section 7.1, Street System. Section 7.7, Circulation Implementation. Section 9.3b, Transportation Phasing.

Objective 2.1b:

Maintain the current level of bus services and expand such services as required when demand levels increase.

The Specific Plan's arterial and major highways can accommodate bus service when demand warrants. Activity centers are sited along potential transit routes and can accommodate transit stops.

Specific Plan References -- Section 7.4, Public Transportation. Section 7.7, Circulation Implementation.

Objective 2.1c:

Insure that adequate on-site parking facilities are provided for all land uses.

On-site parking for all land uses is required per the Specific Plan and the Zoning Ordinance.

Specific Plan References — Section 7.3, Auto Parking. Section 3.2, Residential Design Guidelines. Section 4.1c, Commercial Design Guidelines. Section 4.2c, Institutional Design Guidelines. Section 5.1c, School Design Guidelines. Section 5.2b, Neighborhood Parks. Section 5.2c, Community Park.

Objective 2.1d:

Provide for the safe and convenient use of bicycles throughout the City for recreation and as a viable alternative to the automobile as a form of local transportation.

Bike lanes are proposed paralleling Railroad Canyon Road, as required by the City's Bikeway Plan. Continuous bike lanes are planned in the roadways of major roads within the Cottonwood Hills community, providing access to activity centers.

Specific Plan References — Section 7.5, Bicycle Circulation. Section 7.7, Circulation Implementation.

C.4 Environmental Resources Management Element

Conformance of the Cottonwood Hills Specific Plan to the policies and objectives of the Environmental Resources Management Element (1982) of the General Plan is addressed below.

C.4a Preservation of Natural Resources

POLICY 3.1:

It is the policy of the City of Lake Elsinore to ensure the long-term viability of the community's natural biological environment.

Objective 3.1a:

Protect and maintain significant examples of plant and animal life by reducing negative impacts of human activities.

The Specific Plan provides for the preservation and buffering of the San Jacinto River and Cottonwood Creek riparian areas as significant habitats. A mitigation program for the Stephen's Kangaroo Rat is proposed in conjunction with development. Large areas of steep slopes with native vegetation are preserved as permanent natural open space. Also outlined in the Plan is a fire management program to reduce fire hazards.

Specific Plan References -- Section 6.2, Open Space Design Guidelines. Section 6.8, Biological Resources.

POLICY 3.2:

It is the policy of the City of Lake Elsinore to preserve the sense of open space and important scenic and visual resources.

Objective 3.2a:

Protect the physical resources which create the unique scenic and visual character of the Elsinore Valley.

The Specific Plan retains significant drainage ways, steep slopes, and riparian habitats in permanent open space. Development is placed in relatively flat, non-sensitive areas. The Plan takes into account natural scenic features such as hillsides, rock outcroppings, major tree stands, and prominent landforms, preserving these features where feasible. Through sensitive grading and landscaping, manufactured slopes abutting natural open space are naturalized in appearance to the extent possible. The community road system and residential developments are laid out to take advantage of view opportunities.

Specific Plan References — Section 3.2, Residential Design Guidelines. Section 6.1, Open Space System. Section 6.2, Open Space Design Guidelines. Section 6.3, Open Space Preservation and Maintenance. Section 6.4, Grading.

C.4b Managed Productivity of Natural Resources

POLICY 3.3:

It is the policy of the City of Lake Elsinore to enhance the economic potential of the area's natural resources.

Objective 3.3a:

Promote the economic use of mineral and groundwater deposits in a manner which will generate benefits to present and future generations.

Under the Specific Plan, the existing sand and gravel extraction areas on-site will be phased out. This is due to the realignment and upgrading of Railroad Canyon Road, and to the incompatibility of mining operations with proposed residential development. These operations are not considered significant.

Specific Plan References — Section 6.2d, Extraction Operations. Section 9.3b, Transportation Phasing.

POLICY 3.3b:

Maintain and improve the Planning Area's air quality.

The Specific Plan contains a number of features designed to mitigate air quality impacts:

- o The project is a planned community which offers a variety of services on-site.
- o Abundant landscaping is provided, which tends to shield and filter air on-site, and provides shade to reduce emissions.
- o Setbacks and buffer areas from roadways and parking areas are provided, which allow dispersion of pollutants.
- o Bicycle and pedestrian facilities are provided.
- o Transit services can be accommodated.

- o Energy-efficient buildings will be constructed.

Specific Plan References -- Section 2.2, Overall Specific Plan. Section 6.6, Energy Conservation. Section 7.2, Streetscape Design. Section 7.3, Auto Parking. Section 7.4, Public Transportation. Section 7.5, Bicycle Circulation. Section 7.6, Pedestrian/Equestrian Circulation.

POLICY 3.4:

It is the policy of the City of Lake Elsinore to protect and preserve existing agricultural activity and areas with prime agricultural lands.

Objective 3.4a:

Preserve and maintain agricultural areas which permit production of food and fiber as well as provide open space views which contribute to the rural character of the Lake Elsinore Valley.

Development of portions of Cottonwood Hills will result in the loss of prime agricultural soils with the potential for production. This loss in lands and potential agricultural income is not considered significant in terms of regional agricultural production. Because surrounding areas are being converted or have been converted to development, the community is not considered premature conversion of agricultural land.

Specific Plan References -- Section 1.3b, Development Factors.

C.4c Outdoor Recreation

POLICY 3.5:

It is the policy of the City of Lake Elsinore to protect and enhance natural resources having recreational value to the planning area.

Objective 3.5a:

Utilize the open space system to provide outdoor recreation opportunities for residents and visitors.

The Specific Plan provides locations for trail development within the open space network, to provide outdoor recreational opportunities. The permanent open space system protects the water course areas and steep slope areas.

Specific Plan References -- Section 6.1, Open Space System. Section 7.6b, Hiking/Riding Trails. Section 7.7, Circulation Implementation.

POLICY 3.6:

It is the policy of the City of Lake Elsinore to provide a system of public parks, riding and hiking trails, and outdoor recreation facilities which not only preserve significant areas of natural beauty for citizen enjoyment, but which also serve the needs of the citizens in their immediate environments. This system is to be augmented by private outdoor recreation facilities that are compatible with the goals and objectives of the public system.

Objective 3.6a:

Provide recreational facilities which are easily accessible to all residents.

The Cottonwood Hills project contains four types of public parklands: two neighborhood parks; a community park; elementary school play facilities; and a parkway park and open space corridors ("parkways") following the San Jacinto River and Cottonwood Creek alignments. In addition, trail corridors are proposed which tie into the County system.

Specific Plan References — Section 5.2, Public Parks. Section 5.1, Public Schools. Section 6.1, Open Space System. Section 7.6b, Hiking/Riding Trails.

POLICY 3.7:

It is the policy of the City of Lake Elsinore to provide incentives for the maintenance and restoration of cultural and historic resources in the City and Planning Area.

Objective 3.7a:

Identify and preserve historical and cultural resources within the planning area.

Objective 3.7b:

Identify and preserve significant archaeological sites within the Planning Area.

The Specific Plan and Environmental Impact Report outline programs for the mitigation of all identified archaeological and historic sites on the property.

Specific Plan References — Section 6.7, Cultural Resources.

C.4d Public Health and Safety

POLICY 3.8:

It is the policy of the City of Lake Elsinore to prohibit construction of intense urbanized uses in areas of geologic or seismic hazard.

Objective 3.8:

Reduce the loss of life, property and the economic and social dislocations resulting from geologic and seismic activity.

Development in the specific plan area is subject to grading and engineering guidelines which mitigate any seismic - or geologic - related negative impacts.

Specific Plan References — Section 6.2, Grading.

POLICY 3.9:

It is the policy of the City of Lake Elsinore to prohibit construction of intense urbanized uses in areas of moderate to steep slopes.

Objective 3.9:

Reduce the loss of life, damage to property, and the economic and social dislocations resulting from slope failures and mudslides.

The Specific Plan provides development guidelines for grading, drainage, and phasing of grading, to protect against landslides and severe erosion. In addition, steep slopes and major water courses are preserved in permanent open space.

Specific Plan References — Section 6.1, Open Space System. Section 6.4, Grading. Section 6.5, Drainage and Hydrology. Section 9.3d, Phasing of Grading.

POLICY 3.10:

It is the policy of the City of Lake Elsinore to prohibit construction of intense urbanized uses in areas of potential hydrologic hazards.

Objective 3.10a:

Minimize loss of life, damage to property, and social and economic dislocations resulting from flood or dam failure hazards.

The San Jacinto River and Cottonwood Creek drainage courses are preserved in open space, protecting development from flooding. The Specific Plan specifies drainage development guidelines to avoid drainage impacts.

Specific Plan References — Section 6.1, Open Space System. Section 6.5, Drainage and Hydrology. Section 9.3d, Phasing of Grading.

POLICY 3.11:

It is the policy of the City of Lake Elsinore to restrict construction of structures in areas susceptible to wildland fires, while assuring the availability of adequate fire protection in existing and newly urbanized portions of the planning area.

Objective 3.11a:

Reduce the loss of life, damage to property and the economic and social dislocations resulting from wildland and structural fires.

The Specific Plan outlines a fire management program complementing the City's fire protection service.

Specific Plan References — Section 5.3a, Fire Protection. Section 6.4c, Natural Open Space Interfaces. Section 6.2d, Fire Management.

POLICY 3.12:

It is the policy of the City of Lake Elsinore to assure the availability of adequate police protection.

Objective 3.12a:

Reduce the loss of life, damage to property, and the economic and social dislocations resulting from breaches of security and violations of law.

The Specific Plan includes design guidelines for defensible space and security, complementing the City's police protection service.

Specific Plan References — Section 3.2, Residential Design Guidelines. Section 4.1c, Commercial Design Guidelines. Section 5.3b, Police Protection. Section 5.5, Public Facilities Implementation. Section 5.1c, School Design Guidelines. Section 5.2b, Neighborhood Parks. Section 5.2c, Community Park.

C.5 Noise Element

POLICY 5.1:

It is the policy of the City of Lake Elsinore to establish and support a coordinated program to protect and improve the noise environment in the City.

Objective 5.1a:

Protect and enhance the City's noise environment by simultaneously controlling noise at its source, along its transmission paths, and at the site of the ultimate receiver. First priority shall be given to residential areas to assure an environment free from excessive or damaging noise. Control of noise at its source shall be given priority over changes to residential structures or neighborhoods.

The Specific Plan addresses the City's Noise Element by outlining a series of traffic noise mitigation measures for development adjacent to major roads; and by specifying buffer treatments between residential and other land uses.

Specific Plan References -- Section 7.2d, Traffic Noise Mitigation. Section 4.1c, Commercial Design Guidelines. Section 4.2c, Institutional Design Guidelines. Section 5.1c, School Design Guidelines. Section 5.2b, Neighborhood Parks. Section 5.2c, Community Park.

C.6 Community Design Element

Conformance of the Specific Plan to the Community Design Element (1982) of the General Plan is discussed below. Note that the project site is not in an existing Community Design Study Area, but is proposed as a new, separate area to be designed as a single, distinctive unit.

POLICY 6.1:

It is the policy of the City of Lake Elsinore to create the highest order of visual continuity and functional compatibility among the various physical and historic components of the Lake Elsinore community.

The Cottonwood Hills project is proposed for development as an integrated community, both visually and functionally. The Specific Plan calls out design guidelines for maximizing views, undergrounding utilities, maintaining a low-rise scale, and establishing aesthetic continuity. These guidelines encompass development projects, public facilities and streetscapes.

Specific Plan References — Section 2.2, Overall Specific Plan. See also, Objectives 6.1a - 6.1d below.

Objective 6.1a:

Recognize and protect major views in the City with particular attention to those of scenic hillsides and the lake.

The Specific Plan lays out community roadways to take advantage of permanent open space vistas. Use of view opportunities is addressed in residential design guidelines.

Specific Plan References — Section 3.2, Residential Design Guidelines. Section 6.1, Open Space System. Section 6.4, Grading. Section 7.2, Streetscape Design.

Objective 6.1b:

Enhance the general quality of design and emphasize the unique character of each residential neighborhood, and commercial and industrial areas.

Design guidelines are provided in the Specific Plan to enhance the quality of development for each project planning unit.

Specific Plan References — Section 3.2, Residential Design Guidelines. Section 4.1c, Commercial Design Guidelines. Section 4.2c, Institutional Design Guidelines.

Objective 6.1c:

Encourage the preservation of buildings which have historic and/or architectural merit.

Not applicable to Specific Plan.

Objective 6.1d:

Maintain the present scale of buildings within the City.

The Specific Plan preserves a height limit of no more than three stories for all land uses, as well as coverage limitations and open space requirements consistent with a suburban level of development.

Specific Plan References -- Chapter 8, Zoning Element.

POLICY 6.2:

It is the policy of the City of Lake Elsinore to protect the scenic characteristics of local roads, especially scenic routes.

Objective 6.2a:

Provide safe, attractive, scenic routes which will serve the motoring public, bicyclists, and pedestrians.

The Specific Plan treats the design of major roads as scenic routes. This includes open space vistas, coordinated streetscape treatments along developments, a continuous parkway concept, and coordinated community entrances.

Specific Plan References -- Section 6.1, Open Space System. Section 7.2, Streetscape Design. Section 7.5, Bicycle Circulation. Section 7.6a, Pedestrian System.

C.7 Housing Element

The primary focus of the Housing Element (1982) of the City of Lake Elsinore General Plan is the provision of housing for residents with specialized needs (low income, elderly, handicapped), through rehabilitation and infill. The Cottonwood Hills Specific Plan encourages development of new housing for a range of users, but with an emphasis on single-family detached residential. This approach is in conformance with the draft Land Use Element of the General Plan, which specifies preferred consideration to development proposals which provide for middle and upper income residential development (see Section C.2).

POLICY 8.1a:

The City of Lake Elsinore shall recognize the existing housing needs of current residents.

Not applicable to Specific Plan.

POLICY 8.1b:

The City of Lake Elsinore shall recognize the housing needs of the future population.

The Specific Plan recognizes the housing needs of the future population of the City of Lake Elsinore by providing a new residential community with a mix of housing products for a range of income levels.

Specific Plan References — Section 3.1, Residential Land Use.

POLICY 8.1c:

The City of Lake Elsinore shall recognize the housing needs for residents with specialized needs.

Residential development within Cottonwood Hills will conform with State requirements for handicapped accessibility.

Specific Plan References — Section 3.1c, Balanced Community.

POLICY 8.1d:

The City of Lake Elsinore shall maximize the utilization of local groups such as the Community Development Corporation, Redevelopment Agency, Local Development Corporation, Elsinore Aid and Senior Citizen Information Center to implement housing development and referral programs.

Not applicable to Specific Plan.

POLICY 8.1e:

Streamline and clarify administrative procedures for granting approval and issuing permits. Establish time limits for such approvals allowing developers to ascertain development costs.

The Specific Plan provides for an efficient, orderly approval process for residential development.

Specific Plan References — Section 9.1, Implementation Process.

POLICY 8.1f:

Actively encourage efforts of private lenders to provide alternative financing methods to make home ownership available to a greater number of households.

The Specific Plan creates a balanced community, with a mix of housing types for a range of household incomes.

Specific Plan References — Section 3.1c, Balanced Community.

POLICY 8.1g:

Actively assist private developers in identifying and preparing land suitable for housing development for groups with specialized needs.

See Policy 8.1c.

POLICY 8.2a:

Actively assist private developers in identifying and preparing land suitable for housing development.

Not applicable to Specific Plan.

POLICY 8.2b:

Protect neighborhoods from adverse environmental factors.

While not an existing neighborhood, Cottonwood Hills is planned to protect housing and commercial uses from adverse environmental factors. Development of the Specific Plan area does not detrimentally affect existing City neighborhoods.

Specific Plan References — Chapter 6, Resources Management Element.

POLICY 8.2c:

Improve the physical character of existing neighborhoods.

Not applicable to Specific Plan.

POLICY 8.2d:

Provide a comprehensive coordinated effort to improve city infrastructure.

Although Cottonwood Hills is not an existing neighborhood, the Specific Plan provides a comprehensive coordinated effort to improve and add City infrastructure. This includes streets, utilities, and other public facilities.

Specific Plan References — Section 5.3, Public Services. Section 5.4, Utilities. Section 5.5, Public Facilities and Services Implementation. Section 7.1, Street System. Section 7.7, Circulation Implementation. Section 9.3, Phasing Program.

POLICY 8.3a:

Maximize use of all housing rehabilitation programs.

Not applicable to Specific Plan.

POLICY 8.3b:

Promote the maintenance of existing housing stock and replace or rehabilitate housing by utilizing all legal actions available to the City.

Not applicable to Specific Plan.

POLICY 8.3c:

Encourage investment of public and private resources to reverse the trend of deteriorating neighborhoods.

Not applicable to Specific Plan.

POLICY 8.4a:

Encourage energy conserving and environmentally sensitive site planning, construction, and rehabilitation techniques.

Energy conservation measures for housing are addressed in the Specific Plan.

Specific Plan References -- Section 6.6, Energy Conservation.

POLICY 8.5a:

Encourage public and private efforts to eliminate all forms of discrimination in housing.

An affirmative marketing plan is expected to be utilized in conjunction with all residential projects.

Specific Plan References -- Section 3.1c, Balanced Community.

POLICY 8.5b:

Promote representative citizen participation in the formulation, implementation, and review of housing programs.

The Specific Plan is subject to public review and hearings during the approval process.

Specific Plan References -- Section 1.2, Significance of Specific Plan.

Appendix D - APPROVAL DOCUMENTS

Ordinance No. 854, An Ordinance of the City Council of the City of Lake Elsinore, California, Adopting the Cottonwood Hills Specific Plan	D-2
Resolution No. 89-5, a Resolution of the City Council of the City of Lake Elsinore, California, Making an Amendment to the Land Use Element of the Lake Elsinore General Plan for the First Cycle of the Calendar Year of 1989.	D-7
Resolution No. 89-6, a Resolution of the City Council of the City of Lake Elsinore, California, Consenting to Commencement of Proceedings to Annex to the City of Lake Elsinore Certain Uninhabited Territory Described Herein and Designated "Annexation No. 44 -- Cottonwood Hills Annexation."	D-11
Conditions of Approval for Specific Plan 88-1 (Cottonwood Hills)	D-14
Exhibit B, Mitigation Monitoring Program, Specific Plan 88-1	D-19

ORDINANCE NO. 854

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF LAKE ELSINORE, CALIFORNIA, ADOPTING THE COTTONWOOD HILLS SPECIFIC PLAN.

THE CITY COUNCIL OF THE CITY OF LAKE ELSINORE, CALIFORNIA, DOES ORDAIN AS FOLLOWS:

SECTION 1: That the Cottonwood Hills Specific Plan is hereby approved and adopted by the City Council based on the following findings:

1. The Specific Plan is anticipated to result in several significant adverse environmental impacts which are described in the project Environmental Impact Report. For each significant impact, measures are imposed by the Plan or its conditions of approval which eliminate or substantially lessen their effect. A number of significant impacts are unavoidable and a statement of overriding considerations is recommended to address these (see Finding #2). Specific findings for each significant impact are as listed in Exhibit A (attached). A Mitigation Monitoring Program is hereby adopted. A list of persons and agencies commenting and further responses to comments are hereby adopted as part of the Final E.I.R.
2. Based upon project documents and evidence in the public record, the significant environmental effects of this project associated with the loss of on-site biological habitat, traffic impacts on area roads, degradation of local air quality and a substantial contribution to cumulative impacts of area-wide urban development, although partially mitigated or reduced by the project, remain significant and are unavoidable based upon rejection of project alternatives or additional mitigation measures necessary to reduce these impacts to less than significant levels. These impacts are found to be acceptable due to benefits derived by the project, specifically the provision of quality housing opportunities by the City, the anticipated increase in local government revenues generated by project residents, and the provision of significant improvements to Railroad Canyon Road all of which are expected to support local commercial and industrial development efforts and generate measurable benefits to the local economy and fiscal integrity of City government. The foregoing overriding considerations provide the rationale for a decision to approve this project.
3. The Cottonwood Hills Specific Plan meets the Specific Plan criteria for contents and systematic implementation of the General Plan established by Section 65450 of the California Government Code and Section 17.99 of the City of Lake Elsinore Municipal Code.

4. The Specific Plan is consistent with the City of Lake Elsinore General Plan as follows:

Land Use Element

- a) The Specific Plan (Plan) would provide for residential support of City commercial and industrial development thereby promoting a community-wide balance of land uses.
- b) Services and facilities could be delivered as needed under provisions of the Plan.
- c) Quality site development would be promoted under provisions of the Plan.

Circulation Element

- d) The regionally important link of Railroad Canyon Road west to I-15 would be provided. Opportunities for future linkage improvements to the south and east are preserved.
- e) Transit parking and bike lanes are accommodated.

Environmental Element

- f) Major areas of open space/wildlife habitat are preserved.
- g) No important mineral resources are impacted.
- h) Adverse air quality impacts are partially mitigated.
- i) Prime agricultural lands are being converted to urban uses in a timely manner, based upon surrounding land use and economic conditions.
- j) Recreation opportunities are accommodated.
- k) Cultural resources are anticipated to be adequately mitigated based upon their significance.
- l) Health and safety threats are mitigated by design measures and service delivery mechanisms.

Noise Element

- m) Design measures are anticipated to be imposed during project development.

Community Design Element

- n) Design features are provided to improve the visual quality of the community.

Housing Element

- o) Major segments of the local housing market will be accommodated, including moderate income persons. The project will compliment existing housing opportunities elsewhere in the community for low income persons.
5. The Specific Plan establishes pre-zoning of the project area in anticipation of its annexation to the City of Lake Elsinore.

6. The Specific Plan shall be effective at such time as annexation of the project area to the City of Lake Elsinore is approved by the City and the Local Agency Formation Commission and recorded.
7. The Specific Plan shall expire and be of no effect whatsoever seven years after the date of annexation recordation (as specified in #6 above), unless an implementing Final Tract Map has been recorded prior to the end of the seven year period.
8. The Specific Plan will not be detrimental to the health, safety, comfort or general welfare of the persons residing or working within the neighborhood of the project area, not will it be injurious to property or improvements in that area or the City as a whole, based upon the provisions of the Plan, mitigation measures and Conditions of Approval.
9. A General Plan density designation of 2.17 units per gross acre provides for a transfer of density from all open space areas into adjacent developed areas, and for a density bonus of .17 dwelling units/acre in exchange for provision of Railroad Canyon Road infrastructure at a more extensive level than would otherwise be required by the project, and provision of complete turn-key neighborhood parks and partial community park infrastructure above that which would normally be required. The developer shall convey all development rights of all project open space to the City to be retained in perpetuity and to insure provision of Railroad Canyon Road and parks as specified in this finding and Specific Plan Condition of Approval.

SECTION 2: That the City Clerk of the City of Lake Elsinore shall certify to the passage and adoption of this ordinance and shall cause the same to be published in the Sun Tribune, a newspaper of general circulation, serving the City of Lake Elsinore, in accordance with provisions of the Government Code.

INTRODUCED AND APPROVED UPON FIRST READING this 14th day of March, 1989, upon the following roll call vote:

AYES: COUNCILMEMBER: Buck, Dominguez, Starkey, Washburn,
Winkler

NOES: None

ABSENT: None

ABSTENTIONS: None

PASSED, APPROVED AND ADOPTED UPON SECOND READING this 28th day of March, 1989, upon the following roll call vote:

AYES: COUNCILMEMBER: Buck, Dominguez, Starkey, Washburn,
Winkler

NOES: None

ABSENT: None

ABSTENTIONS: None

/s/ Jim Winkler
Jim Winkler, Mayor

ATTEST:

/s/ Adria L. Bryning
Adria L. Bryning
Deputy City Clerk

(SEAL)

APPROVED AS TO FORM AND LEGALITY:

/s/ John R. Harper
John R. Harper, City Attorney

TABLE 1 (FINDING #1 SP 88-1)

SIGNIFICANT IMPACTS AND FINDINGS OF FACT

- a) Grading - substantially lessened by guidelines contained within the Plan and by existing City ordinances to less than significant levels.
- b) Visual Impacts - substantially lessened by the above guidelines, grade and landscaping buffers to less than significant levels.
- c) Biological Impacts - substantially lessened by avoidance and compensation for riparian habitat losses. Remainder sensitive species habitat to be partially compensated by change in management status to less than significant levels. Stephens' kangaroo rat habitat losses to be partially compensated by an off-site plan which can be adopted and is within the responsibility of the City, County, US Fish and Wildlife Service (USFWS) and State Fish and Game (CFG) agencies. These impacts may not be reduced to less than significant levels and are therefore the subject of the attached statement of overriding considerations.
- d) Cultural Resources - substantially lessened by the study and documentation of each site as conditioned and preservation of significant resources such as the cemetery. These measures will reduce the impact to less than significant levels.
- e) Traffic Circulation - Substantially lessened by the development of on-site roads and Railroad Canyon Road and Holland Road as required by the Plan and conditions of approval. Expansion to Railroad Canyon Road is dependent upon actions which can and should be taken by USFWS and CFG related to wetlands mitigations. Traffic impact will not be reduced to less than significant levels by these measures and is therefore the subject of the attached Statement of Overriding Consideration.
- f) Services and Utilities - substantially lessened by the provision of services and utilities as required by the Plan and conditions of approval to less than significant levels for all impacts except elementary schools. Provision of schools facilities are dependent upon actions by the school districts and the state which can and should be taken related to funding and construction of needed facilities.
- g) Hydrology and Water Quality - substantially lessened by the guidelines and control measures required by the Plan and conditions of approval to less than significant levels.
- h) Geology and Soils - substantially lessened by application of the measures outlined in the Plan and geological report to a level of insignificance.
- i) Noise - substantially lessened by requirements of the Plan for grading design and sound attenuation barriers during construction phases to less than significant levels.
- j) Air Quality - substantially lessened by grading guidelines of the Plan and City ordinances for short-term impacts. Long-term traffic generated impact is expected to be significant and is therefore the subject of the attached Statement of Overriding Considerations.
- k) Energy Conservation - substantially lessened by design guidelines of the Plan and conditions of approval to less than significant levels.
- l) Cumulative Impacts - substantially lessened by provisions of the Plan, but these impacts will not be reduced to less than significant levels and are therefore the subject of the attached Statement of Overriding Considerations.

EXHIBIT "A"

RESOLUTION NO. 89-5

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LAKE ELSINORE, CALIFORNIA, MAKING AN AMENDMENT TO THE LAND USE ELEMENT OF THE LAKE ELSINORE GENERAL PLAN FOR THE FIRST CYCLE OF THE CALENDAR YEAR OF 1989.

THE CITY COUNCIL OF THE CITY OF LAKE ELSINORE, CALIFORNIA, DOES HEREBY RESOLVE AS FOLLOWS:

WHEREAS, Section 65361(a) of the Government Code provides that no mandatory element of a General Plan shall be amended more frequently than four times during any calendar year; and

WHEREAS, the Planning Commission held public hearings on this round of General Plan Amendments on January 17, 1989, and that this public hearing was advertised as required by law. The Planning Commission made a recommendation to the City Council concerning this General Plan Amendment and has filed with the City Council copies of map and report; and

WHEREAS, notice was duly given of the public hearing on the Amendment, which public hearing was held before the City Council on the 14th day of March, 1989, at the hour of 7:00 p.m., with testimony received being made a part of the public record; and

WHEREAS, the City Council certifies that the Final Environmental Impact Report has been completed in compliance with the California Environmental Quality Act and that the information contained within the Environmental Impact Report has been reviewed and considered by the Council prior to approving the project.

NOW, THEREFORE, in consideration of the evidence received at the hearing and for the reasons discussed by the Council members at said hearing, the City Council now finds that the Lake Elsinore General Plan be amended as follows:

A. GENERAL PLAN AMENDMENT 88-1

APPLICANT: Pardee Construction Company
PROPERTY OWNER: Clyde C. Christiansen;
Corona Foothill Company
LOCATION: Along Railroad Canyon Road, Cottonwood
and Holland Roads, one-half mile south of
Canyon Lake.

Change approximately 1,968.7 acres from Riverside County Category II and Mountainous to Specific Plan Area, with a density of 2.17 dwelling units per acre.

Approval is based on the following:

1. This request is anticipated to result in several significant adverse environmental impacts associated with the development of the site as allowed under the General Plan. These impacts are described in the Cottonwood Hills Environmental Impact Report for the proposed Specific Plan. For each significant impact, measures are imposed to eliminate or substantially lessen their effect. A number of these significant impacts are unavoidable and a statement of overriding consideration is recommended as Finding #2. Specific findings on each significant impact are as listed in Table 1 attached hereto as Exhibit A.

2. This request will result in significant environmental impacts associated with loss of habitat, traffic circulation, degradation of air quality and a substantial contribution to the cumulative impacts of area-wide urban development which may be partially mitigated but are anticipated to remain significant upon development of the site as allowed under the General Plan. These impacts are found to be acceptable due to due to benefits derived by development under the General Plan, specifically the provision of quality housing opportunities by the City, the anticipated increase in local government revenues generated by project residents and the provision of significant improvements to Railroad Canyon Road, all of which are expected to support local commercial and industrial development efforts and generate measurable benefits to the local economy and fiscal integrity of City government. The foregoing overriding considerations provide the rationale for a decision to approve this request.
3. The General Plan Amendment is found to be in conformity with the General Plan as follows:

Land Use Element

- a) The Specific Plan (Plan) would provide for residential support of City commercial and industrial development thereby promoting a community-wide balance of land uses.
- b) Services and facilities could be delivered as needed under provisions of the Plan.
- c) Quality site development would be promoted under provisions of the Plan.

Circulation Element

- d) The regionally important link of Railroad Canyon Road west to I-15 would be provided. Opportunities for future linkage improvements to the south and east are preserved.
- e) Transit parking and bike lanes are accommodated.

Environmental Resource Element

- f) Major areas of open space/wildlife habitat are preserved.
- g) No important mineral resources are impacted.
- h) Adverse air quality impacts are partially mitigated.
- i) Prime agricultural lands are being converted to urban uses in a timely manner, based upon surrounding land use and economic conditions.
- j) Recreation opportunities are accommodated.
- k) Cultural resources are anticipated to be adequately mitigated based upon their significance.
- l) Health and safety threats are mitigated by design measures and service delivery mechanisms.

Noise Element

- m) Design measures are anticipated to be imposed during project development.

Community Design Element

- n) Design features are provided to improve the visual quality of the community.

Housing Element

- o) Major segments of the local housing market will be accommodated, including moderate income persons. The project will compliment existing housing opportunities elsewhere in the community for low income persons.
- 4. This General Plan Amendment would provide for residential support of City commercial and industrial development thereby promoting a community-wide balance of land uses.
- 5. This General Plan Amendment density designation of 2.17 units per gross acre provides for a transfer of density from all open space areas into adjacent developed areas, and for a density bonus of .17 dwelling units/acre in exchange for provision of Railroad Canyon Road infrastructure at a more extensive level than would otherwise be required by the project, and provision of complete turn-key neighborhood parks and partial community park infrastructure above that which would normally be required.
- 6. This Amendment will allow land uses in character with the subject property's location, access and constraints.
- 7. This Amendment will permit reasonable development of the property consistent with its constraints and compatible with adjacent properties and proposed development.
- 8. This General Plan Amendment permits development to the highest and best nature within mitigable means to insure maintenance of the general public health, safety and welfare.
- 9. The proposed Amendment will not adversely affect surrounding property with respect to value or precedent.

PURSUANT TO THE ABOVE FINDINGS, IT IS RESOLVED by the City Council of the City of Lake Elsinore, California, that the City of Lake Elsinore General Plan Land Use Map be amended for the first time in calendar year 1989 to reflect General Plan Amendment 88-1

PASSED, APPROVED AND ADOPTED this 14th day of March, 1989, by the following vote:

AYES: COUNCILMEMBERS: BUCK, DOMINGUEZ, STARKEY, WASHBURN, WINKLER.

NOES: COUNCILMEMBERS: NONE

ABSENT: COUNCILMEMBERS: NONE

ABSTENTIONS: COUNCILMEMBERS: NONE

I declare under penalty of perjury that the foregoing is a full, true and correct copy of the original as the same appears in the office.

ATTEST: Adria L. Bryning
VICKI KASAD, City Clerk
DATED 3/14/89

Jim Winkler
Jim Winkler, Mayor

ATTEST:
Adria L. Bryning
Adria L. Bryning,
Deputy City Clerk
(SEAL)

APPROVED AS TO FORM AND LEGALITY:
John R. Harper
John R. Harper, City Attorney

TABLE 1 (FINDING #1 SP 88-1)

SIGNIFICANT IMPACTS AND FINDINGS OF FACT

- a) Grading - substantially lessened by guidelines contained within the Plan and by existing City ordinances to less than significant levels.
- b) Visual Impacts - substantially lessened by the above guidelines, grade and landscaping buffers to less than significant levels.
- c) Biological Impacts - substantially lessened by avoidance and compensation for riparian habitat losses. Remainder sensitive species habitat to be partially compensated by change in management status to less than significant levels. Stephens' kangaroo rat habitat losses to be partially compensated by an off-site plan which can be adopted and is within the responsibility of the City, County, US Fish and Wildlife Service (USFWS) and State Fish and Game (CFG) agencies. These impacts may not be reduced to less than significant levels and are therefore the subject of the attached statement of overriding considerations.
- d) Cultural Resources - substantially lessened by the study and documentation of each site as conditioned and preservation of significant resources such as the cemetery. These measures will reduce the impact to less than significant levels.
- e) Traffic Circulation - Substantially lessened by the development of on-site roads and Railroad Canyon Road and Holland Road as required by the Plan and conditions of approval. Expansion to Railroad Canyon Road is dependent upon actions which can and should be taken by USFWS and CFG related to wetlands mitigations. Traffic impact will not be reduced to less than significant levels by these measures and is therefore the subject of the attached Statement of Overriding Consideration.
- f) Services and Utilities - substantially lessened by the provision of services and utilities as required by the Plan and conditions of approval to less than significant levels for all impacts except elementary schools. Provision of schools facilities are dependent upon actions by the school districts and the state which can and should be taken related to funding and construction of needed facilities.
- g) Hydrology and Water Quality - substantially lessened by the guidelines and control measures required by the Plan and conditions of approval to less than significant levels.
- h) Geology and Soils - substantially lessened by application of the measures outlined in the Plan and geological report to a level of insignificance.
- i) Noise - substantially lessened by requirements of the Plan for grading design and sound attenuation barriers during construction phases to less than significant levels.
- j) Air Quality - substantially lessened by grading guidelines of the Plan and City ordinances for short-term impacts. Long-term traffic generated impact is expected to be significant and is therefore the subject of the attached Statement of Overriding Considerations.
- k) Energy Conservation - substantially lessened by design guidelines of the Plan and conditions of approval to less than significant levels.
- l) Cumulative Impacts - substantially lessened by provisions of the Plan, but these impacts will not be reduced to less than significant levels and are therefore the subject of the attached Statement of Overriding Considerations.

EXHIBIT "A"

RESOLUTION NO. 89-6

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LAKE ELSINORE, CALIFORNIA, CONSENTING TO THE COMMENCEMENT OF PROCEEDINGS TO ANNEX TO THE CITY OF LAKE ELSINORE CERTAIN UNINHABITED TERRITORY DESCRIBED HEREIN AND DESIGNATED "ANNEXATION NO. 44 -- COTTONWOOD HILLS ANNEXATION."

WHEREAS, on September 2, 1989, the proponent of the Annexation, known as "Annexation No. 44 -- Cottonwood Hills Annexation," requested that the City Council consent to the commencement of proceedings to annex said area, which is contiguous to the City of Lake Elsinore; and

WHEREAS, the said annexation area is entirely within the City Sphere of Influence except for the easterly sixty (60) acres, which is the subject of an application for an expansion to the Sphere; and

WHEREAS, the City Planning Commission, at its regular meeting on January 17, 1989, made its report upon the desirability of said Sphere expansion and annexation and made its recommendations in favor of said annexation; and

WHEREAS, it is the desire of the City Council to give its consent to the Sphere of Influence expansion request and commencement of annexation proceedings.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF LAKE ELSINORE, CALIFORNIA:

1. That consent be and hereby is granted for the commencement of annexation proceedings and a Sphere of Influence Expansion in accord with California Government Code Section 56000 et seq. for the uninhabited territory designated hereby as "Cottonwood Hills Annexation No. 44," which consists of 1,968.7 acres located along Railroad Canyon Road, Cottonwood and Holland Roads, one-half mile south of Canyon Lake, and which is shown on the map designated as Exhibit "A" attached hereto and made a part hereof.

2. The City Clerk shall certify to the adoption of this resolution.

PASSED, APPROVED AND ADOPTED this 14th day of March, 1989, at a regular meeting of the City Council of the City of Lake Elsinore, California, by the following vote:

AYES: COUNCILMEMBERS: RUCK, DOMINGUEZ, STARKEY, WASHBURN, WINKLER

NOES: COUNCILMEMBERS: NONE

ABSENT: COUNCILMEMBERS: NONE

ABSTENTIONS: COUNCILMEMBERS: NONE



Jim Winkler, Mayor

ATTEST:

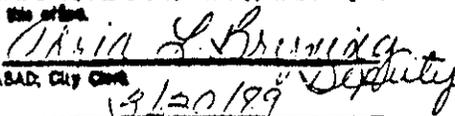

Adria L. Bryning, Deputy City Clerk

(SEAL)

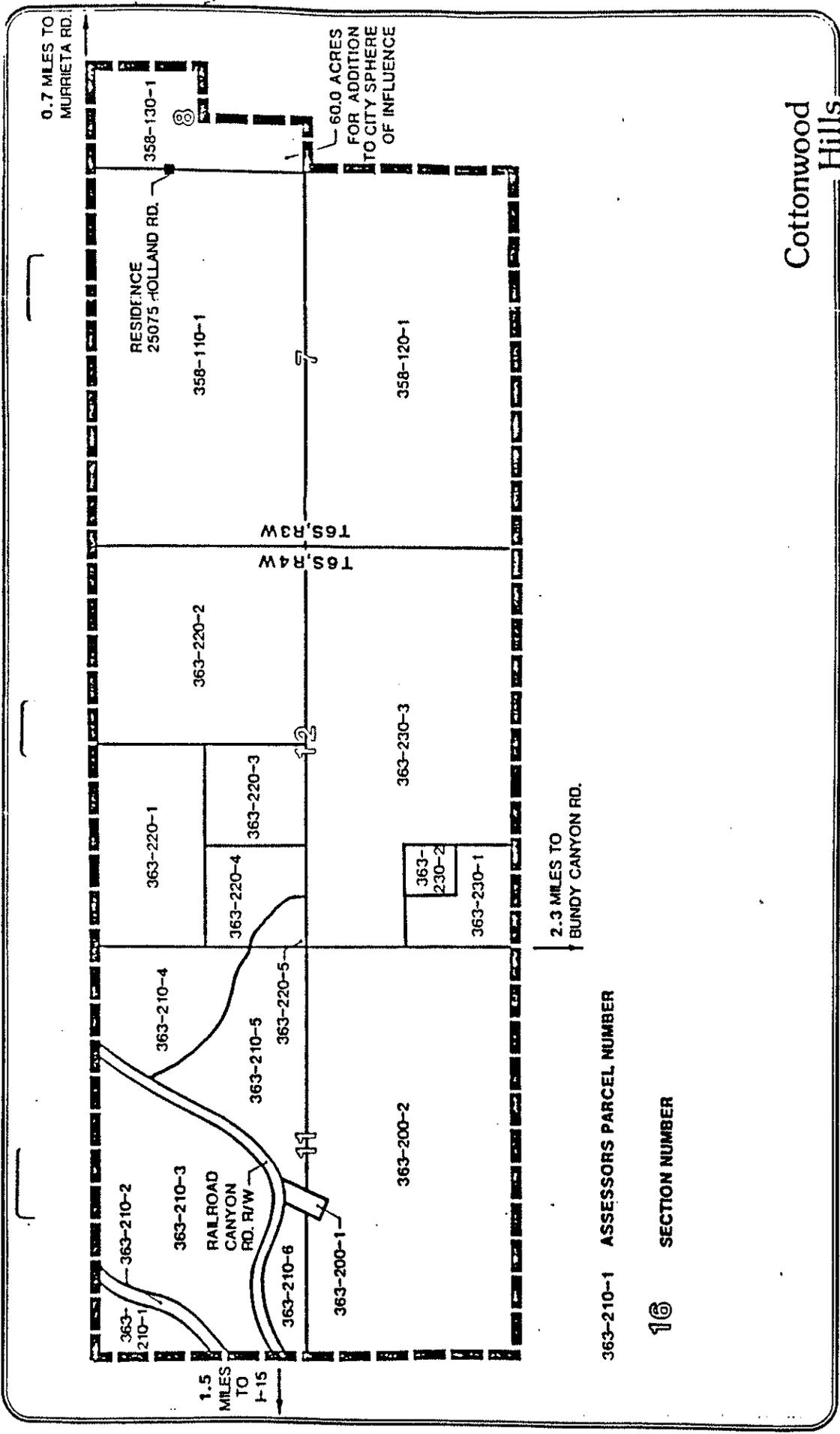
APPROVED AS TO FORM AND LEGALITY:


John R. Harper, City Attorney

I declare under penalty of perjury that the foregoing instrument is a full, true and correct copy of the original on file in this office.

ATTEST: 
VICKI KASAB, City Clerk

DATED 3/20/89



Cottonwood Hills

Annexation Area Map

363-210-1 ASSESSORS PARCEL NUMBER
 16 SECTION NUMBER

Project Design Consultants

6-25-88

CONDITIONS OF APPROVAL FOR SPECIFIC PLAN 88-1 (COTTONWOOD HILLS)

1. The Specific Plan shall be effective at such time as annexation of the project area to the City of Lake Elsinore is approved by the City and the Local Agency Formation Commission and recorded.
2. A General Plan density designation of 2.17 units per gross acre provides for a transfer of density from all open space areas into adjacent developed areas, and for a density bonus of .17 dwelling units/acre in exchange for provision of Railroad Canyon Road infrastructure at a more extensive level than would otherwise be required by the project, and provision of complete turn-key neighborhood parks and partial community park infrastructure above that which would normally be required. The developer shall convey all development rights of all project open space to the City to be retained in perpetuity and to insure provision of Railroad Canyon Road and parks as specified in this finding and Specific Plan Condition of Approval.
3. The text of the draft Final Specific Plan shall be revised to correct any typographical errors including:

Page 8-55, under 8.9f, Exemptions, Item 12 and Page 8-62, under 8.9.p, Commercial Project Signage, Item K: Replace the term "short-term" with the term "temporary."
4. The developer shall produce seven (7) copies of the Final Specific Plan plus an original camera-ready copy which shall be delivered to the City prior to the effective date of the Plan.
5. The Environmental Impact Report shall be produced in Final format in consultation with staff with seven (7) copies plus an original camera-ready copy delivered to the City prior to the effective date of the Plan to include:
 - a) Certification by City Council.
 - b) Comments and responses to comments received through City Council hearings.
 - c) A list of persons and agencies commenting through City Council hearings.
 - d) Any other revisions to the text to correct any typographical errors.
6. A Railroad Canyon Road/San Jacinto River revegetation program shall be reviewed and approved by the City Planning Division prior to approval of grading permit for Railroad Canyon Road. Federal and state wetlands permits shall also be approved prior to grading. The program shall be implemented concurrent with construction to meet the approval of the Community Development Director.
7. A Cottonwood Creek floodplain modification and revegetation program shall be incorporated as a condition of each affected tentative tract map approval to be complied with prior to final map approval or grading permit (whichever occurs first) and which shall take precedence, requiring revisions to the map to comply with the program if necessary. Implementation shall be concurrent with grading and construction of related phase.

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CONDITIONS OF APPROVAL FOR SPECIFIC PLAN 88-1 (COTTONWOOD HILLS)

8. A Stephens' kangaroo rat impact mitigation program shall be incorporated as a condition of each tentative tract map approval and shall be complied with prior to final map approval or grading permit, whichever occurs first. Implementation to compensate for habitat loss shall occur prior to grading permits for occupied habitat on-site.
9. A Cultural Resources survey of affected areas related to Railroad Canyon Road shall be evaluated and approved by the City prior to project grading. Presence of an archeologist or paleontologist for on-site evaluation and full mitigation is required should any resource be uncovered during construction.
10. A cultural resources mitigation program shall be incorporated as a condition of each affected tentative tract map approval to be complied with prior to final map approval or grading permit (whichever occurs first) and which shall take precedence, requiring revisions to the map to comply with the program if necessary. The program shall be based on site evaluations by qualified archeologists and historians to establish significance of each site and a detailed plan for appropriate mitigation, subject to the approval of the Community Development Director.
11. Tentative Tract Maps shall indicate and provide for the base flood (100-year flood) within the existing channel and related facilities of the San Jacinto River and Cottonwood Creek. All other flood areas shall be provided with facilities to convey waters to these channels which shall be required to be implemented concurrent with construction of related phase subject to the approval of the Community Development Director and the City Engineer.
12. All structures shall be designed to incorporate all state and local water conservation regulations, subject to the approval of the Chief Building Official.
13. All site-planning shall incorporate measures to promote waste reduction and recycling to the extent feasible, subject to the approval of the Community Development Director.
14. Grading and construction plans shall incorporate the measures listed in the EIR to reduce and control erosion potential, subject to the approval of the Chief Building Official.
15. Developer shall provide for the paving of Holland Road for two travel lanes from the project boundary east to existing pavement, in the event that said improvement has not previously been provided by Audie Murphy Ranch. This improvement shall meet the approval of the County Road Department to partially mitigate project related traffic impacts as they occur.
16. A program shall be implemented for the construction of Railroad Canyon Road from I-15 to the north project boundary. The program shall be subject to approval of the Community Development Director and the City Engineer prior to issuance of grading permits. The program shall provide for the funding and start of construction of Phase 1 of Railroad Canyon Road prior to issuance of the first building permit; and further that the road construction shall be

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CONDITIONS OF APPROVAL FOR SPECIFIC PLAN 88-1 (COTTONWOOD HILLS)

completed in any case no later than prior to 501st occupancy permit issuance within the project. Phase 1 of Railroad Canyon Road shall consist of a minimum of 110 feet of right-of-way, with full width grading and four (4) lanes of pavement, of which at least two (2) lanes must be reopened to traffic prior to occupying the 501st unit. The City will make its best efforts to create a benefit district or similar program for Railroad Canyon Road which provides for an equitable sharing of costs between all properties within the City accessing the road. This limitation shall not apply to planning areas 28 and 33 which take principal access from Holland Road. The final design and ultimate improvements of Railroad Canyon Road shall be approved by the City Engineer and Community Development Director prior to First Phase Tentative Tract Map approval. Construction of ultimate improvements to full six-lanes width shall be completed prior to Final Tract Map approval of the Final Phase of Specific Plan development.

17. Library Impact Program shall be developed in consultation with the City to off-set capital costs to mitigate impacts of the project.
18. Developer shall incorporate mitigation for isolated oak tree removal throughout the site into the Cottonwood Creek Revegetation Program. Such mitigation shall occur on a 10:1 replacement ratio after individual trees have been evaluated for preservation feasibility by the City in consultation with the developer. This program shall be incorporated as a condition of each affected tentative tract map approval to be complied with prior to final map approval or grading permit (whichever occurs first) and which shall take precedence requiring revisions to the map to comply with the program if necessary.
19. A Fire Protection Impact Mitigation Program shall be reviewed in consultation with Riverside County Fire Department subject to approval by the Community Development Director prior to approval of first final map, which shall include:
 - a. Proportional participation in the establishment of a southeast area fire station and a mechanism for its timely delivery concurrent with demand for services imposed by this project.
 - b. Wildland interface design and management as conveyed in the Specific Plan and other measures required to appropriately reduce fire hazard. This Program shall be reviewed and approved by both the City and County.
20. The Specific Plan to include provision for a Multi-Use Trail Corridor from the north-east corner of the site to connect with the Cottonwood Creek Corridor. Such Corridor shall be established along a topographically acceptable route to be determined by the City in consultation with County Parks staff and developer, which may include County properties off-site. Provision of a trail corridor does not imply construction or right to use until such time as a trail program is implemented.
21. Improvement plans, including sewer and water, shall be approved prior to the final map approval.

APPROVED
3-14-89 C.C.

CONDITIONS OF APPROVAL FOR SPECIFIC PLAN 88-1 (COTTONWOOD HILLS)

22. Conceptual improvement plan necessary to mitigate the impacts of increased storm run-off shall be provided by the developer as determined by the City prior to final map approvals.
23. Developer shall negotiate to assist the schools in providing adequate school facilities to serve the project in a timely manner, which may include dedication of land and improvements in lieu of fees. In the event of failure to reach agreements prior to approval of Phase II tentative tract maps the City shall review the potential requirements of alternative measures by the developer to assist in the provision of facilities in a timely manner.
24. Turn-key public parks shall be provided for Cottonwood Hills as follows:
 - a. Developer shall deliver the land, infrastructure, and rough grading for 30.1 acres of developed park lands, including two (2) 5.0 acre neighborhood parks and 20.1 acres of community park. In addition, the developer shall provide turn-key park improvements for these developed parks per approval of the Community Services Director, not to exceed a total of 1.5 million dollars (1989 dollars adjusted by the annual Engineers News Record index to the years in which parks construction occurs).
 - b. Developer shall also deliver 12.0 acres of passive park development, including the 4.4 acres of parkway park and the 7.6 acres of passive park area within the community park.
25. The City shall contract for progressive increases in police services from the General Fund or City-wide Services District or similar to provide an adequate level of services. The developer shall participate in any City effort to increase levels of service through development of a City-wide program.
26. Developer shall dedicate open space as described in the Specific Plan concurrent with related tentative tract maps. City shall develop management plans to insure preservation of habitat values and protection of public safety. Developer shall dedicate conservation easements to the City for private open space as specified in the Specific Plan prior to final map approvals. Operation and maintenance of open space habitat shall be provided through the Cottonwood Hills Lighting and Open Space Maintenance District to be formed by the developer concurrent with final map approvals.
27. The Developer, at the appropriate related phases, shall be responsible to provide a re-evaluation of project related off-site traffic impacts increases over the present traffic study projections; and provide mitigation measures to the satisfaction of the City Engineer if required. The Developer's financial contribution to Railroad Canyon Road, Cottonwood Canyon Road, Cottonwood Hills Road, Lost Road and Holland Road, both on-site and off-site shall not exceed that amount required by the City's Transportation Mitigation Fee Program except that the developer in any event shall fully construct said road improvements within the Specific Plan boundaries and participate in the Railroad Canyon Road Assessment District as determined by the City Engineer.
28. DELETED.

APPROVED
3-14-89 C.C.

CONDITIONS OF APPROVAL FOR SPECIFIC PLAN 88-1 (COTTONWOOD HILLS)

29. The development of the Specific Plan shall meet the following conditions regarding fire protection:
- a. All water mains and fire hydrants providing required fire flows shall be constructed in accordance with the appropriate sections of Riverside County Ordinance No. 460 and/or 546, subject to the approval by the Riverside County Fire Department.
 - b. All buildings shall be constructed with fire retardant roofing material as described in Section 3203 of the Uniform Building code. Any wood shingles or shakes shall have a Class "B" rating and shall be approved by the Fire Department prior to installation.
 - c. A Homeowners Association or Community Service District shall be responsible for the maintenance of all common open space areas. To insure that fuel modification areas are maintained and that annual vegetation is removed consistent with current fire protection standards, prior to the occupancy of more than fifty percent (50%) of the units adjoining an open individual space area, the appropriate agency shall prepare and submit to the Fire Department for review a five (5) year maintenance plan to include specific recommendations for maintenance of various areas.
 - d. The existing County Fire Stations will be capable of providing coverage for the area along Railroad Canyon Road and approximately 3/4 of a mile south along Cottonwood Canyon Road. Therefore, prior to any development beyond Phase 1 or the Open Phase north of Railroad Canyon Road (Street "A"), a site shall have to be selected near I-15 and Railroad Canyon Road and a fire station shall have been constructed and be in operation.
 - e. Alternate or secondary access will be required for any construction beyond Phase 1. The extension of Cottonwood Canyon Road to Holland Road and a connection to a County maintained road shall be maintained in an all-weather driveable condition for any construction beyond Phase 1.
30. The text of the Final Specific Plan shall be revised as shown in Exhibit "D" (attached) prior to the effective date of the Plan.

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3-14-89 C.C.

EXHIBIT B
 MITIGATION MONITORING PROGRAM
 SPECIFIC PLAN 88-1

IMPACT	SIG.	MITIGATION	MONITORING
1. Riparian Off-Site	*	Railroad Canyon Road/San Jacinto River Revegetation Program	Federal and State Permit. City approval prior to grading permits for Railroad Canyon Road. Implementation concurrent with construction. Final inspection by City staff prior to
2. Riparian On-Site	*	Cottonwood Creek Floodplain modification and revegetation program. This program shall be incorporated as a condition of each affected tentative tract map approval to be complied with prior to final map approval or grading permit (whichever occurs first) and which shall take precedence requiring revisions to the map to comply with the program if necessary.	Incorporation as a condition of each affected tentative tract map. City, Federal and State permit approval and implementation prior to grading of related phase. Final inspection by City staff prior to Certificate of Occupancy of each related phase.
3. Stephens' Kangaroo Rat (SKR)	*	Interim habitat conservation plan to be adopted. U.S. Fish and Wildlife Service approval of interim plan.	Interim Plan approval by the City prior to Tentative Map approvals. City, Federal and/or State approval and implementation prior to grading permits for occupied habitat.
4. Cultural Resources Off-Site	*	A survey of affected areas related to Railroad Canyon Road shall be evaluated by the City prior to project grading. Presence of an archeologist or paleontologist for on-site evaluation and full mitigation should any resource be uncovered during construction.	Condition of grading permit approval for Railroad Canyon Road.
5. Cultural Resources On-Site	*	A cultural resources mitigation program. This program shall be incorporated as a condition of each affected tentative tract map approval to be complied with prior to final map approval or grading permit (whichever occurs first) and which shall take precedence requiring revisions to the map to comply with the program if	Developer to draft a program as a condition of Tentative Tract Map. City approval and implementation prior to grading of related area.

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necessary. The program shall be based on site evaluations by qualified archeologists and historians to establish significance of each site and a detailed plan for appropriate mitigation.

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|--------------------------|---|--|--|
| 6. Flood Hazard | * | Tentative Tract Maps shall provide for the base flood (100-year flood) within the existing channel and related facilities of the San Jacinto River and Cottonwood Creek. All other flood areas shall be provided with facilities to convey waters to these channels. | Improvement plan approval prior to final map approvals. Implementation concurrent with construction of the related phase. Final inspection by City prior release of bonds. |
| 7. Water Conservation | | All structures shall be designed to incorporate all state and local regulations. | Compliance at Building Permit Plan Check. Implementation concurrent with construction, subject to final inspection by City. |
| 8. Recycling | | All site-planning shall incorporate measures to promote waste reduction and recycling to the extent feasible. | Design Review requirement by City. Implementation concurrent with construction, subject to final inspection by City. |
| 9. Erosion Control | * | Grading and construction plans shall incorporate the measures listed in the EIR to reduce and control erosion potential. | Grading Plan Check by City. Implementation concurrent with construction. |
| 10. Circulation off-Site | * | Developer shall provide for the paving of Holland Road for two travel lanes from the project boundary east to existing pavement to meet the approval of the County Road Department to partially mitigate project related traffic impacts as they occur. Impacts on Cottonwood Creek Road and Lost Foad, although potentially significant, are not proposed for mitigation beyond the on-site improvements provided for in the Specific Plan. | Approvals part of related tract maps for Holland and Railroad Canyon Road improvements. Improvement plans for Holland Road off-site to County Road Department standards concurrent with project traffic impacts. |
| | | A program shall be implemented for the construction of Railroad Canyon Road from I-15 to the north project boundary. | Program approval by City prior to first phase final tract map and implementation prior to issuance of first building permit of final development phase (at a minimum). All improvement plans subject to City approval. |

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3-14-89 C.C.

run-off shall be provided by the developer as determined by the City prior to final map approvals.

mentation concurrent with construction. Final inspection prior to occupancy of related phase.

17. Schools *

Developer shall negotiate assistance to the schools in providing adequate school facilities to serve the project in a timely manner which may include dedication of land and improvements in lieu of fees. In the event of failure to reach agreements prior to approval of Phase II tentative tract maps the City shall review the potential requirements of alternative measures by the developer to assist in the provision of facilities in a timely manner.

City approval of agreements or potential alternative measure prior to Phase II tentative tract maps.

Implementation concurrent with development by phases.

18. Parks *

Developer shall deliver complete turn-key park facilities to meet provisions of Condition of Approval #24 for dedication of land and level of improvements as approved by the Community Development Director.

City approval of parks development plans concurrent with related tract maps. Implementation concurrent with related tract development. Completion prior to occupancies of related tracts.

19. Police *

The City shall contract for progressive increases in police services. From the General Fund or City-wide Services District or similar to provide an adequate level of services.

Approval in City budgets starting in year 1 of project occupancy.

20. Open Space *

Developer shall dedicate open space as described in the Specific Plan. City shall develop management plans to insure preservation of habitat values and protection of public safety. Developer shall dedicate conservation easements to the City for private open space as specified in the Specific Plan.

Dedications concurrent with final maps.

City plan adoption prior to final map approvals.

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3-14-89 C.C.

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